

**PUBLIC UTILITIES COMMISSION**

505 VAN NESS AVENUE



March 9, 2020

**Advice Letter 5660-E**

Erik Jacobson  
Director, Regulatory Relations  
Pacific Gas and Electric Company  
77 Beale Street, Mail Code B10C  
P.O. Box 770000  
San Francisco, CA 94177

**SUBJECT: Encroachment Agreement with Hibiscus Properties, LLC, to Construct a Pedestrian/Bike Bridge - Request for Approval Under Section 851 and General Order 173.**

Dear Mr. Jacobson:

Advice Letter 5660-E is approved as of February 27, 2020 per resolution E-5066 Ordering Paragraphs.

Sincerely,

A handwritten signature in cursive script that reads "Edward Randolph".

Edward Randolph  
Deputy Executive Director for Energy and Climate Policy/  
Director, Energy Division



October 15, 2019

**Advice 5660-E**

(Pacific Gas and Electric Company ID U 39 E)

Public Utilities Commission of the State of California

**Subject: Encroachment Agreement with Hibiscus Properties, LLC, to  
Construct a Pedestrian/Bike Bridge - Request for Approval Under  
Section 851 and General Order 173**

**Purpose**

Pacific Gas and Electric (PG&E) requests approval under Public Utilities Code Section 851 (Section 851) and General Order 173 to grant an Encroachment Agreement (Agreement), Attachment 1, between PG&E and Hibiscus Properties, LLC (Hibiscus), an affiliate of Facebook, Inc. The Agreement grants permission to Hibiscus to install one Pedestrian/Bike Bridge (Bridge), a portion of which crosses over a section of PG&E electric transmission easement (Easement) located within property owned by Hibiscus.

PG&E has reviewed the Bridge plans and has determined that the Bridge will not interfere with PG&E's operations or PG&E's ability to provide safe and reliable utility service to its customers. In addition, this Bridge will not be averse to the public interest. The Bridge will provide the public pedestrian and non-motorized bicycle traffic safe passage over and across State Route (SR) 84, Bayfront Expressway (SR 84), a 6-lane State highway, enhancing connectivity between the Belle Haven neighborhood and the Bayfront Park and the Bay Trail. The Bridge will promote pedestrian and bicyclist connections to off-site pedestrian, bicycle and transit networks.

**Background**

PG&E has owned a 95foot wide non-exclusive easement for its Bair-Cooley Landing #1 and #2 60kV electric transmission tower line along SR 84 since 1967 (the "1967 Easement"). In 2000, the 1967 Easement was modified to include telecommunication facilities. In November of 2016, Hibiscus granted to PG&E a 10foot wide non-exclusive easement along a northerly portion of the lands of Hibiscus, and more particularly described in said easement document for PG&E's underground conduits, pipes, service boxes, and other related facilities for the distribution of electric energy and communications (the "2016 Easement"). The 1967 Easement and the 2016 Easement (collectively, the "Easements") are included as Attachment 6. The Easements provides that the fee owner of the property (i.e. Hibiscus) shall not erect or construct any building or other structure within the easement area.

On November 1, 2016, the Menlo Park City Council adopted the Facebook Campus Expansion Project Final EIR, Resolution No. 6351, (Attachment 2). Hibiscus, an affiliate of Facebook, Inc., is the project sponsor and lead developer of the Facebook Campus Expansion Project (Project). The Project is a redevelopment of an approximate 58-acre Facebook campus site (the "Property") located at 301-309 Constitution Drive within the City of Menlo Park, County of San Mateo, State of California, Assessor's Parcel Number 055-260-260. A copy of the Facebook Campus Expansion Project Master Plan is included as Attachment 3. The Property is located off Constitution Drive, east of Chilco Street and south of SR 84 in Menlo Park (the "Property"). The description of the Property can be found in Attachment 4.

The Project includes the installation of the Bridge, which will cross over SR 84, allowing public access to the San Francisco Bay Trail and Bedwell Bayfront Park from the Facebook campus site and the Belle Haven neighborhood. The Bridge will be available to the public and maintained by Hibiscus.

A portion of the Bridge will cross a portion of the Easements and under its overhead Bair-Cooley Landing #1 and #2 conductor facilities. To support the bridge, foundations and columns would be constructed within the PG&E's easement. A Location Map with Bridge Layout and other details is provided in Attachment 7, Part 2 page 4. The Location Map shows that 2,809 square feet, or 0.064 acres, of the Bridge would encroach on PG&E's Easements (also shown in Exhibit B of Attachment 1).

Because PG&E's Bair-Cooley Landing facilities run a long distance electric overhead line along the south side of SR 84, the entire length of and within the Hibiscus property, there is no viable alternative to place the Bridge whereby a grade separated crossing could be installed without crossing PG&E's overhead 60kV electric transmission facilities. The Encroachment Agreement (Attachment 1) consents to the the Bridge encroachment, subject to the terms and conditions set forth in the Agreement and would allow Hibiscus to construct the Bridge. The construction of the bridge provides pedestrian and bicycle traffic safe passage across and over SR 84.

PG&E has determined that the improvements referenced in the Encroachment Agreement do not interfere with PG&E's use of the easement area or impede the safety and reliability of PG&E's electric or communications operations. The Bridge design meets required electrical clearances to the transmission conductors and PG&E will continue to have access to its overhead and underground electric transmission and communication facilities for maintenance or safety-related work that may become necessary. Therefore, PG&E is willing to agree to allow the Bridge subject to the terms and conditions described in the Encroachment Agreement. PG&E requests the California Public Utilities Commission's approval to grant the Agreement under Section 851 and General Order 173, as the Bridge is compatible with PG&E's use, but not compliant with the Easements restriction against the erection buildings and structures by the fee-owner of the property.

**(a) Identity and Addresses of All Parties to the Proposed Transaction:**

Pacific Gas and Electric Company  
Molly Zimney  
Law Department  
P.O. Box 7442  
San Francisco, CA 94120  
Telephone: (415) 973-6840  
Facsimile: (415) 973-5520  
Email: MEZ3@pge.com

Marinus Lamprecht  
Hibiscus Properties, LLC  
1 Facebook Way  
Menlo Park, CA 94025  
650-460-0602  
miamprecht@fb.com

Natalia Bernardi  
Professional Civil Engineer  
BFK Engineers  
4670 Willow Road, Suite 250  
Pleasanton, CA 94588  
925-396-7700  
nbernardi@bkf.com

**(b) Complete Description of the Property Including Present Location, Condition and Use:**

The Project is on an approximately a 58-acre industrial site owned by Hibiscus. The Project located at Constitution Drive, bound by Chilco Street to the west and south, SR 84 (Bayfront Expressway) to the north, and Facebook Way to the east. It is designated as APN 055-260-260.

Within the Easements area, PG&E owns and operates (1) overhead electric transmission facilities, (2) overhead telecommunications facilities, and (3) underground conduits, pipes, service boxes, and other related facilities for the distribution of electric energy and communications. PG&E operates these facilities under the Easements defined and described above.

**(c) Intended Use of the Property:**

A permanent pedestrian/bicycle bridge is proposed across a portion of the Easements.

The Bridge will allow passage by non-motorized modes of traffic, pedestrians and bicyclists and will provide a safe manner to cross SR 84, a 6-lane State highway facility. To support the ridge, underground foundations and columns would be constructed within the PG&E's easement. The portion of the Bridge within the area crossing 2,809 square feet, or 0.064 acres, of the Bridge would encroach on PG&E's Easements. A Location Map with Bridge Layout and other details is provided in Attachment 7 Part 2 page 4.

**(d) Complete Description of Financial Terms of the Proposed Transaction:**

PG&E is not collecting any fees associated with Agreement. PG&E has determined that the construction of the Bridge would not affect the value of its Easements.

**(e) Description of How Financial Proceeds of the Transaction Will Be Distributed:**

Not Applicable

**(f) Statement on the Impact of the Transaction on Ratebase and Any Effect on the Ability of the Utility to Serve Customers and the Public:**

There is no impact to PG&E's rate base nor will granting the proposed Agreement for the installation of the portion of the Bridge within the PG&E's easement affect PG&E's ability to service customers and the public or to maintain/repair/replace PG&E's electric transmission facilities.

**(g) The Original Cost, Present Book Value, and Present Fair Market Value for Sales of Real Property and Depreciable Assets, and a Detailed Description of How the Fair Market Value Was Determined (e.g., Appraisal):**

Not Applicable

**(h) The Fair Market Rental Value for Leases of Real Property, and a Detailed Description of How the Fair Market Rental Value Was Determined:**

Not Applicable

**(i) The Fair Market Value of the Easement or Right-of-Way, and a Detailed Description of How the Fair Market Value Was Determined:**

The improvements within the Easement area do not rise to the level of a right that has any realizable economic value to PG&E.

**(j) A Complete Description of any Recent Past (Within the Prior Two Years) or Anticipated Future Transactions that May Appear to Be Related to the Present Transaction:**

Not Applicable

**(k) Sufficient Information and Documentation (Including Environmental Information) to Show that All of Eligibility Criteria Set Forth in Rule 3 of General Order 173 are Satisfied:**

PG&E has provided information in this Advice Letter to satisfy the eligibility criteria under General Order 173 in that:

- The activity proposed in the transaction will not require environmental review by the CPUC as a Lead Agency;
- The transaction will not have an adverse effect on the public interest or on the ability of PG&E to provide safe and reliable service to its customers at reasonable rates;
- The transaction will not materially impact the rate base of PG&E; and
- The transaction does not warrant a more comprehensive review that would be provided through a formal Section 851 application.

**(l) Additional Information to Assist in the Review of the Advice Letter:**

All information to assist in review has been provided in this Advice Letter.

**(m) Environmental Information**

Pursuant to General Order 173, the Advice Letter program applies to proposed transactions that will not require environmental review by the CPUC as a lead agency under the California Environmental Quality Act ("CEQA") either because: (a) a statutory or categorical exemption applies (the applicant must provide a Notice of Exemption from the Lead Agency or explain why an exemption applies), or (b) because the transaction is not a project under CEQA (the applicant must explain the reasons why it believes that the transaction is not a project), or (c) because another public agency, acting as the Lead Agency under CEQA, has completed environmental review of the project, and the Commission is required to perform environmental review of the project only as a Responsible Agency under CEQA.

For this Advice Letter, the City of Menlo Park has completed an environmental assessment as a Lead Agency, and the Commission can serve as a Responsible Agency.

c. City of Menlo Park as a Responsible Agency under CEQA

If another public agency, acting as the Lead Agency under CEQA, has completed an environmental review of the project and has approved the final CEQA documents, and the Commission is a Responsible Agency under CEQA, the applicant shall provide the following.

- a. The name, address, and phone number of the Lead Agency, the type of CEQA document that was prepared (Environmental Impact Report, Negative Declaration, Mitigated Negative Declaration), the date on which the Lead Agency approved the CEQA document, the date on which a Notice of Determination was filed.

<b>Lead Agency</b>	City of Menlo Park 701 Laurel Street Menlo Park, CA 94025 Attn. Kyle Perata, Sr. Planner 650-330-6721 <a href="mailto:ktperata@menlopark.org">ktperata@menlopark.org</a>
<b>Type of CEQA Document Prepared</b>	Final Environmental Impact Report
<b>Date Name of Document Prepared Adopted</b>	Facebook Campus Expansion Project Final EIR State Clearinghouse No. 2015062056 Adopted November 1, 2016
<b>Date Notice of Determination Filed</b>	November 4, 2016

- b. A copy of all CEQA documents prepared by or for the Lead Agency regarding the project and the Lead Agency’s resolution or other document approving the CEQA documents.

See Attachment 2, Resolution No. 6351 - City of Menlo Park Adopting Findings – Adopted November 1, 2016 and Attachment 8, Notice of Determination – Filed November 4, 2016.

The entire environmental analysis of the Facebook Campus Expansion Project may be viewed at the following website:  
<https://www.menlopark.org/1012/Environmental-Impact-Report>

- c. A list of section and page numbers for the environmental impacts, mitigation measures, and findings in the prior CEQA documents that relate to the approval sought from the Commission.

The references to the Bridge, a portion of which will cross PG&E’s easement and that will provide pedestrian and bicycle traffic safe passage over and across State Route (SR) 84, Bayfront Expressway (SR 84), enhancing public connectivity between the Belle Haven neighborhood and the Bayfront Park and the Bay Trail, may be found in the City of Menlo Park Adopting Findings, Resolution No. 6351, shown in Attachment 2. Below are the specific references to the Bridge in Attachment 2:

1. The Bridge, along with other mitigation measures to avoid or substantially lessen one or more significant effects of the Project, is addressed in Section V. Project Alternatives of Attachment 2, Pages 54 through 56. The Bridge is specifically mentioned in item 7 of Page 55: *Create a bicycle-ped-friendly environment that enhances connectivity between the Belle Haven neighborhood and Bayfront Park and the Bay Trail.*
  2. Section VI, Statement of Overriding Considerations, (b) Social Benefits, 2<sup>nd</sup> bullet, Page 60: *The Project would meet the City's land use planning goals and development of strategies for the M-2 Area, and promote pedestrian and bicyclist connections by creating on-site and off-site pedestrian and bicycle amenities, and approving connections to off-site pedestrian, bicycle and transit networks.*
  3. Section VI, Statement of Overriding Considerations, (e) Local Community Benefits, 2<sup>nd</sup> bullet, Page 61: *The Project would provide a new, publicly-accessible, multi-use bridge across the Bayfront Expressway that will provide a safe connection from Belle Haven to the Bay"*
- d. An explanation of any aspect of the project or its environmental setting which has changed since the issuance of the prior CEQA document.
- Not Applicable – There are no prior CEQA documents.
- e. A statement of whether the project will require approval by additional public agencies other than the Commission and the Lead Agency, and, if so, the name and address of each agency and the type of approval required.

See Attachment 9 -The Bridge was also reviewed under the National Environmental Policy Act (NEPA) with Environmental Assessment (EA) and Finding of No Significant Impact (FONSI) (U.S. Department of the Interior Fish and Wildlife Service as the lead agency).

**Protests**

Anyone wishing to protest this submittal may do so by sending a letter by November 4, 2019, which is 20 days from the date of this submittal. The protest must state the grounds upon which it is based, including such items as financial and service impact, and should be submitted expeditiously. Protests should be mailed to:

CPUC Energy Division  
ED Tariff Unit  
505 Van Ness Avenue, 4<sup>th</sup> Floor  
San Francisco, California 94102

Facsimile: (415) 703-2200  
E-mail: EDTariffUnit@cpuc.ca.gov

Copies of protests also should be mailed to the attention of the Director, Energy Division, Room 4004, at the address shown above.

The protest shall also be sent to PG&E either via E-mail or U.S. mail (and by facsimile, if possible) at the address shown below on the same date it is mailed or delivered to the Commission:

Erik Jacobson  
Director, Regulatory Relations  
c/o Megan Lawson  
Pacific Gas and Electric Company  
77 Beale Street, Mail Code B13U  
P.O. Box 770000  
San Francisco, California 94177

Facsimile: (415) 973-3582  
E-mail: PGETariffs@pge.com

Any person (including individuals, groups, or organizations) may protest or respond to an advice letter (General Order 96-B, Section 7.4). The protest shall contain the following information: specification of the advice letter protested; grounds for the protest; supporting factual information or legal argument; name, telephone number, postal address, and (where appropriate) e-mail address of the protestant; and statement that the protest was sent to the utility no later than the day on which the protest was submitted to the reviewing Industry Division (General Order 96-B, Section 3.11).

**Effective Date**

Pursuant to the review process outlined in General Order 173, PG&E requests that this Tier 3 advice submittal become effective upon Commission Approval.

**Notice**

In accordance with General Order 96-B, Section IV, a copy of this advice letter is being sent electronically and/or via U.S. mail to parties shown on the attached list. Address changes to the General Order 96-B service list should be directed to PG&E at email address PGETariffs@pge.com. For changes to any other service list, please contact the Commission's Process Office at (415) 703-2021 or at Process\_Office@cpuc.ca.gov. Send all electronic approvals to PGETariffs@pge.com. Advice letter submittals can also be accessed electronically at: <http://www.pge.com/tariffs>.

\_\_\_\_\_  
/S/

Erik Jacobson  
Director, Regulatory Relations

**Attachments**

Attachment 1	Encroachment Agreement
Attachment 2	Environmental Approval
Attachment 3	Facebook Campus Expansion Project Master Plan
Attachment 4	Hibiscus Property Description
Attachment 5	Draft Bridge Maintenance Agreement
Attachment 6	PG&E Easements (LD 2305-03-1503, 2305-03-2103 and 2350-03-2243)
Attachment 7	Location Map with Bridge Layout
Attachment 8	Notice of Determination
Attachment 9	Environmental Assessment and Finding of No Significant Impact (U.S. Department of the Interior Fish and Wildlife Service)

**\*\*\*\*\* SERVICE LIST for Advice 5660-E \*\*\*\*\***  
**APPENDIX A**

Jonathan Reiger  
Legal Division  
505 Van Ness Avenue  
San Francisco, CA 94102  
(415) 355-5596  
jzr@cpuc.ca.gov

Mary Jo Borak  
Energy Division  
505 Van Ness Avenue  
San Francisco, CA 94102  
(415) 703-1333  
bor@cpuc.ca.gov

Robert (Mark) Pocta  
Public Advocates Office  
505 Van Ness Avenue  
San Francisco, CA 94102  
(415) 703- 2871  
robert.pocta@cpuc.ca.gov

Andrew Barnsdale  
Energy Division  
505 Van Ness Avenue  
San Francisco, CA 94102  
(415) 703-3221  
bca@cpuc.ca.gov

City of Menlo Park  
Attention: Kyle Perata  
Sr. Planner for Environmental Document  
701 Laurel Street  
Menlo Park, CA 94025  
650-330-6721  
[ktperata@menlopark.org](mailto:ktperata@menlopark.org)

City of Menlo Park  
Attention: Theresa Avedian  
Sr. Civil Engineer  
701 Laurel Street  
Menlo Park, CA 94025  
650-330-6779  
[tavedian@menlopark.org](mailto:tavedian@menlopark.org)



# ADVICE LETTER SUMMARY

## ENERGY UTILITY



MUST BE COMPLETED BY UTILITY (Attach additional pages as needed)

Company name/CPUC Utility No.: Pacific Gas and Electric Company (ID U39 E)

Utility type:

- ELC       GAS       WATER  
 PLC       HEAT

Contact Person: Annie Ho

Phone #: (415) 973-8794

E-mail: PGETariffs@pge.com

E-mail Disposition Notice to: AMHP@pge.com

EXPLANATION OF UTILITY TYPE

ELC = Electric      GAS = Gas      WATER = Water  
 PLC = Pipeline      HEAT = Heat

(Date Submitted / Received Stamp by CPUC)

Advice Letter (AL) #: 5660-E

Tier Designation: 3

Subject of AL: Encroachment Agreement with Hibiscus Properties, LLC, to Construct a Pedestrian/Bike Bridge - Request for Approval Under Section 851 and General Order 173

Keywords (choose from CPUC listing): Agreements, Section 851

AL Type:  Monthly  Quarterly  Annual  One-Time  Other:

If AL submitted in compliance with a Commission order, indicate relevant Decision/Resolution #:

Does AL replace a withdrawn or rejected AL? If so, identify the prior AL: No

Summarize differences between the AL and the prior withdrawn or rejected AL:

Confidential treatment requested?  Yes  No

If yes, specification of confidential information:

Confidential information will be made available to appropriate parties who execute a nondisclosure agreement. Name and contact information to request nondisclosure agreement/ access to confidential information:

Resolution required?  Yes  No

Requested effective date:

No. of tariff sheets: N/A

Estimated system annual revenue effect (%): N/A

Estimated system average rate effect (%): N/A

When rates are affected by AL, include attachment in AL showing average rate effects on customer classes (residential, small commercial, large C/I, agricultural, lighting).

Tariff schedules affected:

Service affected and changes proposed<sup>1</sup>: N/A

Pending advice letters that revise the same tariff sheets: N/A

<sup>1</sup>Discuss in AL if more space is needed.

**Protests and all other correspondence regarding this AL are due no later than 20 days after the date of this submittal, unless otherwise authorized by the Commission, and shall be sent to:**

CPUC, Energy Division  
Attention: Tariff Unit  
505 Van Ness Avenue  
San Francisco, CA 94102  
Email: [EDTariffUnit@cpuc.ca.gov](mailto:EDTariffUnit@cpuc.ca.gov)

Name: Erik Jacobson, c/o Megan Lawson  
Title: Director, Regulatory Relations  
Utility Name: Pacific Gas and Electric Company  
Address: 77 Beale Street, Mail Code B13U  
City: San Francisco, CA 94177  
State: California Zip: 94177  
Telephone (xxx) xxx-xxxx: (415)973-2093  
Facsimile (xxx) xxx-xxxx: (415)973-3582  
Email: [PGETariffs@pge.com](mailto:PGETariffs@pge.com)

Name:  
Title:  
Utility Name:  
Address:  
City:  
State: District of Columbia Zip:  
Telephone (xxx) xxx-xxxx:  
Facsimile (xxx) xxx-xxxx:  
Email:

# **Attachment 1**

## **Encroachment Agreement**



RECORDING REQUESTED BY AND RETURN TO:

**PACIFIC GAS AND ELECTRIC COMPANY**  
245 Market Street, N10A, Room 1015  
P.O. Box 770000  
San Francisco, California 94177

Location: City/Uninc \_\_\_\_\_  
Recording Fee \$ \_\_\_\_\_  
Document Transfer Tax \$ \_\_\_\_\_

- This is a conveyance where the consideration and Value is less than \$100.00 (R&T 11911).
- Computed on Full Value of Property Conveyed, or
- Computed on Full Value Less Liens & Encumbrances Remaining at Time of Sale
- Exempt from the fee per GC 27388.1 (a) (2); This document is subject to Documentary Transfer Tax

\_\_\_\_\_  
Signature of declarant or agent determining tax

(APN 055-260-250)  
LD#

### ENCROACHMENT AGREEMENT

This Encroachment Agreement (this "**Agreement**") is made and entered into this \_\_\_ day of \_\_\_\_\_, 20\_\_ by PACIFIC GAS AND ELECTRIC COMPANY, a California corporation, hereinafter called "**PG&E**", and HIBISCUS PROPERTIES, LLC, a Delaware limited liability company, an affiliate of Facebook, Inc., hereinafter called "**Owner**."

#### RECITALS

A. Owner is the fee title owner of certain real property within the City of Menlo Park, County of San Mateo, State of California, legally described in **Exhibit "A"** (hereinafter, the "**Property**") attached hereto and made a part hereof.

B. PG&E is the owner of those certain easements and rights-of-way (the "**Electric Transmission Easement**") for the its electric transmission line, the Bair-Cooley Landing #1 & #2 60kV, and for all other purposes permitted therein, as set forth in the Easement dated December 15, 1967 and recorded in Volume 5422 of Official Records at page 99, as further modified by that certain Agreement Modifying Easement dated June 23, 2000 and recorded in Document No. 2001-026990 of Official Records, San Mateo County Records. The Electric Transmission Easement provides in part that "First Party shall have the right to use said strip for purposes not inconsistent with second party's full enjoyment of the rights hereby granted, provided that first party shall not



erect or construct any building or other structure, or drill or operate any well, within said strip.” In addition, PG&E is the owner of the easement right of way for its electric distribution facilities (the "**Electric Distribution Easement**") and for all other purposes connected therewith, as set forth in the easement dated November 1, 2016 and recorded in Document No. 2017-037078 of Official Records San Mateo County Records. The Electric Distribution Easement provides in part that “Grantor shall not erect or construct any building or other structure or drill or operate any well within said strip.” The Electric Transmission Easement and the Electric Distribution Easement shall be referred to collectively herein as the "**Easement**." The portion of the Property encumbered by the Easement is hereinafter referred to as the "**Easement Area**."

C. Owner intends to construct a Pedestrian/Bicycle Bridge including other improvements associated therewith such as underground foundations and columns (collectively, the "**Improvements**") on a portion of the Easement Area, the construction of which violates the express restrictions against the construction of any buildings or other structures contained in the Easement. The portion of the Easement Area upon which the improvements are to be made (the "**Encroachment Area**") is identified and shown on the map labeled **Exhibit "B"** attached hereto and made a part hereof.

D. Owner has requested that PG&E grant permission for the construction of the Improvements within the Easement Area. PG&E has determined that the Improvements, to be constructed pursuant to plans and specifications approved by PG&E, do not interfere with the present full use of the Easement Area by PG&E, and PG&E is therefore willing to agree to allow such encroachment on the Easement Area on the terms and subject to the conditions set forth herein.

NOW, THEREFORE, in consideration of the foregoing and for other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, Owner and PG&E hereby agree as follows:

1. Consent to Encroachment. Notwithstanding the restriction in the Easement, PG&E hereby consents to the encroachment of the Improvements onto the Easement Area in the location as more specifically set forth in Exhibit "B" subject to the terms and conditions set forth herein. In addition, Owner shall have the right of ingress and egress over the Easement Area to obtain access to the Encroachment Area and the Improvements when necessary to fulfill Owner's obligations under this Agreement, in such areas as PG&E determines, in its sole and absolute discretion, will occasion the least practicable damage and inconvenience to PG&E, its facilities and operations.

2. Governmental Approvals. This Agreement shall not become effective, notwithstanding that it may have been executed and delivered by the parties, and Owner shall not commence any activity hereunder, unless and until PG&E notifies Owner in writing of receipt of final, unconditional, and unappealable approval (including exhaustion of all administrative appeals or remedies before the CPUC) of this Agreement by the California Public Utilities Commission (the "**CPUC**"), and that the terms and conditions of such CPUC approval are satisfactory to PG&E in its sole and absolute discretion.



3. Termination; Restoration. PG&E may terminate Owner's rights under this Agreement, at any time, upon ninety (90) days written notice to the Owner, if PG&E, in its sole and absolute discretion, should determine that Owner's use of the Easement Area is inconsistent with PG&E's operational needs in the future, or in any way interferes with, impairs or otherwise impedes PG&E's full use of facilities installed or that may be installed by PG&E in the vicinity of the Easement Area. Upon such termination, Owner, at Owner's sole cost and expense, shall remove all Improvements that encroach upon the Easement Area and shall repair and restore the Easement Area as nearly as possible to the condition that existed prior to the construction of said Improvements. Owner shall pay the entire cost of such removal and restoration, and PG&E shall have no liability for any costs caused by or related to any such termination. If Owner fails to remove all Improvements that encroach onto the Easement Area or fails to repair or restore the Property within said ninety (90) day period, PG&E may perform such removal, repair or restoration as necessary and recover such costs and expenses therefore from Owner. Owner agrees to allow access to PG&E onto the Property for such purpose, and Owner shall pay all such costs and expenses within sixty (60) days of receipt of an invoice therefore. Owner further acknowledges that PG&E's termination right shall not be affected by any Improvements that Owner has made to the Easement Area, regardless of the nature or extent of those Improvements. Owner understands and agrees that notwithstanding that Owner may have made a substantial investment in such improvements, Owner shall not be entitled to any compensation whatsoever for the termination of Owner's rights under this Agreement by PG&E. Notwithstanding the foregoing, promptly (and in no event more than fifteen (15) days) following Owner's receipt of PG&E's notice of termination, Owner and PG&E will meet and confer in good faith to attempt to determine a mutually acceptable alternative to termination in order that will minimize the interference, impairment, or encroachment that is the subject of PG&E's notice. (Owner to initial here \_\_\_\_\_).

4. Relocation/Rearrangement of PG&E's Facilities. In recognition of the nature and extent of the Improvements and the potential impacts to Owner's business operations associated with termination of the Encroachment Agreement under Section 3, PG&E may require Owner, at Owner's sole expense, to rearrange or relocate the facilities of PG&E or its authorized licensees installed within the Easement, at any time, upon ninety (90) days written notice to the Owner, if PG&E, in its sole and absolute discretion, should determine that Owner's use of the Encroachment Area interferes with, impairs or otherwise impedes PG&E's full use of facilities installed or that may be installed by PG&E in the Easement, or in any way interferes with, impairs or otherwise impedes PG&E's ability to fulfill its obligations to third party licensees that may attach communication facilities to PG&E's facilities. Upon such notice to rearrange or relocate, Owner, at Owner's sole cost and expense, shall enter into a written agreement with prepayment for such rearrangement or relocation. Owner shall pay the entire cost of such rearrangement or relocation, and PG&E shall have no liability for any costs caused by or related to any such rearrangement or relocation. If for any reason Owner fails or refuses to enter into such an agreement for rearrangement or relocation of PG&E facilities, PG&E shall exercise its right to terminate the Encroachment Agreement in accordance with Section 3.

5. Indemnification; Release.

(a) Indemnification. Owner shall, to the maximum extent permitted by law, indemnify, protect, defend and hold harmless PG&E, its parent corporation, subsidiaries and



affiliates, and their respective officers, managers, directors, representatives, agents, employees, transferees, successors and assigns (each, an “**Indemnitee**” and collectively, “**Indemnitees**”) from and against all claims, losses (including, but not limited to, diminution in value), actions, demands, damages, costs, expenses (including, but not limited to, experts fees and reasonable attorneys’ fees and costs) and liabilities of whatever kind or nature (collectively, “**Claims**”), which arise from or are in any way connected with the occupancy or use of the Easement Area by Owner or Owner’s contractors, agents, or invitees (including any member of the general public that Owner may allow use of the Encroachment Area), or the exercise by Owner of its rights hereunder, or the performance of, or failure to perform, Owner’s duties under this Agreement, including, but not limited to, Claims arising out of: (1) injury to or death of persons, including but not limited to employees of PG&E; (2) injury to property or other interest of PG&E, Owner or any third party; (3) violation of any applicable federal, state, or local laws, statutes, regulations, or ordinances, including all legal requirements relating to human health or the environment, and including any liability which may be imposed by law or regulation without regard to fault; excepting only with respect to any Indemnitee, any Claim arising from the sole, active negligence or willful misconduct of such Indemnitee. In the event any action or proceeding is brought against any Indemnitee for any Claim against which Owner is obligated to indemnify or provide a defense hereunder, Owner upon written notice from PG&E shall defend such action or proceeding at Owner’s sole expense by counsel approved by PG&E, which approval shall not be unreasonably withheld, conditioned or delayed.

(b) Release. Owner accepts all risk relating to its occupancy and use of the Easement Area. PG&E shall not be liable to Owner for, and Owner hereby waives, releases, exonerates, discharges and covenants not to sue PG&E and the other Indemnitees from, any and all liability, whether in contract, tort or on any other basis, for any injury, damage, or loss resulting from or attributable to any occurrence on or about the Encroachment Area, the condition of Encroachment Area, the use or occupancy of the Encroachment Area by Owner, or PG&E’s operation and maintenance of PG&E’s facilities in the vicinity of the Encroachment Area, except in the case of any Indemnitee, any injury, damage, or loss arising from the active negligence or willful misconduct of such Indemnitee.

6. Compliance with Laws. Owner shall, at its sole cost and expense, promptly comply with all laws, statutes, ordinances, rules, regulations, requirements or orders of municipal, state, and federal authorities now in force or that may later be in force, with the conditions of any permit, relating to Owner’s use or occupancy of the Easement Area.

7. Alterations. Except for the Improvements authorized pursuant to this Agreement, Owner shall not construct any additional buildings or structures on the Easement Area, nor shall Owner make any alteration, addition or improvement to the Easement Area that would increase the Encroachment Area, either horizontally or vertically.

8. Damage or Destruction. In the event that the Improvements which encroach onto the Easement Area shall be destroyed or demolished, Owner shall not rebuild the Improvements on any part of the Easement Area except pursuant to plans and specifications approved by PG&E.

9. Condition of Easement Area. Owner accepts the Encroachment Area and the Easement Area in its existing physical condition, without warranty by PG&E or any duty or



obligation on the part of PG&E to maintain the Easement Area. Owner understands that numerous hazards, environmental or otherwise, may be located in, on, or underlying the Easement Area, and that hazardous materials may be used in connection with PG&E facilities that may be operated in the Easement Area, and agrees that entry onto the Easement Area is at Owner's sole risk and expense.

10. Maintenance. Owner shall be responsible for the maintenance of the Improvements in good condition and repair, and Owner shall coordinate all activities regarding the maintenance of the Improvements to reasonably minimize any interference with the use by PG&E of the Easement Area, and Owner shall conduct its activities in such a manner so as not to endanger the Easement Area, the environment and human health and safety. Owner shall be responsible for remediation of any hazardous materials release caused by Owner, and to clean and remove debris and/or promptly repair any damages to the Easement Area following any entry or activity by Owner, returning the Easement Area to a like or better condition.

11. Reserved Rights. PG&E reserves the right to use the Encroachment Area for such purposes as it may deem necessary or appropriate if, and whenever, in the interest of its service to its patrons or consumers or the public, it shall appear necessary or desirable to do so. Furthermore, PG&E reserves the right to restrict access to the Encroachment Area if emergency repairs or maintenance are required to PG&E facilities in the vicinity of the Encroachment Area.

12. Insurance. Prior to the Effective Date of this Agreement, Owner shall procure, and thereafter Owner shall carry and maintain in effect at all times the following insurance: Worker's Compensation in compliance with applicable labor codes, acts, laws or statutes, state or federal, where Owner performs work and Employer's Liability insurance with limits not be less than \$1,000,000 for injury or death, each accident; Commercial General Liability for bodily injury and property damage with limits of not less than \$3,000,000 each occurrence/\$6,000,000 aggregate; Business Auto, code 1 "any auto" combined single limit no less than \$1,000,000 each accident. Owner is also responsible for causing its agents, contractors and subcontractors to comply with the insurance requirements of this Agreement at all relevant times. PG&E reserves the right to review and modify from time to time the coverages and limits of coverage required hereunder, as well as the deductibles and/or self-insurance retentions in effect from time to time (but PG&E agrees that it will not increase required coverage limits more often than once in any five-year period).

13. Notice. Any notices or communications hereunder shall be in writing and shall be personally delivered or sent by first class mail, certified or registered, postage prepaid, or sent by national overnight courier, with charges prepaid for next business day delivery, addressed to the addressee party at its address or addresses listed below, or to such other address or addresses for a party as such party may from time to time designate by notice given to the other party. Notices shall be deemed received, if sent by personal delivery upon actual receipt by the party being sent the notice, or on the expiration of three (3) business days after the date of mailing, or on the following business day if sent by overnight courier



If to PG&E:

Manager, Land Management  
Pacific Gas and Electric Company  
P.O. Box 770000, Mail Code N10A  
San Francisco, CA 94177

With a copy to:

Pacific Gas and Electric Company  
P.O. Box 7442, Mail Code B30A  
San Francisco, California 94120  
Attention: Managing Counsel, Commercial and Environmental

If to Owner:

Hibiscus Properties, LLC  
1 Hacker Way  
Menlo Park, CA 94025  
Attention: Facilities – Bayfront Bridge

With a copy to:

Hibiscus Properties, LLC  
1 Hacker Way  
Menlo Park, CA 94025  
Attention: Real Estate Counsel

14. Governing Law. This Agreement shall in all respects be interpreted, enforced, and governed by and under the laws of the State of California.

15. Entire Agreement. This Agreement and the Easement, supersedes all previous oral and written agreements between and representations by or on behalf of the parties and constitutes the entire agreement of the parties with respect to the subject matter hereof. This Agreement may not be amended except by a written agreement executed by the parties.

16. Binding Effect. This Agreement and the covenants and agreements contained herein shall be binding upon, and shall inure to the benefit of, the parties hereto and their respective heirs, successors and assigns (subject to the provisions of Section 17 below). The covenants of Owner hereunder shall run with the land.

17. Assignment. This Agreement and the rights of Owner hereunder are appurtenant to the Property presently owned by Owner and may not be separately assigned, transferred, conveyed or encumbered. Any purported assignment, transfer, conveyance or encumbrance violating the foregoing condition shall be void and of no effect.



18. Attorneys' Fees. Should either party bring an action against the other party, by reason of or alleging the failure of the other party with respect to any or all of its obligations hereunder, whether for declaratory or other relief, then the party which prevails in such action shall be entitled to its reasonable attorneys' fees (of both in-house and outside counsel) and expenses related to such action, in addition to all other recovery or relief. A party shall be deemed to have prevailed in any such action (without limiting the generality of the foregoing) if such action is dismissed upon the payment by the other party of the sums allegedly due or the performance of obligations allegedly not complied with, or if such party obtains substantially the relief sought by it in the action, irrespective of whether such action is prosecuted to judgment.

19. Survival of Obligations. Owner's obligations under Sections 3 and 4 of this Agreement, and all representations, warranties, indemnities or other provisions which by their nature survive termination shall survive the exercise of PG&E's termination rights pursuant to Section 3 of this Agreement.

20. No Waiver. No waiver with respect to any provision of this Agreement shall be effective unless in writing and signed by the party against whom it is asserted. No waiver of any provision of this Agreement by a party shall be construed as a waiver of any subsequent breach or failure of the same term or condition, or as a waiver of any other provision of this Agreement.

21. Captions. The captions in this Agreement are for reference only and shall in no way define or interpret any provision hereof.

22. Counterparts. This Agreement may be executed in identical counterpart copies, each of which shall be an original, but all of which taken together shall constitute one and the same agreement.

23. Recording. Owner hereby consents and agrees to the recording by PG&E of this Agreement against the Property. Owner agrees to sign any additional documents reasonably required to complete such recording.

24. Ratification of Grant of Easement. Except as modified by this Agreement in regard to the Easement Area, all of the terms, conditions and provisions of the Grant of Easement shall remain in full force and effect and are hereby ratified and confirmed. To the extent the terms of the Grant of Easement are inconsistent with this Agreement, the terms of this Agreement shall control.



IN WITNESS WHEREOF, the parties have executed this Agreement as of the day and year first set forth above.

"PG&E"

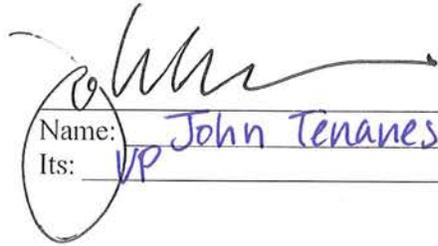
"Owner"

PACIFIC GAS AND ELECTRIC COMPANY,  
a California corporation

HIBISCUS PROPERTIES, LLC,  
a Delaware limited liability company

By: \_\_\_\_\_

Its:

  
Name: John Tenanes  
Its: VP



A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

State of California  
County of \_\_\_\_\_

On \_\_\_\_\_ before me, \_\_\_\_\_ (here insert name and title of officer), personally appeared \_\_\_\_\_, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct

WITNESS my hand and official seal.

Signature \_\_\_\_\_

(Seal)



A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

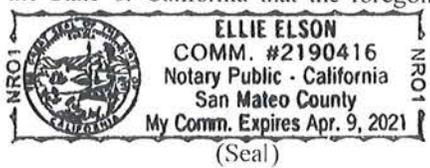
State of California  
County of San Mateo

On August 9, 2019 before me, Ellie Elson, Notary Public (here insert name and title of officer), personally appeared John Tenanes, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/~~she/they~~ executed the same in his/~~her/their~~ authorized capacity(ies), and that by his/~~her/their~~ signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct

WITNESS my hand and official seal.

Signature [Handwritten Signature]



# EXHIBIT "A"



## LEGAL DESCRIPTION LOT LINE ADJUSTMENT

### ADJUSTED PARCEL 2

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL 1 IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006489, AND ALL OF PARCEL A, AS SAID PARCEL A IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 13-01, RECORDED MAY 2, 2013 AS INSTRUMENT NO. 2013-066476, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT THE SOUTHEAST CORNER OF SAID PARCEL A, SAID CORNER BEING A POINT ON THE NORTHERLY LINE OF THE 100 FOOT WIDE SOUTHERN PACIFIC RAILROAD DUMBARTON CUT OFF RIGHT OF WAY;

THENCE ALONG THE SOUTHERLY LINE OF SAID PARCEL A AND SAID PARCEL 1, SOUTH 85° 08' 20" WEST, 3093.09 FEET;

THENCE ALONG THE SOUTHERLY AND WESTERLY LINES OF SAID PARCEL 1 THE FOLLOWING TWELVE (12) COURSES:

1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 50° 31' 12" WEST, HAVING A RADIUS OF 231.72 FEET, THROUGH A CENTRAL ANGLE OF 53° 54' 38" FOR AN ARC LENGTH OF 218.03 FEET,

2) SOUTH 85° 08' 20" WEST, 1370.12 FEET,

3) SOUTH 22° 32' 00" WEST, 42.84 FEET,

4) ALONG THE ARC OF A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 02° 53' 18" WEST, HAVING A RADIUS OF 335.00 FEET, THROUGH A CENTRAL ANGLE OF 112° 10' 00" FOR AN ARC LENGTH OF 655.82 FEET,

5) NORTH 19° 16' 42" EAST, 388.09 FEET,

6) ALONG THE ARC OF A CURVE THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 93° 36' 25" FOR AN ARC LENGTH OF 32.67 FEET,

7) SOUTH 67° 06' 53" EAST, 5.00 FEET,

8) NORTH 22° 53' 07" EAST, 30.00 FEET,

9) SOUTH 67° 06' 53" EAST, 5.63 FEET,

10) NORTH 22° 53' 07" EAST, 30.00 FEET,

11) NORTH 67° 06' 53" WEST, 5.00 FEET, AND

12) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 86° 23' 35" FOR AN ARC LENGTH OF 30.16 FEET;

THENCE SOUTH 65° 36' 59" EAST, 158.40 FEET;

THENCE SOUTH 54° 23' 26" EAST, 198.82 FEET;

THENCE NORTH 22° 56' 17" EAST, 393.03 FEET TO A POINT ON THE NORTHERLY LINE OF SAID PARCEL 1;

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THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL 1, SOUTH 81° 12' 00" EAST, 2132.40 FEET;

THENCE ALONG THE NORTHERLY LINES OF SAID PARCEL 1 AND SAID PARCEL A, NORTH 89° 21' 50" EAST, 823.77 FEET TO THE NORTHWEST CORNER OF PARCEL B, AS SAID PARCEL IS DESCRIBED IN SAID LOT LINE ADJUSTMENT NO. 13-01 (2013-066476);

THENCE ALONG THE NORTHERLY AND EASTERLY LINES OF SAID PARCEL A THE FOLLOWING FIFTEEN (15) COURSES:

- 1) SOUTH 31° 06' 06" EAST, 39.45 FEET,
- 2) SOUTH 0° 38' 10" EAST, 40.00 FEET,
- 3) NORTH 89° 21' 50" EAST, 60.00 FEET,
- 4) NORTH 0° 38' 10" WEST, 40.00 FEET
- 5) NORTH 29° 49' 46" EAST, 39.45 FEET,
- 6) NORTH 89° 21' 50" EAST, 79.91 FEET,
- 7) SOUTH 31° 06' 06" EAST, 39.45 FEET,
- 8) NORTH 89° 21' 50" EAST, 60.00 FEET,
- 9) NORTH 29° 49' 46" EAST, 39.45 FEET,
- 10) NORTH 89° 21' 50" EAST, 1012.83 FEET,
- 11) SOUTH 82° 24' 08" EAST, 162.24 FEET,
- 12) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 45.00 FEET, THROUGH A CENTRAL ANGLE OF 81° 18' 03" FOR AN ARC LENGTH OF 63.85 FEET,
- 13) SOUTH 01° 06' 05" EAST, 171.90 FEET,
- 14) SOUTH 14° 09' 17" WEST, 107.79 FEET, AND
- 15) SOUTH 22° 13' 35" WEST, 112.49 FEET TO THE POINT OF BEGINNING.

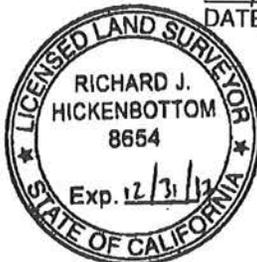
CONTAINING 77.71 ACRES OF LAND, MORE OR LESS.

AS SHOWN ON EXHIBIT "B", ATTACHED HERETO AND MADE A PART HEREOF.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.

  
 RICHARD J. HICKENBOTTOM, LS 8654  
 LICENSE EXPIRES: 12/31/17

2/16/17  
 DATE





## **Attachment 2**

**Environmental Approval**

## RESOLUTION NO. 6351

### **RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK ADOPTING FINDINGS REQUIRED BY THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, A STATEMENT OF OVERRIDING CONSIDERATIONS, A MITIGATION MONITORING AND REPORTING PROGRAM, AND CERTIFYING THE FINAL ENVIRONMENTAL IMPACT REPORT FOR THE FACEBOOK CAMPUS EXPANSION PROJECT LOCATED AT 301-309 CONSTITUTION DRIVE**

**WHEREAS**, Hibiscus Properties, LLC (“Project Sponsor”), an affiliate of Facebook, Inc., submitted an application to redevelop the property located at 301-309 Constitution Drive in the City of Menlo Park with two office buildings and a hotel, publicly accessible open space, and a bicycle and pedestrian bridge across the Bayfront Expressway (“Project”); and

**WHEREAS**, the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts and mitigation measures that, in the view of the City of Menlo Park (“City”), justify approval of the Project; and

**WHEREAS**, the City released a Notice of Preparation (“NOP”) for the Project on June 18, 2015 for a 30-day public review period. The City held a public scoping meeting on July 13, 2015 before the City Planning Commission to receive comments on the NOP prior to the close of the public review period. Comments received by the City on the NOP and at the public scoping meeting were considered during preparation of the Draft Environmental Impact Report (“EIR”); and

**WHEREAS**, the Draft EIR was released on May 26, 2016 for a 45-day review period that ended on July 11, 2016. The public review period included several public meetings, including meetings of the Bicycle, Transportation, Housing and Environmental Quality Commissions. On June 20, 2016, the City held a duly noticed Planning Commission hearing that was open to the public and provided the public an opportunity to comment on the Draft EIR. Comments on the Draft EIR were received from 10 public agencies, 8 organizations, and 43 individuals.

**WHEREAS**, the Draft EIR was filed with the California Office of Planning and Research and copies of the Draft EIR were made available at the Community Development Department, on the City’s website and at the Menlo Park Library; and

**WHEREAS**, on September 15, 2016, the City published a Response to Comments Document that contains all of the comments received during the public comment period, including a transcript of the public hearing, and written responses to those comments, prepared in accordance with CEQA and the CEQA Guidelines. The Draft EIR and Response to Comments Document constitute the Final EIR; and

**WHEREAS**, all required public notices and public hearings were duly given and held according to law; and

**WHEREAS**, after notice having been lawfully given, a duly noticed public hearing was held before the City Planning Commission on September 26, 2016 at which all persons interested had the opportunity to appear and comment and at which the Planning Commission considered and made recommendations to the City Council regarding the Final EIR and the merits of the Project; and

**WHEREAS**, the City Planning Commission having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter, voted affirmatively to recommend that the City Council certify the Final EIR, make the findings required by CEQA, adopt the Statement of Overriding Considerations, adopt the Mitigation Monitoring and Reporting Program, and approve the Project; and

**WHEREAS**, after notice having been lawfully given, a duly noticed public hearing was held before the City Council on November 1, 2016 at which all persons interested had the opportunity to appear and comment and at which the City Council considered the Final EIR and the merits of the Project; and

**WHEREAS**, the City Council has reviewed the Final EIR, all staff reports pertaining to the Final EIR, the Planning Commission hearing minutes and reports, and all evidence received by the City, including at the Planning Commission and at the City Council hearings and found that the Final EIR was prepared in compliance with CEQA; and

**WHEREAS**, after closing the public hearing, the City Council acting on its independent judgment and analysis voted affirmatively to certify the Final EIR pursuant to CEQA; and

**WHEREAS**, the City Council certifies that it has reviewed the comments received and responses thereto and finds that the Final EIR provides adequate, good faith and reasoned responses to the comments. Pursuant to Public Resources Code Section 21082.1(c)(3), the City also finds that the Final EIR reflects the City's independent judgment as the lead agency for the Project and is supported by substantial evidence; and

**WHEREAS**, the Final EIR identified certain significant and potentially significant adverse effects on the environment caused by the Project; and

**WHEREAS**, the City Council specifically finds that where more than one reason for approving the Project and rejecting alternatives is given in its findings or in the record, and where more than one reason is given for adopting the Statement of Overriding Considerations, the Council would have made its decision on the basis of any one of those reasons; and

**WHEREAS**, the City Council desires, in accordance with CEQA, to declare that, despite the potential for significant environmental effects that cannot be substantially lessened or avoided through the adoption of feasible mitigation measures or feasible

alternatives, there exist certain overriding economic, social, and other considerations for approving the project that the Council believes justify the occurrence of those impacts; and

**WHEREAS**, the City Council having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter, voted affirmatively to make the findings required by CEQA, adopt the Statement of Overriding Considerations, and adopt the Mitigation Monitoring and Reporting Program.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Menlo Park hereby certifies the Final EIR, makes the following findings with respect to the Project's significant effects on the environment as identified in the Final EIR, as required under Sections 15091, 15092, and 15093 of the CEQA Guidelines, and adopts the Mitigation Monitoring and Reporting Program ("MMRP"):

**I. CERTIFICATION OF THE FINAL ENVIRONMENTAL IMPACT REPORT**

In accordance with CEQA Guidelines Section 15090, the City of Menlo Park, acting by and through its City Council hereby certifies that the Final EIR has been completed in compliance with the CEQA and the CEQA Guidelines. The City further certifies that it has been presented with the Final EIR and that it has reviewed and considered the information contained in the Final EIR prior to approving the Project. The City further certifies that the Final EIR reflects its independent judgment and analysis.

**II. RECORD OF PROCEEDINGS**

For purposes of CEQA and these findings, the record of proceedings consists of the following documents and testimony:

- (a) The NOP and all other public notices issued by the City in conjunction with the Project;
- (b) All applications for approvals and development entitlements related to the Project and submitted to the City;
- (c) The Draft EIR for the Project, dated May 2016;
- (d) All comments submitted by agencies or members of the public on the Draft EIR;
- (e) The Final EIR for the Project, including comments received on the Draft EIR, responses to those comments, and the technical appendices, dated September 2016;
- (f) The MMRP for the Project;
- (h) All reports, studies, memoranda, maps, staff reports, or other planning documents related to the Project prepared by the City, or consultants to the City, or by the Applicant with respect to the City's compliance

with the requirements of CEQA and with respect to the City's action on the Project;

- (i) All documents submitted to the City (including the Planning Commission and City Council) by other public agencies or members of the public in connection with the Project;
- (j) Any minutes and/or verbatim transcripts of all information sessions, public meetings, and public hearings held by the City in connection with the Project;
- (k) All matters of common knowledge to the Planning Commission and City Council, including, but not limited to:
  - (i) City's General Plan and other applicable policies;
  - (ii) City's Zoning Ordinance and other applicable ordinances;
  - (iii) Information regarding the City's fiscal status;
  - (iv) Applicable City policies and regulations; and
  - (v) Federal, state and local laws and regulations.
- (l) Any other materials required for the record of proceedings by CEQA Section 21167.6(e).

The documents described above comprising the record of proceedings are located in the Community Development Department, City of Menlo Park, 701 Laurel Street, Menlo Park, California 94025. The custodian of these documents is the City's Community Development Director or his/her designee.

### **III. FINDINGS FOR SIGNIFICANT IMPACTS AVOIDED OR MITIGATED TO A LESS-THAN-SIGNIFICANT LEVEL**

The Final EIR concluded that the Project would result in potentially significant environmental impacts. The City finds that by incorporating into the Project all the feasible mitigation measures outlined in the MMRP, the impacts discussed below would be reduced to a less than significant level.

#### **A. AESTHETICS**

**Impact AES-3: New Sources of Light and Glare.** The Project could create a new source of substantial light or glare that could adversely affect daytime or nighttime views in the area.

Mitigation Measure AES-3.1: Design Lighting to Meet Minimum Safety and Security Standards. Concurrent with the building permit submittal, the Project Sponsor shall incorporate lighting design specifications to meet minimum safety and security standards. The comprehensive site lighting plans shall be subject to review and

approval by the City's Community Development Department, Planning Division, prior to building permit issuance for the first building on the site.

The following measures shall be included in all lighting plans.

- Luminaries shall be designed with cutoff-type fixtures or features that cast low-angle illumination to minimize incidental spillover of light onto adjacent private properties. Fixtures that shine light upward or horizontally shall not spill any light onto adjacent private properties.
- Luminaries shall provide accurate color rendering and natural light qualities. Low-pressure sodium and high-pressure sodium fixtures that are not color-corrected shall not be used, except as part of an approved sign or landscape plan.
- Luminary mountings shall be downcast and pole heights minimized to reduce the potential for backscatter into the nighttime sky and incidental spillover onto adjacent properties and undeveloped open space. Light poles shall be no higher than 20 feet. Luminary mountings shall be treated with non-glare finishes.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the lighting designs are feasible and would reduce potential light spillage impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to light spillage would not be significant.

Mitigation Measure AES-3.2: Treat Reflective Surfaces. The Project Sponsor shall ensure the application of a low-emissivity coating on exterior glass surfaces of proposed structures. The low-emissivity coating shall reduce the reflection of visible light that strikes the exterior glass and prevent interior light from being emitted brightly through the glass.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the anti-reflection designs are feasible and would reduce light reflection and glare impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to light reflection and glare would not be significant.

## B. TRANSPORTATION

**Impact TRA-1:** Impacts on Peak-Hour Traffic at Study Intersections under Background Plus-Project Conditions. Increases in traffic associated with the Project would result in increased delays during peak hour, causing potentially significant impacts on the operation of specific study intersections identified below.<sup>1</sup>

Mitigation Measure TRA-1.1: Provide Increased Traffic Capacity on Peak-Hour LOS under Background-Plus Project Conditions. Mitigation Measure TRA-1.1 incorporates the analysis of potential impacts on intersection identifies potential measures to mitigate or reduce Project impacts where feasible.

Mitigation Measure TRA-1.2. Reduce the Peak-Hour Share of Vehicle Trips Allowable under the Trip Cap, for both the Project Site and Buildings 10-19 to no more than 50 Percent of Allowable Vehicle Trips During each 2 Hour Peak Commute Period. The Project trip cap, as proposed, would allow up to 69 percent of vehicle trips within each 2-hour peak commute period to enter or exit the site within a single peak hour. Similarly, the approved vehicle trip caps for Buildings 10-20 currently allow up to 70 percent of permitted vehicle trips within each 2-hour peak commute period to occur within a single hour. The proposed mitigation would reduce the maximum number of allowable peak-hour vehicle trips to no more than 50 percent of the 2-hour peak-period vehicle trip cap for both the Project site and Buildings 10-19. Table 3.3-7 provides a comparison of the potential peak 1-hour vehicle trips under the Project and with the proposed mitigation. As shown, the proposed mitigation would reduce the total volume of allowed peak-hour vehicle trips to the Project site and Buildings 10-19 by 28 percent.

(a) *Sand Hill Road & I-280 Northbound On-Ramp (#2)*

During the p.m. peak hour, the intersection of Sand Hill Road and the I-280 northbound on-ramp (study intersection #2) operates unacceptably at LOS E under existing and background conditions, reflecting the delay experienced by westbound vehicles when approaching I-280. Traffic associated with the Project would increase average delay to approximately 19 seconds, exceeding the impact threshold of 4 seconds for Caltrans intersections. The increased delay could be mitigated by modifying signal timing during the p.m. peak hour to increase the allocation of green time to the westbound approach (by up to 10 seconds during the p.m. peak hour). However, as described below, this mitigation would not be necessary under background plus-Project conditions because Mitigation Measure TRA-1.2 (discussed below) would modify the Project trip cap to limit the number of vehicle trips that could occur during a single peak hour (see Mitigation Measure TRA 1.2).

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<sup>1</sup> This section discusses impacts on those study intersections that are avoided or reduced to less-than-significant levels with implementation of feasible mitigation. Impacts on study intersections that are found to be significant and unavoidable are addressed in Section IV, below. Specific study intersection impacts are identified in the Final EIR by letter (i.e., (a), (b), (c), etc.); that same format is carried forward in these Findings for consistency.

With implementation of Mitigation Measure TRA-1.2, the net increase in the number of vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. Therefore, with Mitigation Measure TRA-1.2, the change in delay would not be anticipated to exceed 4 seconds, and the impact would be reduced to a less-than-significant level.

(e) *Bayfront Expressway & Chilco Street (#40)*

During the p.m. peak hour, the potential impact reflects increased eastbound delay on Bayfront Expressway where traffic approaches the Dumbarton Bridge due to an increase in conflicting northbound movements at Chilco Street under background plus-Project conditions. The analysis assumes that two left-turn lanes and a separate right-turn lane would be provided as planned and funded improvements. However, the intersection would be anticipated to continue to operate at an unacceptable LOS of E.

The provision of one additional eastbound lane (for a total of four through lanes) on Bayfront Expressway would mitigate the intersection impact but would not improve net vehicle delay at the approach to the Dumbarton Bridge unless accompanied by measures to reduce downstream delay. The mitigation is not be feasible given the downstream lane configurations and environmental constraints, including those related to the wetlands and marsh area north of Bayfront Expressway.

With implementation of Mitigation Measure TRA-1.2 (described above), the net increase in the number of vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. This intersection would be anticipated to operate acceptably at LOS D. Therefore, with Mitigation Measure TRA-1.2, the impact would be reduced to a less-than-significant level.

(f) *Chilco Street & Constitution Drive (#45)*

During the a.m. and p.m. peak hours, traffic volumes at this all-way stop-controlled intersection would increase significantly, because this intersection would serve as one of the two vehicle access points to the Project site. The intersection operates acceptably under existing conditions during both the a.m. and p.m. peak hours. The intersection would continue operating acceptably under background conditions during the a.m. peak hour but would operate unacceptably at LOS F during the p.m. peak hour, reflecting increased traffic with full occupancy of Building 23 under background conditions in combination with the additional traffic that would be generated by approved projects in the Bayfront (formerly M-2) area.

The Project would provide motor vehicle access to proposed Building 22, existing Building 23, and the proposed hotel via the Chilco Street and Constitution Drive intersection; direct motor vehicle access to proposed Building 21 would be provided from a proposed signalized intersection on Bayfront Expressway. Approximately 58

percent of the proposed parking supply would be accessed from the Chilco Street and Constitution Drive intersection. No changes to lane configurations or intersection control at the Chilco Street and Constitution Drive intersection are proposed as part of the Project. The Project would result in LOS F during the a.m. peak hour under background plus-Project conditions; southbound vehicle queues on Chilco Street, at the approach to the Project entrance, would extend onto Bayfront Expressway. During the p.m. peak hour, vehicles would experience significant delay when exiting the Project site; the delay would exceed the impact threshold. Although queuing at the intersection of Bayfront Expressway and Chilco Street in the a.m. peak hour is not considered an impact, based on the City's LOS criteria, it is a safety concern for the site. The improvements identified below were designed to mitigate this impact.

The proposed mitigation for peak-hour impacts at the intersection of Chilco Street and Constitution Drive would provide the following elements to accommodate inbound a.m. and outbound p.m. traffic movements:

- Installation of a traffic signal and signalized pedestrian crossings on all four legs of the intersection.
- Provision of three southbound lanes on the one-block segment of Chilco Street, between Bayfront Expressway and Constitution Drive, to include two southbound left-turn lanes to accommodate the volume of left-turning vehicles entering the Project site. In addition, during the a.m. peak hour, provision of a "split-phase" signal operation on Chilco Street is recommended.
- Provision of a northbound left-turn lane on Chilco Street approaching Constitution Drive.
- Provision of two outbound lanes on Chilco Street exiting the Project site.

With implementation of this mitigation measure, the intersection would operate acceptably at LOS D during both peak hours. Bicycle lanes should be accommodated in the proposed improvements on Chilco Street, tying into the proposed improvements the Project Sponsor is constructing on Chilco Street as a separate project, and on Constitution Drive. These improvements are required to be operational prior to Building 22 occupancy. With these improvements, this impact would be reduced to a less-than-significant level.

*(i) University Avenue & US 101 Southbound Ramps (#56)*

During the a.m. peak hour, the intersection operates acceptably at LOS C under existing conditions and LOS D under background conditions. The addition of Project traffic would result in an unacceptable LOS of E during the a.m. peak hour at this Caltrans-controlled intersection in East Palo Alto. During the p.m. peak hour, the intersection currently operates unacceptably at LOS E under existing conditions; the net change under background plus-Project conditions would not exceed the 4-second threshold. Therefore this impact would be less than significant during the p.m. peak hour but potentially significant during the a.m. peak hour.

Mitigation Measure TRA-1.2 (described above) would reduce allowable net Project vehicle trip generation by more than 75 percent during the a.m. peak hour. This intersection would be anticipated to operate acceptably at LOS D during the a.m. peak hour. Therefore, with Mitigation Measure TRA-1.2, the impact would be reduced to a less-than-significant level.

(j) *University Avenue & Woodland Avenue (#57)*

During the a.m. peak hour, this intersection operates at LOS F under existing and background conditions. Under background plus-Project conditions, the increase in a.m. peak-hour delay compared with background conditions would exceed the applicable impact threshold for East Palo Alto intersections that operate at LOS F (i.e., delay to critical movements increases by more than 4 seconds and the critical v/c ratio increases by 0.01).

Provision of a dedicated right-turn lane on the westbound approach leg from Woodland Avenue would mitigate the impact. However, this potential mitigation is not recommended because it would encourage cut-through traffic via Woodland Avenue, potentially affecting the Willows neighborhood in Menlo Park and Woodland neighborhood in East Palo Alto. To avoid facilitating use of Woodland Avenue as a cut-through route, this potential mitigation is not recommended, consistent with City policies that discourage cut-through traffic in residential neighborhoods. In addition, because the intersection is not within the city of Menlo Park, implementation of this potential mitigation cannot be guaranteed.

Mitigation Measure TRA-1.2 (described above) would reduce the allowable net Project vehicle trip generation by more than 75 percent during the a.m. peak hour; the net change in delay to critical movements would not exceed the thresholds described above. Therefore, with Mitigation Measure TRA-1.2, the impact would be reduced to a less-than-significant level.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* For each of the intersections identified above, changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that mitigation measures described above in connection with each identified intersection, in addition to Mitigation Measure TRA-1.2, which would require the Project Sponsor to spread trips equally between the peak hours and apply the same requirement to its existing Campus buildings, are feasible and would reduce impacts on these study intersections to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to the identified study intersections would not be significant.

**Impact TRA-4:** Pedestrian Connections Under Background Plus-Project Conditions. The Project would result in a lack of adequate pedestrian connections to the area circulation system under background plus-Project conditions.

Mitigation Measure TRA-4.1: Provide External Pedestrian Connections to the Area Circulation System and Adjacent Land Uses. The proposed mitigation would include providing and/or contributing to the cost of pedestrian improvements to eliminate gaps in the sidewalk network in key areas that provide access routes to and from the Project site. The improvements outlined below were selected to provide an immediate connection to the Project site.

a. Constitution Drive

The Project Sponsor shall construct sidewalks along one side of Constitution Drive between Chilco Street and Chrysler Drive and pedestrian crosswalks and curb ramps at Chilco Street & Constitution Drive and Jefferson Drive & Constitution Drive. Construction of a sidewalk and crossing improvements along this section of Constitution Drive, in conjunction with other planned and funded sidewalk construction in the area, will provide continuous pedestrian access from the Project site throughout the Bayfront Area (formerly M-2 area).

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the external pedestrian connections/improvements are feasible and would reduce impacts to pedestrians to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to pedestrian connections would not be significant.

**Impact TRA-5:** Bicycle Connections Under Background Plus-Project Conditions. The Project would result in a lack of adequate bicycle connections to the area circulation system under background plus-Project conditions, resulting in potentially significant impacts.

Mitigation Measure TRA-5.1: Provide bicycle connections to the area circulation system and adjacent land uses. The recommended mitigation would include providing and/or contributing to the cost of bicycle improvements to eliminate gaps in the bicycle network that are likely to be used as key access routes to the Project site, including bicycle connections to and from the Menlo Park Caltrain station.

a. Hamilton Avenue

The Project Sponsor shall install bicycle boulevard treatments on Hamilton Avenue between Chilco Street and the pedestrian/bicycle overcrossing of US 101. Bicycle boulevards generally include treatments to facilitate travel by bicyclists. Typical treatments generally include stop-sign modifications, lane markings, signage, and wayfinding elements. This designation is consistent with the street classification proposed in the ConnectMenlo draft Circulation Element.

b. Northbound Access to the Project Site for Bicyclists

The Project Sponsor shall provide facilities for northbound bicyclists to cross Willow Road and access the Project site, thereby minimizing vehicle/bicycle conflicts. Such facilities may include a two-stage left-turn queue box, or similar improvements, to accommodate northbound left-turn movements for bicyclists at the Willow Road/Hamilton Drive intersection from the curbside bicycle lane, in conjunction with a Class I pathway or similar improvements for northbound bicyclists to the travel on the west side of Willow Road between Hamilton Avenue and the Project site.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the bicycle improvements are feasible and would reduce impacts to bicycle connections to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to bicycle connections would not be significant.

**Impact TRA-6:** Pedestrian and/or Bicycle/Vehicle Conflicts. The Project design would cause increased potential for pedestrian and/or bicycle/vehicle conflicts, resulting in potentially significant impacts.

**Mitigation Measure TRA-6.1:** Refine the Project Design to Minimize Conflicting Movements between Bicycles, Pedestrians, and Other Travel Modes within the Project Site. The design for bicycle and pedestrian crossings, similar to the design at the Building 20 driveway, should direct bicycle and pedestrian traffic to the signalized intersection at Bayfront Expressway to avoid conflicts with motor vehicles and shuttle buses at uncontrolled crossings. The Project Sponsor shall work to minimize conflicts to the satisfaction of the transportation manager prior to approval of the site plan for construction.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant

environmental effect as identified in the EIR. The City finds that the design for bicycle and pedestrian crossings are feasible and would reduce pedestrian and/or bicycle/vehicle conflicts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to pedestrian and/or bicycle/vehicle conflicts would not be significant.

**Impact TRA-10:** Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Existing General Plan Plus-Project Conditions. Increases in peak-hour vehicle traffic associated with the Project would result in increased delays during AM and PM peak hours causing significant and unavoidable impacts on the operation of study intersections under Cumulative 2040 Conditions with the Existing General Plan.

Mitigation Measure TRA-10.1: Provide Increased Traffic Capacity to Address Project Impacts on Peak-Hour LOS under Cumulative 2040 Existing General Plan plus-Project Conditions. Mitigation Measure TRA 10.1 identifies potential measures to mitigate or reduce Project impacts where feasible.

(a) *Sand Hill Road and I-280 Northbound Off-Ramp (#1)*

During the a.m. peak hour, the eastbound intersection of Sand Hill Road and the I-280 Northbound Off-Ramp (study intersection #1) operates at LOS D under Existing Conditions, would operate unacceptably at LOS E under Cumulative 2040 Existing General Plan Conditions without the Project, and would degrade further to LOS F with the addition of Project trips, reflecting traffic delay exiting the I-280 northbound freeway.

With implementation of Mitigation Measure TRA-1.2, the net increase in peak hour vehicle trips resulting from the Project during the a.m. peak hour is reduced by more than 75 percent. With Mitigation Measure TRA-1.2 the intersection would operate at LOS E and the net change in delay resulting from the Project would be reduced to less than 4 seconds. Therefore, with Mitigation Measure TRA 1.2 the Project contribution to the impact at this location under 2040 existing General Plan plus-Project conditions would be reduced to a less-than-significant level.

(c) *El Camino Real & Ravenswood Avenue-Menlo Avenue (#28)*

During the a.m. peak hour, traffic associated with the Project would result in an unacceptable LOS under cumulative 2040 existing General Plan plus-Project conditions. Potential mitigation would be to provide a right-turn pocket on Menlo Avenue, where it approaches El Camino Real, and a third through lane on El Camino Real in both the northbound and southbound directions. These measures are identified in the City's TIF program. The Project Sponsor's payment of the TIF shall mitigate this impact. With implementation of this mitigation measure, the intersection would operate acceptably, and the impact would be reduced to a less-than-significant level.

(g) *Chilco Street & Constitution Drive (#45)*

This impact, identified under background plus-Project conditions, pertains to the design of the Project entrance, as described above in Mitigation Measure TRA-1.1f. With implementation of this mitigation measure, the intersection would operate acceptably, and this impact would be reduced to a less-than-significant level.

(h) *Chrysler Drive & Constitution Drive (#46)*

During the p.m. peak hour, the intersection of Chrysler Drive and Constitution Drive (study intersection #46) operates acceptably at LOS C under cumulative 2040 existing General Plan conditions without the Project. Traffic associated with the Project would cause LOS to degrade to an unacceptable LOS of D during the p.m. peak hour under cumulative 2040 existing General Plan plus-Project conditions.

With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent, and the intersection would operate acceptably at LOS C. Therefore, with Mitigation Measure TRA-1.2, the Project impact at this location under 2040 existing General Plan plus-Project conditions would be reduced to a less-than-significant level.

(i) *University Avenue & Adams Drive (#47)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during both the a.m. and p.m. peak hours (see Mitigation Measure TRA-1.1g). This impact would remain significant and unavoidable under existing General Plan plus-Project conditions. (SU)

Installation of a traffic signal at this location would be recommended under 2040 cumulative conditions with the proposed General Plan. Therefore, if the proposed General Plan is adopted, this impact could be mitigated to a less-than-significant level (see Mitigation Measure TRA-13.1i).

(j) *University Avenue & Bay Road (#51)*

The Project was identified to have a potential impact during the p.m. peak hour under cumulative 2040 existing General Plan plus-Project conditions. With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. With Mitigation Measure TRA-1.2, the change in delay would not be anticipated to exceed 4 seconds, and the impact would be reduced to a less-than-significant level.

(l) *University Avenue & US 101 Southbound Ramps (#56)*

Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering a potential impact during the p.m. peak hour.

With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. Therefore, with Mitigation Measure TRA-1.2, the change in delay would not be anticipated to exceed 4 seconds, and the impact would be reduced to a less-than-significant level.

(m) *University Avenue & Woodland Avenue (#57)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during the p.m. peak hour.

With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. Therefore, with Mitigation Measure TRA-1.2, the change in delay would not be anticipated to exceed 4 seconds, and the impact would be reduced to a less-than-significant level.

In addition, implement Mitigation Measure TRA-1.2, above.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* For each of the intersections identified above, changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that mitigation measures described above in connection with each identified intersection, in addition to Mitigation Measure TRA-1.2, which would require the Project Sponsor to spread trips equally between the peak hours and apply the same requirement to its existing Campus buildings, are feasible and would reduce impacts on these study intersections under cumulative 2040 existing General Plan plus-Project conditions to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to the identified study intersections would not be significant.

**Impact TRA-13:** Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Proposed General Plan Conditions. Increases in peak-hour vehicle traffic associated with the Project would contribute to increased delays during the a.m. and p.m. peak hours in 2040 under the proposed General Plan (ConnectMenlo), causing a significant and unavoidable impact on the operation of study intersections.

Mitigation Measure TRA-13.1: Increase Traffic Capacity to Address Impacts on Peak-Hour LOS under Cumulative 2040 Proposed General Plan Conditions. This measure describes the types of mitigation measures that would be necessary to mitigate impacts at each affected location to less than significant.

(a) *Sand Hill Road & I-280 Northbound Off-ramp (#1)*

This a.m. peak-hour impact was identified under cumulative 2040 existing General Plan plus-Project conditions (see Impact TRA-10.1a) and mitigated to less-than-significant levels with the peak-hour trip reduction described under Mitigation Measure TRA-1.2. Average delay would change by less than 1 second under the proposed General Plan, and impact findings would remain consistent with cumulative 2040 General Plan plus-Project conditions. The Project impact would, therefore, remain less than significant with Mitigation Measure TRA-1.2 under cumulative 2040 proposed General Plan conditions.

(b) *El Camino Real & Ravenswood Avenue-Menlo Avenue (#28)*

The intersection would operate unacceptably during both the a.m. and p.m. peak hours under cumulative 2040 proposed General Plan conditions. As described above under Mitigation Measure TRA 10.1c, the provision of a right-turn pocket on Menlo Avenue, where it approaches El Camino Real, and a third through lane on El Camino Real is identified in the City's TIF program. The Project Sponsor's payment of the TIF shall mitigate this impact to a less-than-significant level.

(f) *Chrysler Street & Constitution Drive (#45)*

This impact, also identified under background plus-Project conditions, pertains to the design of the Project entrance (see Mitigation Measure TRA-1.1). With implementation of this Project mitigation measure, the intersection would operate acceptably and this impact would be reduced to a less-than-significant level.

(g) *Chilco Street & Constitution Drive (#46)*

This impact was also identified under cumulative 2040 existing General Plan plus-Project conditions. With implementation of Mitigation Measure TRA-1.2, the net increase in peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent and the Project contribution to increased delay would be less than 4 seconds. Therefore, with Mitigation Measure TRA-1.2, the Project impact at this location under 2040 proposed General Plan conditions would be reduced to a less-than-significant level.

(h) *University Avenue & Adams Drive (#47)*

LOS at this intersection reflects delay on the side-street stop-controlled approach from Adams Drive. Signalization of this intersection would be warranted under

cumulative 2040 proposed General Plan conditions with buildout of ConnectMenlo, including the Project. Therefore, signalization of this intersection should be included in the City's TIF program. The Project Sponsor's payment of the TIF shall mitigate this impact, and the impact would be less than significant.

(i) *University Avenue & Bay Road (#51)*

The intersection operates at LOS F during the p.m. peak hour under existing conditions, reflecting primarily northbound traffic as it approaches the Dumbarton Bridge.

Increased delay would exceed the significance threshold under cumulative 2040 proposed General Plan conditions, reflecting added traffic to/from the other development sites (west of University Avenue and east of Willow Road) identified under the proposed General Plan. Replacement of the east/west "split-phase" signal on Bay Street with standard protected signal phases would allow eastbound and westbound pedestrian crossings to occur simultaneously and reduce p.m. peak-hour delay at this intersection. Because the intersection is not under the control of the City of Menlo Park, implementation of potential mitigation to reduce peak-hour delay at this location cannot be guaranteed.

Project traffic would occur primarily in the reverse-peak direction (southbound) during the p.m. peak hour. In addition, Mitigation Measure TRA-1.2 would reduce the net increase in the number of p.m. peak-hour vehicle trips generated by the Project by approximately 90 percent. Therefore, the Project would not result in increased p.m. peak-hour delay that would exceed the impact threshold under background plus-Project or cumulative 2040 existing General Plan plus-Project conditions.

Under cumulative 2040 proposed General Plan conditions with Mitigation Measure TRA 1.2, the Project would not be anticipated to result in additional delay to critical movements that would exceed 4 seconds, and Project trips would not result in the critical v/c ratio exceeding the impact threshold. The Project contribution to this cumulative impact would be less than significant with Mitigation Measure TRA-1.2.

(k) *University Avenue & US 101 Southbound Ramps (#56)*

During the p.m. peak hour, this intersection operates unacceptably at LOS E under existing conditions; it would remain at LOS E under background plus-Project and cumulative 2040 existing General Plan plus-Project conditions. With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project during the p.m. peak hour would be reduced by more than 90 percent. Therefore, with Mitigation Measure TRA-1.2, the intersection would be anticipated to operate at LOS E, consistent with existing conditions. The Project contribution to this cumulative impact would be less than significant with Mitigation Measure TRA-1.2.

In addition, implement Mitigation Measure TRA-1.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* For each of the intersections identified above, changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that mitigation measures described above in connection with each identified intersection, in addition to Mitigation Measure TRA-1.2, which would require the Project Sponsor to spread trips equally between the peak hours and apply the same requirement to its existing Campus buildings, are feasible and would reduce impacts on these study intersections under cumulative 2040 Proposed General Plan plus Project conditions to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to the identified study intersections would not be significant.

### C. AIR QUALITY

**Impact AQ-2a:** Construction Criteria Air Pollutant Emissions. Construction activities at the Project site could result in the generation of regional criteria pollutant emissions during construction in excess of BAAQMD thresholds.

Mitigation Measure AQ-2.1: Implement BAAQMD Basic Construction Mitigation Measures to Reduce Construction-Related Dust. The Project Sponsor shall require all construction contractors to implement the basic construction mitigation measures recommended by BAAQMD to reduce fugitive dust emissions. Emission reduction measures shall include, at a minimum, the following measures. Additional measures may be identified by BAAQMD or contractor as appropriate.

- All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day. Recycled water, to be purchased through advance arrangement with the City of Redwood City or the City of Palo Alto, shall be used to water all exposed surfaces.
- All haul trucks transporting soil, sand, or other loose material offsite shall be covered.
- All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
- All vehicle speeds on unpaved roads shall be limited to 15 mph.
- All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- A publicly visible sign shall be posted with the telephone number and name of the person to contact at the lead agency regarding dust complaints. This person

shall respond and take corrective action within 48 hours. BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that dust control measures are feasible and would ensure that air emissions during construction impacts remain at a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction air emissions would not be significant.

**Impact AQ-2b:** Operational Criteria Air Pollutant Emissions. Operational activities at the Project site could result in the generation of regional criteria pollutant emissions during operation in excess of BAAQMD thresholds.

Mitigation Measure AQ-2.2: Offset NOx Emissions Generated during Project Operation that are above the BAAQMD NOx Average Daily Emission Threshold. The Project Sponsor shall, prior to occupancy of the first building within the Project, enter into an agreement with the City to develop an alternative or complementary offsite mitigation program to offset operational NOx emissions to the level established by the BAAQMD thresholds for the years in which the Project's emissions exceed the BAAQMD threshold. The offsite mitigation program shall require Project Sponsor to provide a onetime payment to the City to establish a program to fund emission reduction projects through grants and similar mechanisms within the City of Menlo Park. The amount of such payment shall be calculated based on then-current BAAQMD Carl Moyer Program cost-effectiveness limit multiplied by the emissions that exceed BAAQMD's average daily threshold for each year that emissions exceed the threshold plus a five percent administrative fee to fund procurement of offsite emission reductions for the Project's projected operational emissions.

Potential projects shall be limited to those which will reduce emissions for each year in which the project's emissions exceed the BAAQMD threshold through the end of 2025, which is when the Project's operational emissions are projected to be below the average daily thresholds, including, but not limited to, the following:

- Alternative fuel, low-emission school buses, transit buses, and other vehicles.
- Diesel engine retrofits and repowers.
- Bike Sharing Programs.
- Electric vehicle charging stations and plug-ins

All offsite reductions must be quantifiable, verifiable, and enforceable. The Project Sponsor shall engage a qualified air quality expert to coordinate with the City to identify a list of potential projects eligible for funding. Emission reduction projects shall be funded so that the Project's emissions are reduced each year until the end of 2025. The air quality expert retained by the Project Sponsor shall provide a report within one year of occupancy of the first building within the Project identifying the projects that were funded and associated NOx emissions expected to be realized for each year out until the end of 2025. Annual reporting of the implementation of emissions reduction projects shall be required until the Project's emissions are less than the BAAQMD threshold without the offsets.

If a sufficient number of emissions reduction projects are not identified to meet the required performance standards in the City of Menlo Park, Project Sponsor shall consult with a qualified air quality expert to ensure conformity is met through some other means of achieving the performance standard of achieving net zero operational emissions in excess of BAAQMD's average daily thresholds through 2025, including (but not limited to) payment of a one-time mitigation offset fee to BAAQMD's Strategic Incentives Division plus a five percent administrative fee to fund one or more emissions reduction projects within the San Francisco Bay Area Air Basin. Reporting for any emissions reduction projects outside the City shall be completed on the same schedule as indicated above for emission reduction projects in the City.

If annual reports indicate that emission reductions do not adequately reduce project emissions to a level below the BAAQMD threshold for any year, then a penalty of 200 percent shall be imposed that will require the Project Sponsor to obtain an additional year of offsets based on the amount of emissions by which the Project's emissions exceed the BAAQMD threshold for the next following year (e.g., if the 2019 emissions exceed the threshold by five tons, then 10 tons of emissions must be provided by 2020).

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that offsetting NOx emissions are feasible and would ensure that air emissions during Project operation remain at a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction air emissions would not be significant.

**Impact AQ-3:** Result in a Cumulatively Considerable Net Increase of any Criteria Pollutant for which the Project Region is Nonattainment. The Project could result in the generation of criteria pollutant emissions that would result in a cumulatively considerable net increase.

Mitigation Measures: Implement Mitigation Measures AQ-2.1 and AQ-2.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that offsetting criteria pollutant emissions are feasible and would ensure that cumulative air emissions during Project operation remain at a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to cumulative air emissions would not be significant.

**Impact AQ-4a:** Exposure of Existing Sensitive Receptors to Substantial Pollutant Concentrations During Construction. The Project would expose existing sensitive receptors to substantial pollutant concentrations during construction.

Mitigation Measures: Implement Mitigation Measure AQ-2.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that dust control measures are feasible and would ensure that air emissions during construction impacts remain at a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction air emissions would not be significant.

**Impact C-AQ-2b:** Cumulative Criteria Pollutants during Operation. Operational activities associated with the Project could generate substantial ROG, NOX, and PM10 emissions in excess of BAAQMD thresholds.

Mitigation Measures: Implement Mitigation Measure AQ-2.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that offsetting cumulative NOx emissions are feasible and would ensure that air emissions during Project operation remain at a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction air emissions would not be significant.

#### **D. GREENHOUSE GAS EMISSIONS**

**Impact GHG-1:** Greenhouse Gas Emissions. The Project would not generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment.

Mitigation Measure GHG-1.1: Implement BAAQMD Best Management Practices for Construction. The Project Sponsor shall require all construction contractors to implement the BMPs recommended by BAAQMD to reduce GHG emissions. Emissions reduction measures shall include, at a minimum, the use of local building materials (at least 10 percent), the recycling and reuse of at least 50 percent of construction waste or demolition material, and the use of alternative-fuel vehicles for construction vehicles/equipment (at least 15 percent of the fleet).

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that BAAQMD BMPs are feasible and would reduce potential greenhouse gas impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to greenhouse gas emissions would not be significant.

#### **E. NOISE**

**Impact NOI-1:** Exposure to Excessive Noise Levels. The Project could expose persons to or generate noise levels in excess of standards established in the General Plan, noise ordinance or applicable standards of other agencies.

Mitigation Measure NOI-1.1: Implement Noise Control Measures to Reduce Construction Noise during Project Construction. The Project Sponsor shall submit a Construction Noise Plan for review and approval by the Planning and Building Divisions prior to the issuance of the demolition permit. The Project Sponsor shall comply with construction noise limits specified in Section 8.06 of the City of Menlo Park Municipal Code by implementing measures during demolition and construction of the Project. These measures may include, but are not limited to:

- To the extent feasible, schedule the noisiest construction activities, such as demolition and grading activities, during times that would have the least impact on nearby residential and other receptors. This could include restricting construction activities in the areas of potential impact to the early and late hours

of the workday, such as from 8:00 a.m. to 10:00 a.m. or 4:00 p.m. to 6:00 p.m., Monday through Friday.

- Use best available noise control techniques (e.g., improved mufflers, equipment redesign, intake silencers, ducts, engine enclosures, acoustically attenuating shields or shrouds) on equipment and trucks used for Project construction wherever feasible.
- Use hydraulically or electrically powered impact tools (e.g., pile drivers, jack hammers, pavement breakers, rock drills) used for Project construction wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, use an exhaust muffler on the compressed air exhaust; this muffler can lower noise levels from the exhaust by up to about 10 dBA. Use external jackets on the tools themselves where feasible. This could achieve a reduction of 5 dBA. Use quieter equipment, such as drills, rather than impact equipment whenever feasible.
- Use “quiet” gasoline-powered compressors or electric compressors, and use electric rather than gasoline- or diesel-powered forklifts for small lifting to the extent feasible.
- Locate stationary noise sources, such as temporary generators, as far from nearby receptors as possible; such sources shall be muffled and enclosed within temporary enclosures and shielded by barriers or other measures to the extent feasible.
- Install temporary noise barriers (generally approximately 8 feet in height) around construction areas adjacent to sensitive receptors to reduce construction noise from equipment to acceptable levels. Specifically, the noise barriers shall reduce noise levels during the hours of 8:00 a.m. to 6:00 p.m. on weekdays to 85 dBA at a distance of 50 feet from the construction equipment. In addition, the noise barriers shall reduce overall construction noise to less than 60 dBA Leq, as measured at the applicable property lines of adjacent uses, during the hours of 7:00 a.m. to 8:00 a.m. and 6:00 p.m. to 10:00 p.m. weekdays and 7:00 a.m. to 10:00 p.m. on Saturdays. The noise barriers shall be installed unless an acoustical engineer submits documentation that confirms that barriers are not necessary to achieve these attenuation levels or provides specific locations and heights to achieve the required attenuation.
- Prohibit trucks from idling along streets serving the construction site.
- Prior to any pile-driving activities, notify all surrounding property owners and occupants within 300 feet of the Project site, informing them of the estimated start date and duration.
- Implement “quiet” pile-driving technology (e.g., vibratory pile driving or pre-drilled pile holes) where feasible, in consideration of geotechnical and structural requirements and conditions.
- Monitor the effectiveness of noise attenuation measures by taking noise measurements during pile-driving activities to ensure compliance with the 85 dBA standard at 50 feet for construction equipment and during general

construction occurring during non-exempted daytime hours to ensure compliance with the 60 dBA Leq daytime standard.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the noise control measures are feasible and would reduce potential construction equipment noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction equipment noise would not be significant.

Mitigation Measure NOI-1.2: Implement Noise Control Measures to Reduce HVAC Noise during Project Operation. The Project Sponsor shall design the Project HVAC system to limit noise to the applicable standard at the property line of nearby noise-sensitive receptors. Measures that can be implemented to achieve this include, but are not limited to:

- Maximize the distance between HVAC systems and nearby sensitive receptors,
- Provide enclosures around the HVAC units,
- Incorporate local barriers around equipment, and
- Utilize mufflers or silencers on HVAC systems.

Prior to the issuance of building permits, the Project Sponsor shall prepare a report, identifying measures that will be implemented to ensure that exterior HVAC noise levels will comply with the following noise limits:

- The 60 dBA Leq daytime and 50 dBA Leq nighttime noise standards for equipment located on the ground,
- The zoning ordinance limit of 50 dBA at a distance of 50 feet for roof-mounted equipment.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the noise control measures to reduce HVAC noise during Project operation are feasible and would reduce potential operational noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to HVAC noise would not be significant.

Mitigation Measure NOI-1.3: Install Sound Enclosures around Emergency Generators. The Project Sponsor shall reduce the sound level from the operating generators to a maximum sound level of less than the 60 dBA noise standard at nearby noise-sensitive land uses. Measures that could accomplish this standard include, but are not limited to:

- Installing sound enclosures around all emergency generators,
- Utilizing mufflers to reduce generator noise, and
- Utilizing equipment that meets this standard.

Prior to the issuance of building permits, the Project Sponsor shall prepare a report, identifying measures that shall be implemented to ensure that exterior noise levels from emergency generators comply with the 60 dBA Leq daytime/nighttime noise standards.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that installing sound enclosures around emergency generators is feasible and would reduce potential operational noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to emergency generator noise would not be significant.

Mitigation Measure NOI-1.4: Limit Generator Testing to Daytime Hours. The Project Sponsor shall limit generator testing to between the hours of 8:00 a.m. and 6:00 p.m.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that limiting generator testing to daytime hours is feasible and would reduce potential operational noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to operational noise would not be significant.

Mitigation Measure NOI-1.5: Design Enclosures around Mechanical Equipment Associated with the Recycled Water System to Limit Exterior Noise. The Project

Sponsor shall design the recycled water system such that noise generated by mechanical equipment complies with the City noise standards of 60 dBA Leq (daytime) and 50 dBA Leq (nighttime) at nearby residences. Measures that could accomplish this include, but are not limited to:

- Designing equipment room enclosures, access doors, and other equipment room openings to limit noise that could be transmitted to the exterior
- Utilizing mufflers to limit blower noise

Prior to the issuance of building permits, the Project Sponsor shall prepare a report, identifying measures that shall be implemented to ensure that exterior noise levels from the recycled water system comply with the daytime and nighttime noise standards.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that enclosures around mechanical equipment associated with the recycled water system are feasible and would reduce potential operational noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to operational noise would not be significant.

**Impact NOI-3:** Substantial Permanent Increase in Ambient Noise Levels. The Project would result in a permanent increase in ambient noise levels in the Project vicinity, above levels existing without the Project.

Mitigation Measures: Implement Mitigation Measures NOI-1.2 through NOI-1.5.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the operational noise control measures are feasible and would reduce potential operational noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to operational noise would not be significant.

**Impact NOI-4:** Substantial Temporary or Periodic Increase in Ambient Noise Levels. The Project could result in a substantial temporary or periodic increase in ambient noise levels in the Project vicinity, above levels existing without the Project.

Mitigation Measures: Implement Mitigation Measure NOI-1.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the noise control measures are feasible and would reduce potential construction equipment noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction equipment noise would not be significant.

**Impact C-NOI-1:** Cumulative Exposure to Excessive Noise. The Project, in combination with other development within the city, could result in a substantial increase in exposure of persons to noise in excess of the standards established in the City General Plan or Municipal Code. The Project's contribution would be cumulatively significant.

Mitigation Measures: Implement Mitigation Measure NOI-1.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the noise control measures are feasible and would reduce potential construction equipment noise impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to construction equipment noise would not be significant.

## **F. CULTURAL RESOURCES**

**Impact CUL-2:** Impacts on Archaeological Resources. The Project has the potential to encounter and damage or destroy previously unknown subsurface archaeological resources during construction.

Mitigation Measure CUL-2.1: Perform Construction Monitoring, Evaluate Uncovered Archaeological Features, and Mitigate Potential Disturbance of Identified Significant Resources at the Project Site. Prior to demolition, excavation, grading, or other construction-related activities on the Project site, the Project Sponsor shall hire a qualified professional archaeologist (i.e., one who meets the Secretary of the Interior's professional qualifications for archaeology or one under the supervision of

such a professional) to monitor, to the extent determined necessary by the archaeologist, Project-related earth-disturbing activities (e.g., grading, excavation, trenching). In the event that any prehistoric or historic-period subsurface archaeological features or deposits, including locally darkened soil (midden), that could conceal cultural deposits, animal bone, obsidian, and/or mortar are discovered during demolition/construction-related earthmoving activities, all ground-disturbing activity within 100 feet of the discovery shall be halted immediately, and the Planning and Building Divisions shall be notified within 24 hours. The City shall consult with the Project archaeologist to assess the significance of the find. Impacts on any significant resources shall be mitigated to a less-than-significant level through data recovery or other methods determined adequate by the City that are consistent with the Secretary of the Interior's Standards for Archaeological Documentation. If Native American archaeological, ethnographic, or spiritual resources are discovered, all identification and treatment of the resources shall be conducted by a qualified archaeologist and Native American representatives who are approved by the local Native American community as scholars of the cultural traditions. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. When historic archaeological sites or historic architectural features are involved, all identification and treatment is to be carried out by historical archaeologists or architectural historians who meet the Secretary of the Interior's professional qualifications for archaeology and/or architectural history.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that monitoring, evaluation, and mitigation of archaeological features is feasible and would reduce potential impacts to archaeological features to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to archaeological features would not be significant.

**Impact CUL-3:** Impacts on Paleontological Resources. The Project could destroy a unique paleontological resource or site or unique geologic feature.

Mitigation Measure CUL-3.1: Conduct Protocol and Procedures for Encountering Paleontological Resources. Prior to the start of any subsurface excavations that would extend beyond previously disturbed soils, all construction forepersons and field supervisors shall receive training by a qualified professional paleontologist, as defined by the SVP, who is experienced in teaching non-specialists to ensure they recognize fossil materials and follow proper notification procedures in the event any such materials are uncovered during construction. Procedures to be conveyed to workers include halting construction within 50 feet of any potential fossil find and notifying a qualified paleontologist, who shall evaluate its significance.

If a fossil is determined to be significant and avoidance is not feasible, the paleontologist shall develop and implement an excavation and salvage plan in accordance with SVP standards. Construction work in these areas shall be halted or diverted to allow recovery of fossil remains in a timely manner. Fossil remains collected during the monitoring and salvage portion of the mitigation program shall be cleaned, repaired, sorted, and cataloged. Prepared fossils, along with copies of all pertinent field notes, photos, and maps, shall then be deposited in a scientific institution with paleontological collections. A final Paleontological Mitigation Plan Report shall be prepared that outlines the results of the mitigation program. The City shall be responsible for ensuring that the monitor's recommendations regarding treatment and reporting are implemented.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the protocol and procedures for encountering paleontological resources is feasible and would reduce potential impacts to paleontological features to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to paleontological features would not be significant.

**Impact CUL-4:** Impacts on Human Remains. The Project has the potential to encounter or discover human remains during excavation or construction.

Mitigation Measure CUL-4.1: Comply with State Regulations Regarding the Discovery of Human Remains at the Project Site. If human remains are discovered during any construction activities, all ground-disturbing activity within 50 feet of the remains shall be halted immediately, and the county coroner shall be notified immediately, according to Section 5097.98 of the State Public Resources Code and Section 7050.5 of California's Health and Safety Code. Additionally, the Planning and Building Divisions shall be notified. If the remains are determined by the county coroner to be Native American, the NAHC shall be notified within 24 hours, and the guidelines of the NAHC shall be adhered to in the treatment and disposition of the remains. The Project Sponsor shall also retain a professional archaeologist with Native American burial experience to conduct a field investigation of the specific site and consult with the Most Likely Descendant, if any, identified by the NAHC. As necessary, the archaeologist may provide professional assistance to the Most Likely Descendant, including the excavation and removal of the human remains. The City of Menlo Park Community Development Department, Planning Division, shall be responsible for approval of recommended mitigation as it deems appropriate, taking account of the provisions of state law, as set forth in State CEQA Guidelines Section 15064.5(e) and Public Resources Code Section 5097.98. The applicant shall implement approved mitigation, to be verified by the Planning Division, before the

resumption of ground-disturbing activities within 50 feet of where the remains were discovered.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the State regulations for discovery of human remains during construction are feasible and would reduce potential impacts to human remains to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to human remains would not be significant.

## **G. BIOLOGICAL RESOURCES**

**Impact BIO-2:** Indirect Impacts on Special-Status Species. The Project could result in increased predation of special-status bird and mammal species that inhabit nearby saltwater and brackish water marshes in the Don Edwards National Wildlife Refuge.

Mitigation Measure BIO-2.1: Install Bird Perching Deterrents on All New Buildings and Other Elevated Structures, Including the Bicycle/Pedestrian Bridge. The Project Sponsor shall implement the following measures to protect special-status species from increased predation on the Project site:

- For all new buildings constructed on the Project site, as well as the bicycle/pedestrian bridge and northern bridge approaches, the Project Sponsor shall install bird deterrents along suitable perching sites to deter avian predators of special-status species that inhabit the adjacent salt marshes. Such deterrents may include one or more of the following: bird spikes, bird netting, an electric shock track, sound deterrents, or perching deterrents approved by CDFW and/or USFWS.
- Trees that are used for replacement landscaping, especially those planted on rooftops, shall consist of species that generally do not exceed 30 feet in height to limit the visibility of adjacent salt marshes to the north. These trees may include native or non-invasive nonnative ornamental species. Species with broad canopies are preferred because trees with tall, narrow canopies (e.g., palms or conifers) generally provide better hunting perches for raptors. Additionally, trees that are planted on the rooftops of the new buildings shall be located away from the edge of the roof and planted with a reduced line of sight to the Bay.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that the installing bird perching deterrents on all buildings and other elevated structures are feasible and would reduce potential impacts to special-status species to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to special-status species would not be significant.

**Impact BIO-3:** Impacts on Native Wildlife Nursery Sites. The removal of buildings, trees, shrubs, or woody vegetation and the installation of new buildings and lighting could affect native migratory birds.

Mitigation Measure BIO-3.1: Conduct Pre-construction Surveys for Nesting Migratory Birds. The Project Sponsor shall implement the following measures to reduce impacts on nesting migratory birds:

- To facilitate compliance with state and federal law (California Fish and Game Code and the MBTA) and prevent impacts on nesting birds, the Project Sponsor shall avoid construction during the nesting season (February 1 through September 14) or conduct pre-construction surveys, as described below.
- If it is not feasible to avoid the nesting season, the Project Sponsor shall hire a qualified wildlife biologist with demonstrated experience to conduct a survey for nesting birds, including raptors, no earlier than 3 days prior to the commencement of ground-disturbing activities and vegetation removal (including clearing, grubbing, and staging). The area surveyed shall include all construction areas within the Project site as well as areas within 250 feet outside the boundaries of the areas to be cleared or as otherwise determined by the biologist.
- If construction activities related to the multi-use bicycle/pedestrian bridge and occurring on the northern side of the Bayfront Expressway are initiated during the nesting bird season, within 3 days prior to the start of construction, a survey shall be conducted by a qualified biologist to determine whether western snowy plovers are nesting within 600 feet of the proposed construction area. Surveys shall be conducted on two week intervals, between February 1 and through May 30, or longer, if necessary, as determined by the biologist based on the behavior and habitat. If an active nest is identified, a buffer of 600 feet shall be established between the construction area and the nest, and the nest shall be periodically monitored by a qualified biologist to determine when it is no longer active (at which point the buffer will no longer be needed). If there is a visual barrier, such as a levee or dense vegetation, between the construction area and the nest, such that the plover will not be able to see construction activity from the nest, then the Project Sponsor may coordinate with the USFWS to

determine whether a reduced buffer would be sufficient to allow work to occur without disturbing the nesting plovers.

- A nest survey shall be required prior to implementation of Phase 1 and Phase 2 of the Project and when construction work stops at a portion of the site where suitable nesting habitat remains for more than 15 days. Additionally, at least one nest survey shall be conducted at the beginning of each year of Project implementation between February and May. As discussed in Chapter 2, Project Description, Project implementation will occur between 2016 and 2022. The need for additional surveys shall be determined by the qualified wildlife biologist and based on the results of the initial survey.
- If the biologist finds active nests during the survey, he or she shall establish species-specific no-disturbance buffer zones for each nest with use of high-visibility fencing, flagging, or pin flags. No construction activities shall be allowed within the buffer zones. The size of the buffer shall be based on the species sensitivity to disturbance and planned work activities in the vicinity. The buffer shall remain in effect until the nest is no longer active.
- If structure demolition activities cannot occur outside of the nesting season, the Project Sponsor or its contractor shall remove inactive nests from the structure to be demolished and install nest exclusion measures (i.e., fine mesh netting, panels, or metal projectors) outside of the nesting season. All exclusionary devices shall be monitored and maintained throughout the breeding season to ensure that they are successful in preventing the birds from accessing cavities or nest sites. No more than 3 days prior to building demolition activities, a qualified biologist shall conduct a pre-construction survey of all potential nesting habitat on the structure to be demolished and the surrounding areas for the presence of active nests. If active nests are found on the building or in the affected area, then demolition activities shall not proceed until the biologist verifies that all nests on the building are inactive.
- After all surveys and/or nest deterrence activities are completed, the biologist shall complete a memorandum detailing the survey effort and results and submit the memorandum to the City within 7 days of survey completion.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that preconstruction surveys are feasible and would reduce potential impacts to nesting birds to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to nesting birds would not be significant.

Mitigation Measure BIO-3.2: Implement Bird-Safe Design Standards into Project Buildings and Lighting Design. The Project Sponsor or its contractor shall implement the following measures to minimize hazards to birds:

- Reduce large areas of transparent or reflective glass.
- Locate water features, trees, and bird habitat away from building exteriors to reduce reflection.
- Reduce or eliminate the visibility of landscaped areas behind glass.
- Turn non-emergency lighting off at night, especially during bird migration season (February–May and August–November).
- Include window coverings that adequately block light transmission from rooms where interior lighting is used at night and install motion sensors or controls to extinguish lights in unoccupied spaces.
- Design and/or install lighting fixtures that minimize light pollution, including light trespass, over-illumination, glare, light clutter, and skyglow, while using bird-friendly colors for lighting when possible. San Francisco's Standards for Bird-safe Buildings document<sup>2</sup> provides a good overview of building design and lighting guidelines to minimize bird/building collisions.
- Nighttime construction work near Pond R3 shall be avoided. If nighttime construction work cannot be avoided, lighting will be directed to the work area and away from habitat for the western snowy plover.

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<sup>2</sup> City and County of San Francisco. 2011. *Standards for Bird-Safe Buildings*. San Francisco Planning Department. July 14. Available: <[http://www.sf-planning.org/ftp/files/publications\\_reports/bird\\_safe\\_bldgs/Standards%20for%20Bird%20Safe%20Buildings %20-%202011-30-11.pdf](http://www.sf-planning.org/ftp/files/publications_reports/bird_safe_bldgs/Standards%20for%20Bird%20Safe%20Buildings%20-%202011-30-11.pdf)>.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that bird-safe design standards are feasible and would reduce potential bird hazards to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to birds would not be significant.

## **H. HYDROLOGY AND WATER QUALITY**

**Impact WQ-1:** Violation of Water Quality Standards or Waste Discharge Requirements. The Project could violate water quality standards or waste discharge requirements.

Mitigation Measure WQ-1.1: Implement Construction Dewatering Treatment (if necessary). Dewatering treatment would be necessary if groundwater is encountered during excavation activities, dewatering is necessary to complete the Project, or the dewatered water is discharged to any storm drain or surface water body. Because there is potential for groundwater to be contaminated with VOC's or fuel products at the Project site, the Project Sponsor would be required to comply with the San Francisco Bay RWQCB's VOC and Fuel General Permit (Order No. R2-2012-0012).

If dewatering activities require discharges into the storm drain system or other water bodies, the water shall be pumped to a tank and tested for water quality using grab samples and sent to a certified laboratory for analysis. If it is found that the water does not meet water quality standards, it should either be treated as necessary prior to discharge so that all applicable water quality objectives (as noted in Tables 3.10-1 and 3.10-2) are met or hauled offsite instead for treatment and disposal at an appropriate waste treatment facility that is permitted to receive such water. Water treatment methods shall be selected that achieve maximum removal of contaminants found in the groundwater and represent the best available technology that is economically achievable. Implemented methods may include the retention of dewatering effluent until particulate matter has settled before it is discharged, the use of infiltration areas, filtration, or other means. The contractor shall perform routine inspections of the construction area to verify that the water quality control measures are properly implemented and maintained, conduct visual observations of the water (i.e., check for odors, discoloration, or an oily sheen on groundwater), and perform other sampling and reporting activities prior to discharge. The final selection of water quality control measures shall be submitted in a report to the San Francisco Bay RWQCB for approval prior to construction. If the results from the groundwater laboratory do not meet water quality standards and the identified water treatment measures cannot ensure treatment that meets all standards for receiving water

quality, then the water shall be hauled offsite instead for treatment and disposal of at an appropriate waste treatment facility that is permitted to receive such water.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that implementing construction dewatering treatment is feasible and would reduce potential impacts to water quality standards or waste discharge requirements to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to water quality standards or waste discharge requirements would not be significant.

**Impact WQ-5:** Impacts from Flooding. The Project could expose people or structures to a significant risk of loss, injury, or death involving flooding, including flooding as a result of the failure of a levee or dam, but would not place structures within a 100-year flood hazard area.

Mitigation Measure WQ-5.1: Flood-Proofing of Project Underground Infrastructure. Prior to or, at a minimum, concurrent with the issuance of the first construction activity permit at the Project site, and in connection with applicable FEMA requirements, the Project Sponsor shall ensure that the Project incorporates design features, including storm drains, sewers, and equipment facilities, that would flood-proof underground infrastructure, thereby allowing it to withstand hydrostatic forces and buoyancy from SLR changes in groundwater levels. Onsite recycled-water wetland treatment areas shall be located at grade, with underground tanks placed in elevated areas to provide protection from the 100-year BFE plus 16 inches.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that flood-proofing of Project underground infrastructure is feasible and would reduce potential impacts from flooding to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to flooding would not be significant.

Mitigation Measure WQ-5.2: Provide Adequate Stormflow Conveyance Capacity for Sea-Level Rise Conditions at the Project Site. Prior to or, at a minimum, concurrent with the issuance of the first construction activity permit at the Project site, the Project Sponsor shall provide current documentation in the form of a technical report to ensure that, as a result of Project design features, the storm drain system's

existing conveyance capacity is not constricted by SLR at the outlets, including the offsite Chrysler pump station, as a result of the Project design.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that providing adequate stormflow conveyance capacity is feasible and would reduce potential impacts from sea level rise to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to sea level rise would not be significant.

**Impact C-WQ-1:** Cumulative Hydrology and Water Quality Impacts. The Project, in combination with other foreseeable development in the vicinity, could contribute considerably to cumulative impacts on water quality, groundwater recharge and supplies, storm drain capacity, or current flooding.

Mitigation Measure: Implement Mitigation Measure WQ-1.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that implementing construction dewatering treatment is feasible and would reduce potential cumulative impacts to water quality standards or waste discharge requirements to a less-than-significant level.

*Remaining Impacts:* Any remaining cumulative impacts related to water quality standards or waste discharge requirements would not be significant.

## **I. HAZARDS AND HAZARDOUS MATERIALS**

**Impact HAZ-2:** Upset and Accident Conditions Involving Hazardous Materials. The Project could create a potentially significant hazard to human health and/or the environment involving the release of hazardous materials.

Mitigation Measure HAZ-2.1: Soil and Groundwater Management. Soil Management Plans that cover the entire Project site shall be prepared and implemented. These Soil Management Plans shall, as appropriate, incorporate the analytical results from the most recent groundwater monitoring event and soil investigations and include protocols for managing both known and potentially undocumented residual soil and groundwater contamination that may be encountered during Project construction, including naturally occurring asbestos. The Soil Management Plans shall include

dust control measures that describe how construction and grading operations will minimize dust emissions and ensure that no equipment or operations will emit visible dust across the property line. Although naturally occurring asbestos has not been detected in the vicinity of Buildings 307-309, in accordance with CARB's Asbestos ATCM for Construction, Grading, Quarrying, and Surface Mining Operations, if naturally occurring asbestos is encountered during construction, then dust control measures must meet the requirements of an ADMP approved by the BAAQMD. These Soil Management Plans shall be approved by DTSC and implemented during Project construction.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that implementation of Soil Management Plans are feasible and would reduce potentially significant hazard to human health and/or the environment involving the release of hazardous materials to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to the release of hazardous materials would not be significant.

Mitigation Measure HAZ-2.2: Additional Site Investigation. If required by DTSC, additional site investigations shall be performed to delineate the source and extent of contamination on the Project site. At DTSC's discretion, these investigations may be incorporated into the Soil Management Plans required by DTSC for the Project site. The analytical results shall be compared to risk-based human health screening levels approved by DTSC. The site investigation(s) shall be prepared and evaluated by a licensed professional, and a technical report summarizing the field activities, results, and conclusion shall be submitted to DTSC for review and approval prior to issuance of building permits.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that additional site investigations to delineate the source and extent of contamination is feasible (as required by DTSC) and would reduce potential contamination impacts to a less-than-significant level.

*Remaining Impacts:* Any remaining impacts related to potential contamination would not be significant.

Mitigation Measure HAZ-2.3: Remedial Action. According to the results of additional site investigations (if any), the Project Sponsor shall coordinate with DTSC to select

and implement remedial actions (as necessary) to protect future site users from conditions that could pose an unacceptable health risk. Remedial measures may include, but are not limited to, source removal of contaminated materials, in-situ treatment, engineering controls, and/or modification of institutional controls described in the existing LUC for the Project site. Remedial actions shall be implemented prior to building occupancy. At DTSC's discretion, remedial actions may be completed during implementation of the Soil Management Plans required by DTSC for the Project site.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect as identified in the EIR. The City finds that coordination with DTSC to select and implement remedial actions (as necessary) is feasible and would reduce potential contamination impacts to a less-than-significant level.

#### **IV. FINDINGS FOR REGARDING SIGNIFICANT AND UNAVOIDABLE IMPACTS**

The Final EIR concluded that the Project would result in potentially significant environmental impacts. The City finds that by incorporating into the Project all the feasible mitigation measures outlined in the MMRP, the impacts would be reduced. However, even after mitigation, some impacts would remain significant and unavoidable. A significant and unavoidable impact is an impact that cannot be mitigated to a less than significant level if the Project is implemented, because no feasible mitigation has been identified. The City finds that there is no additional feasible mitigation that could be imposed beyond what is detailed herein. For the reasons set forth in the Statement of Overriding Considerations below, the City finds that there are economic, legal, social, technological, or other benefits of the Project that override the following significant and unavoidable impacts of the Project.

##### **A. TRANSPORTATION**

**Impact TRA-1:** Impacts on Peak-Hour Traffic at Study Intersections under Background Plus-Project Conditions. Increases in traffic associated with the Project would result in increased delays during peak hour, causing significant and unavoidable impacts on the operation of study intersections.

Mitigation Measure TRA-1.1: Provide Increased Traffic Capacity on Peak-Hour LOS under Background-Plus Project Conditions. Mitigation Measure TRA-1.1 incorporates the analysis of potential impacts on intersection identifies potential measures to mitigate or reduce Project impacts where feasible.

Mitigation Measure TRA-1.2. Reduce the Peak-Hour Share of Vehicle Trips Allowable under the Trip Cap, for both the Project Site and Buildings 10-19 to no more than 50 Percent of Allowable Vehicle Trips During each 2 Hour Peak Commute Period. The Project trip cap, as proposed, would allow up to 69 percent of vehicle

trips within each 2-hour peak commute period to enter or exit the site within a single peak hour. Similarly, the approved vehicle trip caps for Buildings 10-20 currently allow up to 70 percent of permitted vehicle trips within each 2-hour peak commute period to occur within a single hour. The proposed mitigation would reduce the maximum number of allowable peak-hour vehicle trips to no more than 50 percent of the 2-hour peak-period vehicle trip cap for both the Project site and Buildings 10-19. Table 3.3-7 provides a comparison of the potential peak 1-hour vehicle trips under the Project and with the proposed mitigation. As shown, the proposed mitigation would reduce the total volume of allowed peak-hour vehicle trips to the Project site and Buildings 10-19 by 28 percent.

(a) *Willow Road & Hamilton Avenue (#36)*

During the p.m. peak hour, the intersection would be anticipated to operate unacceptably at LOS F under both background and background plus-Project conditions. Project traffic would increase delay and exceed the City threshold of 0.8 second for critical movements. The increase in delay reflects a forecast increase in left-turn volumes related to vehicles from the Project site traveling through residential neighborhoods via Chilco Street and Hamilton Avenue to by-pass eastbound delay on the segment of Bayfront Expressway where it approaches Willow Road.

Although the provision of an eastbound left-turn lane on Hamilton Avenue where it approaches Willow Road would reduce the delay, this potential mitigation is not recommended because it would encourage cut-through traffic via Chilco Street and Hamilton Avenue, potentially affecting the Belle Haven neighborhood. Therefore, to avoid facilitating the use of Chilco Street and Hamilton Avenue as cut-through routes in the adjacent residential neighborhood, mitigating this traffic impact is not recommended, consistent with City policies that discourage cut-through traffic in residential neighborhoods. Other mitigation measures are discussed below (TRA-3.1 and TRA-3.2) to discourage cut-through traffic in the Belle Haven neighborhood. Mitigation Measure TRA-1.2 (described above) would reduce the impact, but the net change in delay would still be likely to exceed the 0.8-second threshold for critical movements. The peak-hour traffic impact would remain significant and unavoidable.

(b) *Bayfront Expressway & Willow Road (#37)*

Given the applicable criteria for state-controlled intersections that operate at LOS E or F, a significant impact would occur if the combination of Project and future traffic would increase average delay by 4 seconds or more. The net change in average delay from existing conditions, under both background and background plus-Project conditions, would exceed the 4-second threshold. Furthermore, the addition of Project trips would result in a net change between background and background plus-Project conditions that would also exceed the 4-second threshold.

During the a.m. and p.m. peak hours, approximately 70 percent of Dumbarton Bridge vehicle trips pass through this intersection (more than 4,500 vehicles during each peak hour under existing conditions). Similarly, the majority of allowable peak-

hour vehicle trips to/from the Project site and Buildings 10–20 under the trip caps would also pass through this intersection because there are few viable alternative routes to/from most directions (particularly for trips to/from the south) and limited access points to both Campuses. As a result, the volume of conflicting movements at this intersection would increase significantly under background conditions because of the significant increase in conflicting movements. The addition of Project traffic would increase average delay by more than 80 seconds during the a.m. peak hour and more than 20 seconds during the p.m. peak hour, thereby exceeding the applicable impact threshold. Physical improvement options to expand approach capacity would be constrained given the proximity of the Dumbarton rail tracks and adjacent wetlands. Similarly, signal timing, which is necessary to accommodate the high volume of peak-direction traffic to/from the Dumbarton Bridge, would limit the viability of some mitigation options.

During the a.m. peak hour, the Project impact would be greatest in the northbound direction on Willow Road, affecting northbound through and northbound left-turn movements when approaching Bayfront Expressway from Willow Road. During the p.m. peak hour, the Project impact would be greatest in the eastbound direction on Bayfront Expressway when approaching the intersection with Willow Road because of the high volume of bridge approach traffic. In addition, during the p.m. peak hour, mitigation options at this intersection would ultimately be affected by downstream capacity limitations at the entrance to the Dumbarton Bridge.

Localized queuing and delays in the p.m. peak hour may be minimized by extending the eastbound right-turn pocket from Bayfront Expressway to Willow Road. The turn pocket should be extended toward the Building 20 entrance to maximize queue storage for motorists who wish to turn right to access US 101 south. Delays for p.m. peak-hour traffic as it approaches the Dumbarton Bridge could be reduced if a dedicated receiving lane were to be provided on Bayfront Expressway for northbound right-turn movements from Willow Road. Such a mitigation, if feasible, would allow both northbound right-turn and eastbound through movements to occur concurrently. However, the mitigation would not reduce net travel time for motorists when approaching the Dumbarton Bridge, given downstream capacity at the Bayfront Expressway and University Avenue intersection. Similarly, grade separation to allow conflicting movements to occur simultaneously, if feasible, could reduce the impact on approach delay directly at the intersection but would not reduce net travel time unless accompanied by similar measures at downstream intersections.

Increasing capacity between US 101 and the Dumbarton Bridge via the Marsh Road/US 101 interchange, to provide a viable alternate route to Willow Road, could reduce the impact, if feasible and if designed to accommodate vehicles to/from the south on US 101 without increasing travel time (compared with the travel time to Willow Road). Such improvements could include one additional northbound exit lane on US 101 between Willow Road and Marsh Road and a similar measure to accommodate southbound traffic when entering US 101 via the Marsh Road intersection.

The Project Sponsor shall be required to design and construct a lengthened eastbound right-turn pocket and a dedicated receiving lane on Bayfront Expressway for northbound right-turn traffic. Because the improvements would be under Caltrans jurisdiction, the Project Sponsor would be required to coordinate with Caltrans for review and approval of the improvements. The potential mitigation options described above are not under the control of the City, and thus, the City cannot guarantee their implementation. In addition, with implementation of Mitigation Measure TRA-1.2 (described above), the net increase in the number of peak-hour vehicle trips resulting from the Project during the a.m. and p.m. peak hours would be reduced. However, the increased delay at this intersection would still be anticipated to exceed the significance threshold. Therefore, this impact would remain significant and unavoidable.

(c) *Bayfront Expressway & University Avenue (#38)*

Given the applicable criteria for state-controlled intersections that operate at LOS E or F, a significant impact would occur if the combination of Project and future cumulative traffic would increase average delay by 4 seconds or more. This intersection would be anticipated to operate at LOS F under existing, background, and background plus-Project conditions. The net change in average delay with the addition of Project traffic would not exceed the 4-second threshold. Therefore, Project trips would not significantly affect LOS, based strictly on the approach delay at the intersection. However, the Project would contribute to an increase in upstream delay, thereby affecting access to the Dumbarton Bridge, including increased eastbound delays where traffic would approach the Bayfront Expressway and Willow Road intersection under background plus-Project conditions. Given both the upstream and downstream capacity limitations on both sides of the Dumbarton Bridge corridor, including traffic congestion and capacity limitations on US 101 as well as I-880 on the east side of San Francisco Bay (Bay), peak-hour traffic volumes on the Dumbarton Bridge would not be anticipated to increase significantly. Instead, a greater portion of peak-hour traffic demand on the Dumbarton Bridge would be anticipated to occur outside of the peak hours, including some trips that would be delayed because of peak-hour congestion on connecting facilities. Grade separation that would allow conflicting movements to occur simultaneously, if feasible, would reduce delay where traffic would approach the intersection but could result in secondary impacts at downstream locations.

The Project Sponsor shall be required to initiate design concepts through a Project Study Report (PSR), or other appropriate development document, for potential future grade separation at this intersection. Because the intersection would be under Caltrans jurisdiction, the Project Sponsor would be required to coordinate with Caltrans and the City. This potential mitigation is not under the control of the City, and the impact would remain significant and unavoidable.

(d) *University Avenue & Adams Drive (#47)*

Unacceptable LOS F occurs at this intersection under existing conditions, reflecting delay on the stop-controlled side street where it approaches University Avenue.

Traffic volumes on Adams Drive where it approaches the stop sign are very low (i.e., 11 vehicle trips during the a.m. peak hour and 51 vehicles during the p.m. peak hour).

Under background plus-Project conditions, the side-street approach volume is forecast to increase to 86 vehicles. However, traffic levels would remain below the threshold for warranting a traffic signal. Thus, a traffic signal is not recommended under background plus-Project conditions. The impact under background plus-Project conditions would therefore be significant and unavoidable.

Installation of a traffic signal at this location would be recommended under 2040 cumulative conditions with the proposed General Plan. Therefore, if the proposed General Plan is adopted, this impact could be mitigated to less-than-significant levels (see Mitigation Measure TRA 13.1).

*(k) Chilco Street & Hamilton Avenue (#60)*

This all-way stop-controlled intersection, located within the Belle Haven neighborhood south of the Project site, would operate at an unacceptable LOS of F during the p.m. peak hour because a portion of Project vehicle trips would be anticipated to exit the site via Chilco Street southbound to Hamilton Avenue or other streets in the Belle Haven neighborhood to access Willow Road. Signalizing the intersection would improve LOS to an acceptable level.

Mitigation Measure TRA-1.2 (described above) would reduce allowable net Project vehicle trip generation during the p.m. peak hour, but this intersection would still be anticipated to operate unacceptably given the proximity to the Project entrance and the LOS standard of C or better that applies to this intersection. However, any mitigation to improve traffic operations would encourage use of Chilco Street as a cut-through route, which conflicts with City of Menlo Park goals that aim to reduce cut-through traffic in residential neighborhoods. Therefore, to avoid facilitating use of Chilco Street and Hamilton Avenue as cut-through routes, mitigating this traffic impact by increasing capacity is not recommended, consistent with City policies that discourage cut-through traffic in residential neighborhoods. Other mitigation measures are discussed below (Mitigation Measure TRA-3.1 and TRA 3.2) to discourage cut-through traffic in the Belle Haven neighborhood. The peak-hour impact on intersection LOS is therefore significant and unavoidable.

*(l) Bayfront Expressway & Facebook Building 20 Entrance (#65)*

The intersection, which opened following the completion of Building 20 in 2015, would operate at LOS C during the a.m. peak hour and LOS F during the p.m. peak hour under background conditions. However, LOS would degrade to LOS E during the a.m. peak hour and LOS F during the p.m. peak hour under background plus-Project conditions.

During the a.m. peak hour, traffic in the single westbound left-turn lane from Bayfront Expressway (entering Building 20) would exceed storage capacity, resulting in delays for peak-direction traffic when traveling westbound on Bayfront Expressway. Provision of a two-lane left-turn pocket at the proposed adjacent entrance to the Project site at Building 21 would reduce the potential impact during the a.m. peak hour by allowing a portion of left-turn demand to use the adjacent intersection (see Mitigation Measure TRA-3.1).

During the p.m. peak hour, delay would increase for exiting eastbound vehicles traveling toward Willow Road under background plus-Project conditions. This would be caused by the high volume of eastbound vehicles traveling between the Project site and Willow Road via a short segment of Bayfront Expressway. Building 20 currently has a driveway to Willow Road that allows Project traffic to exit directly to Willow Road. Encouraging greater use of that driveway for outbound trips could help to reduce a portion of the eastbound traffic volume on Bayfront Expressway traveling toward Willow Road during the p.m. peak hour.

The a.m. peak hour impact would be reduced to less-than-significant levels by providing a two-lane westbound left-turn pocket at the adjacent intersection of Bayfront Expressway and the Building 21 entrance. However, the right-of-way along Bayfront Expressway is constrained by the wetlands located adjacent to the roadway; therefore, this mitigation measure may not be feasible. Alternatively, the Project Sponsor shall be required to conduct a micro-simulation evaluation as part of the proposal to install a new traffic signal at the proposed entrance to Building 21 and ensure that queues do not extend onto the Bayfront Expressway at either intersection (see Mitigation Measure TRA-1.1m, below). During the p.m. peak hour, the provision of one additional eastbound through lane on Bayfront Expressway would mitigate the impact but would not improve net vehicle delay where traffic approaches the Dumbarton Bridge unless accompanied by measures to reduce downstream delay. The mitigation may not be feasible given downstream capacity constraints. Furthermore, the intersection is under the jurisdiction of Caltrans; therefore, the City cannot guarantee that this improvement would be implemented.

Mitigation Measure TRA-1.2 (described above) would reduce net Project vehicle trip generation during both peak hours, but the increase in eastbound traffic on Bayfront Expressway between Chilco Street and Willow Road would still be anticipated to result in a significant impact on p.m. peak-hour LOS at this intersection. Therefore, the impact would remain significant and unavoidable.

*(m) Bayfront Expressway & Proposed Building 21 Entrance (#66)*

As part of the Project, this would be a signalized intersection, providing two outbound travel lanes, one inbound right-turn lane, and one inbound left-turn for vehicles entering the Project site from Bayfront Expressway. During the p.m. peak hour, the intersection would operate unacceptably at LOS F. During the a.m. peak hour, the intersection would operate acceptably, based on LOS, but the anticipated queue length for vehicles entering the site via the single proposed westbound left-turn lane (from Bayfront Expressway to the Building 21 entrance) would exceed

storage capacity, resulting in delays for westbound through traffic on Bayfront Expressway.

The proposed mitigation to reduce a.m. peak-hour impacts on Bayfront Expressway and the Project impact at the entrance to Building 20 is the provision of a two-lane left-turn pocket for northbound vehicles that would enter Building 21 from Bayfront Expressway. However, the right-of-way along Bayfront Expressway is constrained by the wetlands located adjacent to the roadway; therefore, this mitigation measure may not be feasible. Alternatively, the Project Sponsor shall be required to conduct a micro-simulation evaluation as part of the proposal to install a new traffic signal at this location and ensure that queues do not extend onto Bayfront Expressway at either intersection (see Mitigation Measure TRA-1.1I, above) while maintaining an acceptable intersection LOS of D or better. With the proposed mitigation, if feasible, the impact would be less than significant during the a.m. peak hour.

During the p.m. peak hour, the provision of one additional eastbound through lane on Bayfront Expressway would mitigate the impact but would not improve net vehicle delay where traffic approaches the Dumbarton Bridge unless accompanied by measures to reduce downstream delay. This potential mitigation is unlikely to be feasible given downstream capacity constraints.

Mitigation Measure TRA-1.2 (described above) would reduce net Project trip generation by more than 75 percent during the a.m. peak hour and more than 90 percent during the p.m. peak hour; the volume of inbound and outbound vehicle trips via the proposed Building 21 driveway would be reduced by approximately 30 percent during both peak hours. With Mitigation Measure TRA-1.2, the impact would be less than significant during the p.m. peak hour. Because the feasibility of the a.m. peak-hour mitigation described above has not yet been confirmed, the impact would remain significant and unavoidable.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Mitigation Measure TRA-1.1 identifies potential improvements that could mitigate or reduce the impacts of the Project. However, the identified improvements would require the acquisition of additional right-of-way, are physically infeasible or of speculative efficacy, are economically infeasible, would result in adverse ancillary impacts related to cut-through traffic, are outside of the City's jurisdiction, or are beyond the scope of what can reasonably be expected of a single project; the City therefore finds that the described improvements are not feasible. Although Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would mitigate or reduce these impacts of the Project, it would not reduce them below less-than-significant levels. Impacts on the identified study intersections would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts at the affected intersections would remain significant and unavoidable.

**Impact TRA-2:** Impacts on Routes of Regional Significance under Background Plus Project Conditions. Some Routes of Regional Significance would operate at or below their LOS threshold with the addition of Project trips, and Project traffic would exceed the allowable 1 percent threshold resulting in significant and unavoidable impacts.

Mitigation Measure TRA-2.1: Implement Improvements to Routes of Regional Significance to Address Background Plus-Project Effects. Providing additional travel lanes would increase segment capacity but would not be feasible on all segments given available right-of-way widths and both downstream and downstream capacity limitations on facilities such as US 101 and the Dumbarton Bridge. In addition, the routes are under the control of Caltrans and the City cannot guarantee mitigation.

In addition, implement Mitigation Measure TRA-1.2, above.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Routes of Regional Significance include US Highway 101 and the Bayfront Expressway at specific roadway segments, both of which are within Caltrans' jurisdiction. A typical mitigation measure would seek to widen these highways to add travel lanes and capacity. However, these roadways are not under the jurisdiction of the City, as noted. Freeway improvement projects which add travel lanes are also planned and funded on a regional scale, and the City is not aware of any current planned improvements on the impacted segments. Last, there are physical constraints affecting available right-of-way widths and capacity limitations that make widening improvements infeasible. The City therefore finds that providing additional travel lanes would not be feasible mitigation. Although Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would reduce these impacts of the Project, it would not reduce them below less-than-significant levels. Impacts on the identified Routes of Regional Significance would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts at the foregoing Routes of Regional Significance would remain significant and unavoidable.

**Impact TRA-3:** Increase in Daily Traffic Volumes on Roadway Segments Under Background Plus-Project Conditions. Increases in daily traffic associated with the Project under near term plus-Project Conditions would result in increased ADT volumes on specific Project area roadway segments, resulting in significant and unavoidable impacts, as described in the Final EIR.

Mitigation Measure TRA-3.1: Provide Measures to Reduce Cut-Through Traffic in the Belle Haven Neighborhood via Chilco Street (South of the Dumbarton Rail

Corridor), Newbridge Street, and Ivy Drive. The Project Sponsor shall provide measures to prevent cut-through traffic, which could include prohibiting left-turns exiting the Project site via Chilco Street during the p.m. peak period. The provision of physical traffic calming measures could also be included, where such measures would not affect emergency access and/or transit service, subject to community and City approval.

Because community members and other potentially affected stakeholders may be affected by such improvements, the Project Sponsor shall fund a Neighborhood Traffic Plan to identify appropriate measures for reducing cut-through traffic.

Mitigation Measure TRA-3.2: Provide Multi-Modal improvements on study segments that would be potentially impacted by increased ADT. The Project Sponsor shall provide measures to improve mobility options via walking, bicycling, and transit, consistent with the City's complete streets goals, which would help to offset the effect of daily traffic generated by the Project. In particular, such measures could include pedestrian enhancements across Willow Road at Hamilton Drive, Ivy Drive, and Newbridge Street as well as at other affected study segment locations. These measures are discussed further under Impacts TRA-4 and TRA-5 (described above).

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* The City finds that it is feasible to require the Project Sponsor to work with the community to provide measures to prevent cut-through traffic and fund a Neighborhood Traffic Plan in Belle Haven, and further finds that it is appropriate for the community and local stakeholders to participate in the formation of such a Plan. The City also finds that it is feasible to provide measures to improve mobility options (pedestrian and bicycle improvements) to help offset the effect of daily traffic generated by the Project at the identified segments, as further described above in Mitigation Measures TRA-5.1 and TRA-5.2. However, Mitigation Measures TRA-3.1 and TRA-3.2 (which would require implementation of Mitigation Measures TRA-5.1 and TRA-5.2) would not reduce the Project's impacts on area roadway segments identified in the Final EIR to less-than-significant levels. Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would also reduce these impacts of the Project, but would not reduce them below less-than-significant levels. Additional mitigations would include adding capacity by widening these roadway segments, which would require additional rights-of-way and affect property owners, result in degradation of bicycle and pedestrian facilities, including increased intersection times, and create indirect quality-of-life impacts on local residents, commuters, employees and businesses in the area, as explained in the Final EIR; the City therefore finds that roadway widening and capacity improvements are not feasible. Impacts on the affected roadway segments would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts to local roadway segments would remain significant and unavoidable.

**Impact TRA-8:** Delay to Public Transit Vehicles Under Background Plus Project Conditions. The Project would result in significant and unavoidable delays to public transit vehicles under Background Plus Project Conditions.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Although the Dumbarton Rail Corridor Study (funded by the Project Sponsor) is currently pending and is anticipated to recommend a phased program of operational and infrastructure improvements that could increase transit mobility, no recommendations currently exist that would reduce the potential impacts on the Project on AC Transit's Dumbarton bus service. Other improvements, such as capacity-enhancing measures including transit signal preemption or queue-jump lanes, would be outside of the City's jurisdiction and control, and are found by the City to be infeasible. No additional mitigation measures to reduce potential impacts on public transit vehicles are found to be feasible.

*Remaining Impacts:* The delay to public transit vehicles would remain significant and unavoidable.

**Impact TRA-10:** Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Existing General Plan Plus-Project Conditions. Increases in peak-hour vehicle traffic associated with the Project would result in increased delays during AM and PM peak hours causing significant and unavoidable impacts on the operation of study intersections under Cumulative 2040 Conditions with the Existing General Plan.

Mitigation Measure TRA-10.1: Provide Increased Traffic Capacity to Address Project Impacts on Peak-Hour LOS under Cumulative 2040 Existing General Plan plus-Project Conditions. Mitigation Measure TRA 10.1 identifies potential measures to mitigate or reduce Project impacts where feasible.

(a) *El Camino Real & Glenwood Avenue (#25)*

During the a.m. peak hour, traffic associated with the Project would result in an unacceptable LOS of E under cumulative 2040 existing General Plan plus-Project conditions.

The provision of a dedicated right-turn lane on Glenwood Avenue, where it approaches El Camino Real, is identified in the City's TIF program. The Project Sponsor's payment of the TIF shall partially mitigate this impact. The provision of one additional through lane on Glenwood Avenue would be needed to improve LOS to an acceptable LOS of D and fully mitigate this impact. However, the provision of an additional through lane is not feasible given the right-of-way constraints.

Therefore, this impact would be considered significant and unavoidable under cumulative 2040 existing General Plan plus-Project conditions.

(d) *Willow Road & Hamilton Avenue (#36)*

The Project impact was identified under background plus-Project conditions (see TRA-1.1b). No additional feasible mitigation measures were identified to reduce this peak-hour traffic impact, which would remain significant and unavoidable.

(e) *Bayfront Expressway & Willow Road (#37)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur during the a.m. peak hour under cumulative 2040 existing General Plan plus-Project conditions. As discussed in Mitigation Measure TRA-1.1c, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.

(f) *Bayfront Expressway & University Avenue (#38)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during both the a.m. and p.m. peak hours. As discussed in Mitigation Measure TRA-1.1d, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.

(i) *University Avenue & Adams Drive (#47)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during both the a.m. and p.m. peak hours (see Mitigation Measure TRA-1.1g). This impact would remain significant and unavoidable under existing General Plan plus-Project conditions. (SU)

Installation of a traffic signal at this location would be recommended under 2040 cumulative conditions with the proposed General Plan. Therefore, if the proposed General Plan is adopted, this impact could be mitigated to a less-than-significant level (see Mitigation Measure TRA-13.1i).

(k) *University Avenue & Donohoe Street (#54)*

This state-controlled intersection located adjacent to the US 101 northbound ramps in East Palo Alto operates at LOS F under existing conditions during both the a.m. and p.m. peak hours. The addition of Project traffic under cumulative 2040 existing General Plan plus-Project conditions would result in additional delay that would exceed the 4-second significance threshold during both peak hours.

With implementation of Mitigation Measure TRA-1.2, the net increase in the number of peak-hour vehicle trips resulting from the Project would be substantially reduced, but the increase in delay would still be anticipated to exceed the 4-second significance threshold.

Potential mitigation options are limited given the proximity of adjacent freeway ramp intersections and recent development near the intersection. The provision of additional westbound lane capacity on Donohoe Street, including an extended dual left-turn pocket, dedicated through lane, and dual right-turn lanes, would reduce delay but would not be feasible given the right-of-way limitations, including proximity to the adjacent property on the northeastern corner and the relatively short block length to the upstream

US 101 northbound off-ramp. Similarly, providing a southbound right-turn lane on University Avenue and lengthening the northbound turn pockets, if feasible, would reduce delay but would most likely be constrained by adjacent land uses and proximity to the US 101 overpass and two northbound on-ramps. Furthermore, because the intersection is not under the control of the City of Menlo Park, implementation of potential mitigation to reduce peak-hour delay at this location, even if feasible options were available, cannot be guaranteed. This impact is therefore considered significant and unavoidable.

*(n) Chilco Street & Hamilton Avenue (#60)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during the p.m. peak hour. As discussed in Mitigation TRA-1.1k, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.

*(o) Bayfront Expressway & Facebook Building 20 Entrance (#65)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during the p.m. peak hour. As discussed in Mitigation Measure TRA-1.1l, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable during the p.m. peak hour.

*(p) Bayfront Expressway & Proposed Building 21 Entrance (#66)*

The Project impact was identified under background plus-Project conditions. Additional delay would occur under cumulative 2040 existing General Plan plus-Project conditions, triggering an impact during the a.m. and p.m. peak hours. As discussed in Mitigation Measure TRA-1.1m, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable during the a.m. peak hour.

In addition, implement Mitigation Measure TRA-1.2, above.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Mitigation Measure TRA-10.1 identifies potential improvements that could mitigate or reduce the impacts of the Project. However, the identified improvements would require the acquisition of additional right-of-way, are physically infeasible or of speculative efficacy, are economically infeasible, would result in adverse ancillary impacts related to cut-through traffic, are outside of the City's jurisdiction, or are beyond the scope of what can reasonably be expected of a single project; the City therefore finds that the described improvements are not feasible. Although Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would mitigate or reduce these impacts of the Project, it would not reduce them below less-than-significant levels. Impacts on the identified study intersections would therefore remain significant and unavoidable under the 2040 existing General Plan plus Project conditions.

*Remaining Impacts:* The Project-specific impacts to intersections would remain significant and unavoidable under the 2040 existing General Plan plus Project conditions.

**Impact TRA-11:** Impacts on Routes of Regional Significance Under Cumulative 2040 Existing General Plan Plus Project Conditions. Some Routes of Regional Significance would operate at or below their LOS threshold with the addition of Project trips, and Project traffic would exceed the allowable 1 percent threshold, resulting in significant and unavoidable impacts.

Mitigation Measure: Implement Mitigation Measure TRA-2.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Routes of Regional Significance include US Highway 101 and the Bayfront Expressway at specific roadway segments, both of which are within Caltrans' jurisdiction. A typical mitigation measure would seek to widen these highways to add travel lanes and capacity. However, these roadways are not under the jurisdiction of the City, as noted. Freeway improvement projects which add travel lanes are also planned and funded on a regional scale, and the City is not aware of any current planned improvements on the impacted segments. Last, there are physical constraints affecting available right-of-way widths and capacity limitations that make widening improvements infeasible. The City therefore finds that providing additional travel lanes would not be feasible mitigation. Although Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would reduce these impacts of the Project, it would not reduce them below less-than-significant levels. Impacts on the identified Routes of Regional Significance under cumulative

2040 existing General Plan plus Project conditions would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts at the foregoing Routes of Regional Significance would remain significant and unavoidable under cumulative 2040 existing General Plan plus Project conditions.

**Impact TRA-12:** Increase in Daily Traffic Volumes on Roadway Segments under Cumulative 2040 Existing General Plan Plus-Project Conditions. Increases in daily traffic under existing General Plan plus-Project conditions would result in increased ADT volumes on Project area roadway segments, resulting in significant and unavoidable impacts.

Mitigation Measures: Implement Mitigation Measures TRA-3.1 and TRA-3.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* The City finds that it is feasible to require the Project Sponsor to work with the community to provide measures to prevent cut-through traffic and fund a Neighborhood Traffic Plan in Belle Haven, and further finds that it is appropriate for the community and local stakeholders to participate in the formation of such a Plan. The City also finds that it is feasible to provide measures to improve mobility options (pedestrian and bicycle improvements) to help offset the effect of daily traffic generated by the Project at the identified segments, as further described above in Mitigation Measures TRA-5.1 and TRA-5.2. However, Mitigation Measures TRA-3.1 and TRA-3.2 (which would require implementation of Mitigation Measures TRA-5.1 and TRA-5.2) would not reduce the Project's impacts on the area roadway segments identified in the Final EIR to less-than-significant levels. Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would also reduce these impacts of the Project, but would not reduce them below less-than-significant levels. Additional mitigations would include adding capacity by widening these roadway segments, which would require additional rights-of-way and affect property owners, result in degradation of bicycle and pedestrian facilities, including increased intersection times, and create indirect quality-of-life impacts on local residents, commuters, employees and businesses in the area, as explained in the Final EIR; the City therefore finds that roadway widening and capacity improvements are not feasible. Impacts on the affected roadway segments under cumulative 2040 existing General Plan plus Project conditions would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts to local roadway segments would remain significant and unavoidable under cumulative 2040 General Plan plus Project conditions.

**Impact TRA-13:** Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Proposed General Plan Conditions. Increases in peak-hour vehicle traffic associated with the Project would contribute to increased delays during the a.m. and p.m. peak hours in 2040 under the proposed General Plan (ConnectMenlo), causing a significant and unavoidable impact on the operation of study intersections.

Mitigation Measure TRA-13.1: Increase Traffic Capacity to Address Impacts on Peak-Hour LOS under Cumulative 2040 Proposed General Plan Conditions. This measure describes the types of mitigation measures that would be necessary to mitigate impacts at each affected location to less than significant.

(c) *Willow Road & Hamilton Avenue (#36)*

This potential impact on p.m. peak-hour traffic operations was identified as significant and unavoidable under background plus-Project conditions (see Mitigation Measure TRA-1.1b) and would remain significant and unavoidable under cumulative 2040 existing General Plan plus-Project conditions.

Under cumulative 2040 proposed General Plan conditions, delay would further increase during the p.m. peak hour, thereby exceeding the significance threshold. Project impacts would remain significant and unavoidable, as described under Mitigation Measure TRA-1.1b.

(d) *Bayfront Expressway & Willow Road (#37)*

This potential impact on p.m. peak-hour traffic operations was identified as significant and unavoidable under background plus-Project conditions (see Mitigation Measure TRA-1.1c) and would remain significant and unavoidable under cumulative 2040 existing General Plan plus-Project conditions.

Under cumulative 2040 proposed General Plan conditions, additional delay would exceed the significance threshold (see Mitigation Measure TRA-1.1c for a discussion of potential mitigation and constraints to mitigation). Mitigation Measure TRA-1.2 would partially reduce the impact, but it would remain significant. This impact would remain significant and unavoidable, as described under Mitigation Measure 1.1c.

(e) *Bayfront Expressway & University Avenue (#38)*

This potential impact on peak-hour traffic operations was identified as significant and unavoidable under background plus-Project conditions (see Mitigation Measure TRA-1.1d) and would remain significant and unavoidable under cumulative 2040 existing General Plan plus-Project conditions. Increased delay is anticipated during the p.m. peak hour under the proposed General Plan. This impact would remain significant and unavoidable, as described under Mitigation Measure TRA 1.1d.

(j) *University Avenue & Donohoe Street (#54)*

This state-controlled intersection located adjacent to the US 101 northbound ramps in East Palo Alto operates at LOS F under existing conditions during both the a.m. and p.m. peak hours. The addition of Project traffic under cumulative 2040 existing General Plan plus-Project conditions would result in additional delay that would exceed the 4-second significance threshold during both peak hours. Additional delay would occur under cumulative 2040 proposed General Plan conditions during the a.m. peak hour.

This impact was identified under cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-10.1j) and would remain significant and unavoidable under cumulative 2040 proposed General Plan conditions.

*(l) Chilco Street & Hamilton Avenue (#60)*

The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-1.1k). This impact would remain significant and unavoidable.

*(m) Bayfront Expressway & Facebook Building 20 Entrance (#65)*

The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-1.1l). This impact would remain significant and unavoidable during the p.m. peak hour.

*(n) Bayfront Expressway & Proposed Building 21 Entrance (#66)*

The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA 1.1m). With the proposed mitigation, the impact would remain significant and unavoidable during the a.m. peak hour.

In addition, implement Mitigation Measure TRA-1.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Mitigation Measure TRA-13.1 involves intersection improvements to mitigate or reduce the impacts of the Project. However, intersection impacts would remain significant and unavoidable since the impact cannot be fully mitigated as described above under each specific intersection.

*Remaining Impacts:* The Project-specific impacts at the affected intersections would remain significant and unavoidable.

**Impact TRA-14:** Impacts on Routes of Regional Significance Under Cumulative 2040 Proposed General Plan Conditions. Some Routes of Regional Significance

would operate at or below their LOS threshold with the addition of Project trips. However, Project traffic would exceed the allowable 1 percent threshold, resulting in significant and unavoidable impacts.

Mitigation Measure: Implement Mitigation Measure TRA-2.1.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* Mitigation Measure TRA-10.1 identifies potential improvements that could mitigate or reduce the impacts of the Project. However, the identified improvements would require the acquisition of additional right-of-way, are physically infeasible or of speculative efficacy, are economically infeasible, would result in adverse ancillary impacts related to cut-through traffic, are outside of the City's jurisdiction, or are beyond the scope of what can reasonably be expected of a single project; the City therefore finds that the described improvements are not feasible. Although Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would mitigate or reduce these impacts of the Project, it would not reduce them below less-than-significant levels.

Impacts on the identified study intersections would therefore remain significant and unavoidable under the 2040 existing General Plan plus Project conditions.

*Remaining Impacts:* The Project-specific impacts to intersections would remain significant and unavoidable under the 2040 Proposed General Plan plus Project conditions.

**Impact TRA-15:** Increase in Daily Traffic Volumes on Roadway Segments Under Cumulative 2040 Proposed General Plan Conditions. Increases in daily traffic associated with the Project under Cumulative 2040 Proposed General Plan Conditions would result in increased ADT volumes on Project area roadway segments resulting in significant and unavoidable impacts.

Mitigation Measures: Implement Mitigation Measures TRA-3.1 and TRA-3.2.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* The City finds that it is feasible to require the Project Sponsor to work with the community to provide measures to prevent cut-through traffic and fund a Neighborhood Traffic Plan in Belle Haven, and further finds that it is appropriate for the community and local stakeholders to participate in the formation of such a Plan. The City also finds that it is feasible to provide measures to improve mobility options (pedestrian and bicycle improvements) to help offset the effect of daily traffic generated by the Project at the identified segments, as further described above in Mitigation Measures TRA-5.1 and TRA-5.2. However, Mitigation Measures TRA-3.1 and TRA-3.2 (which would require implementation of

Mitigation Measures TRA-5.1 and TRA-5.2) would not reduce the Project's impacts on the area roadway segments identified in the Final EIR to less-than-significant levels. Mitigation Measure TRA-1.2, which involves the reduction of the peak-hour share of vehicles allowed under the proposed trip cap, would also reduce these impacts of the Project, but would not reduce them below less-than-significant levels. Additional mitigations would include adding capacity by widening these roadway segments, which would require additional rights-of-way and affect property owners, result in degradation of bicycle and pedestrian facilities, including increased intersection times, and create indirect quality-of-life impacts on local residents, commuters, employees and businesses in the area, as explained in the Final EIR; the City therefore finds that roadway widening and capacity improvements are not feasible. Impacts on the affected roadway segments under cumulative 2040 Proposed General Plan plus Project conditions would therefore remain significant and unavoidable.

*Remaining Impacts:* The Project-specific impacts to local roadway segments would remain significant and unavoidable under cumulative 2040 Proposed General Plan plus Project conditions.

### **C. Greenhouse Gas (GHG) Emissions**

**Impact GHG-2:** Conflicts with Applicable Plans and Policies. The Project would conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of GHGs. Although the Project would not conflict with the City's Climate Action Plan and its projected GHG emissions would be below the 2030 "substantial progress" efficiency metric and goals associated with Executive Order B-30-15, it cannot be determined whether the Project emissions would be consistent with the long-term goals of Executive Order S-3-05, which sets forth emissions reductions targets for 2050.

**FINDINGS:** Based upon the entire record before the City, the City Council finds that:

*Effects of Mitigation:* No mitigation measures are feasible.

*Remaining Impacts:* The Project's potential to conflict with the 2050 emissions reductions targets set forth in Executive Order S-3-05 would remain a significant and unavoidable impact of the Project.

## **V. PROJECT ALTERNATIVES**

CEQA requires that an EIR assess feasible alternatives or mitigation measures that may substantially lessen the significant effects of projects prior to approval (CEQA Section 21002). With the exception of the No Project alternative, the specific alternatives or types of alternatives that must be assessed are not specified (CEQA Guidelines Section 15126.6). A range of potential alternatives to the Project shall include those that could feasibly accomplish most of the basic

purposes of the Project and could avoid or substantially lessen one or more significant effects of the Project (CEQA Guidelines Section 15126.6(a)).

The Project has been designed to meet the following objectives:

- (1) Responsibly expand the Facebook Campus within Menlo Park to create a diverse and economically resilient workspace for Facebook and its affiliated companies, such as Instagram, Oculus, and [Internet.org](https://www.internet.org/).
- (2) Provide high-quality, highly sustainable office space at a permitted development density of 0.45 FAR that incorporates a robust TDM Plan.
- (3) Develop a highly connected Campus that provides flexible workspace.
- (4) Minimize traffic and greenhouse gas emissions by providing multiple transportation options for employees.
- (5) Use highly sustainable design techniques to promote energy and water efficiency.
- (6) Connect the Campus to the community by including publicly accessible open space.
- (7) Create a bicycle-/pedestrian-friendly environment that enhances connectivity between the Belle Haven neighborhood and Bayfront Park and the Bay Trail.
- (8) Rehabilitate an existing industrial site, and remediate hazardous materials where appropriate.
- (9) Provide new green spaces and additional habitat and landscaped areas with native drought-tolerant plant species.
- (10) Create an accessible Campus that enhances connectivity, and promotes a sense of transition between the Belle Haven neighborhood and the Bay.
- (11) Generate new revenue for the City and other public entities, including potential transit occupancy tax revenue from a potential new limited service hotel.

The EIR included several project alternatives. The City Council finds that the EIR incorporated the required No Project alternative and all feasible alternatives that could accomplish the most basic project objectives and could avoid or substantially lessen one or more significant effects. The City hereby concludes that the EIR sets forth a reasonable range of alternatives to the Project so as to foster informed public participation and informed decision making. The EIR also identified several alternatives that were considered during the preparation of the EIR, including alternative locations and an on-site housing alternative, among others, which were rejected as infeasible and not analyzed further in the EIR. The City Council funds

that all reasonable alternatives were reviewed, analyzed and disclosed in the review process of the EIR and for the decision on the Project. The City evaluated the alternatives listed below.

#### **A. No Project Alternative.**

No additional construction would occur at the Project site with implementation of the No Project Alternative. Under a separate project, Buildings 307–309 would be demolished, but the No Project Alternative would retain Buildings 301–306. Not including Building 23, the Project site would encompass approximately 511,700 gsf of building area. In order to meet the definition of a “No Project” alternative under CEQA, the No Project Alternative would not be permitted discretionary approvals, entitlements, or other environmental reviews. Therefore, Buildings 301–306 could not be occupied and would remain vacant. No additional employees would be added to the Project site. The same amount of parking spaces would be provided at the Project site as under existing conditions (1,690 spaces), which could be used by the occupants of Building 23. The multi-use bicycle/pedestrian bridge over SR 84 would not be constructed, and the publicly accessible open space would not be installed. Access to the Project site would remain the same, and no new site access points would be constructed.

The No Project Alternative would avoid most of the environmental impacts of the Project.

**FINDINGS:** The No Project Alternative would not meet any of the primary objectives of expanding the Facebook Campus as a diverse and economically resilient workspace for Facebook and its affiliated companies and as a bicycle- and pedestrian-friendly, sustainable, flexible workspace that is highly interconnected and connected to the Belle Haven neighborhood and the Bay and that generates new revenue for the City and other public entities and that rehabilitates an existing industrial site. Instead, the Project site would remain unoccupied and the site would be underutilized. The No Project Alternative would not demolish the existing buildings at the Project site (which would remain vacant) and would not construct the proposed buildings, the publicly accessible open space, and bicycle and pedestrian paths. No connection between the Belle Haven neighborhood and the Bay Trail and Bayfront Park would be built. Additionally, the No Project Alternative would not create jobs; instead of approximately 6,550 jobs under the Project, the No Project Alternative would provide no new jobs at the Project site (not including Building 23). The tax revenues for the City would stay the same rather than increase with implementation of the Project. As such, the No Project Alternative would not meet the Project objectives.

#### **B. Reduced Intensity Alternative.**

To determine how large an office development on the Project site could be before it triggered significant impacts, a sensitivity analysis was conducted which concluded an approximately 30 percent reduction in daily trips and employees

would be necessary. The Reduced Intensity Alternative therefore includes a 30 percent reduction in the amount of office floor area and number of office employees compared to the Project. This would equate to approximately 673,680 gsf of office building area. The Reduced Intensity Alternative would accommodate approximately 4,630 employees, with 4,480 Facebook employees and 150 hotel employees. As with the Project, the Reduced Intensity Alternative would include office uses and a hotel. The Reduced Intensity Alternative would result in an equivalent decrease in daily trips, which would equate to a trip cap of approximately 12,000 daily trips as a result of the Reduced Intensity Alternative. Including Buildings 20 and 23, the total site-wide trip cap would be approximately 22,127 daily trips.

It is assumed that the site plan for the Reduced Intensity Alternative would be similar to that of the Project, with a reduced building footprint. Because the building footprints would be smaller, all footprint-based impacts would be the same as or less than those of the Project. The maximum building height would be the same as under the Project at 75 feet. Accordingly, the Reduced Intensity Alternative would still require rezoning of the Project site to a M-2(X) (Conditional Development Overlay) zoning district with a corresponding conditional development permit in order to increase the permitted building heights and define the development standards for the Project

**FINDINGS:** The Reduced Intensity Alternative would not fully achieve many of the Project Sponsor's objectives because the reductions in office space would not allow the Project Sponsor to meet its business objectives for the expansion or operate at its desired level. It also would not achieve the objective of a developing high-quality office space at a permitted development density of 0.45 floor area ratio.

### **C. Environmentally Superior Alternative**

The *CEQA Guidelines* state that an EIR shall identify an environmentally superior alternative. If the environmentally superior alternative is the "No Project" alternative, the EIR shall also identify an environmentally superior alternative among the other alternatives (Section 15126.6(e)(2)). Based upon the previous discussion, the environmentally superior alternative would be the Reduced Project Alternative, which would avoid most of the significant and unavoidable impacts of the Project.

## **VI. STATEMENT OF OVERRIDING CONSIDERATIONS**

The City Council adopts and makes the following Statement of Overriding Considerations regarding the significant unavoidable impacts of the Project. After review of the entire administrative record, the City Council finds that, pursuant to CEQA section 21081(b) and CEQA Guidelines section 15093, specific economic, legal, social, technological and other benefits of the Project outweigh the Project's unavoidable adverse impacts and the City Council finds that the significant and unavoidable adverse impacts are acceptable in light of the Project's benefits.

### **A. Significant Unavoidable Impacts**

With respect to the foregoing findings and in recognition of those facts that are included in the entire administrative record, the City has determined that the Project would result in significant unavoidable transportation impacts to specific study intersections, Routes of Regional Significance, roadway segments, and delay to transit vehicles. Significant and unavoidable impacts would also occur associated with conflicts with an Executive Order S-3-05 related to GHG emissions.

The City hereby finds that, where possible, changes or alterations have been required in or incorporated into the Project that substantially lessen the significant environmental effects identified in the Final EIR. The City further finds that there are no additional feasible mitigation measures that could be imposed to further reduce or eliminate the significant and unavoidable impacts listed above.

## **B. Overriding Considerations**

The City Council finds that each of the overriding considerations set forth below constitutes a separate and independent ground for a finding that the significant and unavoidable impacts identified above are outweighed by the Project's benefits and are acceptable in light of the benefits of the Project, based on the findings below:

- The City has made a reasonable and good faith effort to eliminate or substantially mitigate the potential impacts resulting from the Project, as described above.
- All Mitigation Measures recommended in the EIR have been incorporated into the Project and will be implemented through the MMRP.
- All alternatives to the Project, set forth in the EIR, reduce the project's significant and unavoidable impacts to less than significant but do not achieve the project objectives, and the City finds that project objectives and/or specific economic, social and other benefits outweigh any environmental benefits of the alternatives.
- In accordance with CEQA Guidelines Section 15093, the City has, in determining whether or not to approve the Project, balanced the economic, legal, social, technological, and other benefits, including region-wide or statewide environmental benefits of the Project against these unavoidable environmental risks, and has found that the benefits of the Project outweigh the unavoidable adverse environmental effects. The following statements specify the reasons why, in the City's judgment, the benefits of the Project outweigh its unavoidable environmental risks. The City also finds that any one of the following reasons for approval cited below is sufficient to justify approval of the Project.

**(a) Economic Benefits**

- The Project would redevelop an underutilized industrial site that currently contains aging office and light industrial buildings with a highly-sustainable, high-quality office development and a new hotel that supports business growth in the City.
- The Project would include a high quality office and a new hotel located adjacent to the Bayfront Expressway and contribute to the revitalization of the M-2 Area.
- The Project would generate revenue for the City through development fees applicable to the Project, including payment of impact fees.
- The Project would generate revenue for the City through increased property tax revenue, transient occupancy tax revenue, and tax revenue from commercial development.
- The Project provides a recurring Public Benefit Payment of \$300,000 per year for twenty years (increased by CPI every five years) to the City's General Fund, which will help the City maintain a healthy fiscal balance to provide its residents with high-quality services.
- The Project provides a payment of four times the amount of sales tax historically captured from the site (totaling \$336,000 per year) until two years after the existing tenants at the site vacate, and then guarantees, \$1.25 million per year in transient occupancy taxes.
- The Project provides a commitment to pay a 1% higher TOT rate for the hotel than would otherwise be applicable.
- The Project would maximize the capture of sales and use taxes arising from construction materials, furniture, equipment and personal property acquired for the Project.
- The Project would provide a property tax guarantee assuring the City that the assessed value of the property following completion of the project will be \$695 million (which is projected to generate new property taxes of approximately \$550,000 per year for the City's General Fund).
- The Project would result in increased revenue from the City's Utility User's Tax, as well as increased revenue from the City's Utility User's Tax generated by existing Building 20.
- Development of the Project would create approximately 6,550 new jobs.

**(b) Social Benefits**

- The Project would lead to the redevelopment of an underutilized site served by existing transportation and utility infrastructure.
- The Project would meet the City's land use planning goals and development strategies for the M-2 Area, and promote pedestrian and bicyclist connections by creating on-site and off-site pedestrian and bicycle amenities, and improving connections to off-site pedestrian, bicycle, and transit networks.
- The Project would improve the overall aesthetic and visual quality of the M-2 Area.
- The Project would provide a landscaped site and includes new landscape amenities and open space areas, well-designed publicly visible and accessible open space, and planting of almost 3,000 new trees on-site.

**(c) Transportation and Infrastructure Benefits**

- The Project would provide \$1 million to fund future recommendations arising from the Dumbarton Corridor Study, and a commitment to participate in new projects that arise from the Dumbarton Corridor Study.
- The Project would provide \$100,000 towards a Transportation Management Association program serving the areas surrounding the Facebook campus and to share its best TDM practices with the City and other local landowners and employers, which would provide regional benefits by allowing for additional shuttle destinations, landowner participants, revenue sources, programs, and areas served to be added over time.
- The Project would provide a \$700,000 funding commitment for the design of a pedestrian/bicycle path between East Palo Alto and Redwood City Caltrain Station.
- The Project would provide \$1 million in funding to sponsor a partnership with the cities of Menlo Park and East Palo Alto, San Mateo County and Santa Clara County, and regional stakeholders to convene a regional forum to evaluate innovative ways that the recommendations of the Dumbarton Corridor Study could be executed quickly and with minimal delays, and to help develop design, operational, and construction strategies to implement recommendations following the forum.
- The Project would result in the completion of the Chilco Street Improvements, including extensive streetscape, pedestrian and bicycle improvements.

**(d) Housing Benefits**

- The –Project Sponsor will explore opportunities and identify projects that can be directly financed and implemented as soon as is feasible in order to leverage approximately \$6.5 million in BMR housing fees to create the maximum number of affordable units feasible.
- The Project would provide \$350,000 in funding to conduct a *Housing Inventory and Local Supply Study* in partnership with the City of Menlo Park and East Palo Alto to assess local housing conditions and facilitate development of a regional housing strategy.
- The Project would provide \$1.5 million in funding to establish a *Housing Innovation Fund* to identify near-term actions that can be taken within the local community as a direct outcome of the *Housing Inventory and Local Supply Study*.
- The Project would provide \$1 million to establish a pilot *Housing Preservation Fund* to identify and purchase housing in the immediate vicinity of our campus to protect at-risk populations. The Project would initiate a pilot *Workforce Housing Program* in the Belle Haven community that will reduce rents for up to 22 units of workforce housing at 777 Hamilton Avenue for five years (up to \$430,000 per year for five years, or \$2.15 million) with priority being given to local teachers.
- The Project Sponsor would commit to the planning and design of at least 1,500 housing units on the Menlo Science & Technology Park site consistent with the General Plan Update and agreement that any residential project on the Menlo Science & Technology Park site must include 15% BMR units and/or workforce housing units (regardless of whether the units are for sale or rentals).

**(e) Local Community Benefits**

- The Project would provide a new, two-acre publicly accessible open space to be privately maintained by Facebook for use by the community, with space for programming and events such as farmer’s markets, movie-nights and food truck festivals.
- The Project would provide a new, publicly-accessible, multi-use bridge across the Bayfront Expressway that will provide a safe connection from Belle Haven to the Bay.
- The Project would fund improvements for the local Belle Haven Pool (\$60,000 per year for five years) and the local community fund (\$100,000 per year for five years).

- The Project would result in the establishment of a new scholarship program for students residing in Menlo Park and East Palo Alto (\$100,000 per year for ten years).
- The Project would provide \$1 million in funding to the Bedwell Bayfront Park Maintenance Fund for maintenance and operations.

**(f) Region-wide or Statewide Environmental Benefits**

- The Project would provide highly-sustainable buildings constructed to meet the intent of LEED® Gold design standard, and will increase energy and water efficiency by developing a recycled water system on-site to reduce water demand by approximately 20 million gallons annually, reducing passive solar heat gain and heat loss, improving energy performance through innovative mechanical design, and generating on-site solar energy through the installation of solar panels.
- The Project would provide \$25,000 to help fund a feasibility study for a Bayfront Area-wide recycled water system.
- The Project would fund a docent for two years, a new interpretive display associated with the multi-use bridge, realignment of the Bay Trail and a snowy plover enhancement study to benefit Bedwell Bayfront Park.
- The Project would promote compact growth by increasing job opportunities at a location near existing transportation and utility infrastructure.
- The Project is consistent with the City's Climate Action Plan and would support the City's efforts to reduce dependency on fossil fuels and nonrenewable energy, decrease its share of GHG emissions and contributions to global climate change, and help make the City a more attractive place to live.
- The Project's trip cap and TDM program would be designed to reduce parking, driving, and pollution, would encourage workers to commute using transit and other alternatives to single-occupancy vehicles, and would be enforceable through conditions of approval adopted and enforced by the City.

Having identified the significant environmental effects of the Project, adopted all feasible mitigation measures, identified all unavoidable significant impacts, and balanced the specific economic, legal, social, technological, and other benefits of the Project, the City Council has determined that the significant and unavoidable adverse impacts are outweighed by the benefits and may be considered acceptable, and therefore approves the Project as described herein.

**VII. ADOPTION OF THE MMRP**

The City Council hereby adopts the mitigation measures set forth for the Project in the Final EIR and the MMRP attached hereto as Exhibit A and incorporated herein by this reference.

**VIII. SEVERABILITY**

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Pamela Aguilar, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the first day of November, 2016, by the following votes:

AYES: Carlton, Cline, Keith, Mueller, Ohtaki  
NOES: None  
ABSENT: None  
ABSTAIN: None

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this first day of November, 2016.



Pamela Aguilar, CMC  
City Clerk

# Facebook Campus Expansion Project Mitigation Monitoring and Reporting Program

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## Introduction

The California Environmental Quality Act (CEQA) requires the adoption of feasible mitigation measures to reduce the severity and magnitude of significant environmental impacts associated with project development. The Environmental Impact Report (EIR) prepared and certified for the proposed Facebook Campus Expansion Project (Project) includes all feasible mitigation measures to reduce the potential environmental effects of the Project.

CEQA also requires reporting on and monitoring of mitigation measures adopted as part of the environmental review process (Public Resources Code Section 21081.6). This Mitigation Monitoring and Reporting Program (MMRP) is designed to aid the City of Menlo Park in its implementation and monitoring of measures adopted from the certified EIR.

The mitigation measures in this MMRP are assigned the same number they had in the EIR. The MMRP is presented in table format and describes the actions that must take place to implement each mitigation measure, the timing of those actions, the entities responsible for implementing and monitoring the actions, and verification of compliance. Additional information is provided in the certified EIR for the Project.

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b>AESTHETICS</b>				
<b><i>IMPACT BEING ADDRESSED: New Sources of Light and Glare. The Project could create a new source of substantial light or glare that could adversely affect daytime or nighttime views in the area. (AES-3)</i></b>				
<p><i>AES-3.1: Design Lighting to Meet Minimum Safety and Security Standards.</i> Concurrent with the building permit submittal, the Project Sponsor shall incorporate lighting design specifications to meet minimum safety and security standards. The comprehensive site lighting plans shall be subject to review and approval by the City’s Community Development Department, Planning Division, prior to building permit issuance for the first building on the site.</p> <p>The following measures shall be included in all lighting plans.</p> <ul style="list-style-type: none"> <li>• Luminaries shall be designed with cutoff-type fixtures or features that cast low-angle illumination to minimize incidental spillover of light onto adjacent private properties. Fixtures that shine light upward or horizontally shall not spill any light onto adjacent private properties.</li> <li>• Luminaries shall provide accurate color rendering and natural light qualities. Low-pressure sodium and high-pressure sodium fixtures that are not color-corrected shall not be used, except as part of an approved sign or landscape plan.</li> <li>• Luminary mountings shall be downcast and pole heights minimized to reduce the potential for backscatter into the nighttime sky and incidental spillover onto adjacent properties and undeveloped open space. Light poles shall be no higher than 20 feet. Luminary mountings shall be treated with non-glare finishes.</li> </ul>	Incorporate lighting design specification to meet minimum safety and security standards.	Submittal of lighting plan prior to building permit issuance for the first building on the site.	Project Sponsor	City of Menlo Park Community Development Department (CDD)
<p><i>AES-3.2: Treat Reflective Surfaces.</i> The Project Sponsor shall ensure the application of a low-emissivity coating on exterior glass surfaces of proposed structures. The low-emissivity coating shall reduce the reflection of visible light that strikes the exterior glass and prevent interior light from being emitted brightly through the glass.</p>	Apply low-emissivity coating on exterior glass surfaces of the proposed structures.	Concurrent with building permit application.	Project Sponsor	CDD

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b>TRANSPORTATION</b>				
<b><i>IMPACT BEING ADDRESSED: Impacts on Peak-Hour Traffic at Study Intersections under Background Plus-Project Conditions. Increases in traffic associated with the Project would result in increased delays during peak hour, causing significant and unavoidable impacts on the operation of study intersections. (TRA-1)</i></b>				
<i>TRA-1.1 Provide Increased Traffic Capacity under Background-Plus Project Conditions.</i>	See below	See below	See below	See below
<i>a. Sand Hill Road &amp; I-280 Northbound On-Ramp (#2)</i> With implementation of Mitigation Measure TRA-1.2, the impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See below	See below	See below
<i>b. Willow Road &amp; Hamilton Avenue (#36)</i> Mitigation measures TRA-3.1 and TRA-3.2 to discourage cut-through traffic in the Belle Haven neighborhood and Mitigation Measure TRA-1.2 would reduce this impact, but not to a less-than-significant level. This impact would remain significant and unavoidable.	Implement Mitigation Measures TRA-1.2, TRA-3.1, and TRA-3.2.	See below	See below	See below
<i>c. Bayfront Expressway &amp; Willow Road (#37)</i> The Project Sponsor shall be required to design and construct a lengthened eastbound right-turn pocket on Bayfront Expressway to Willow Road and a dedicated receiving lane on Bayfront Expressway for northbound right-turn traffic from Willow Road. Because the improvements would be under Caltrans jurisdiction, the Project Sponsor would be required to coordinate with Caltrans for review and approval of the improvements. The potential mitigation options described above are not under the control of the City, and thus, the City cannot guarantee their implementation.  In addition, with implementation of Mitigation Measure TRA-1.2, this impact would be reduced but would remain significant and unavoidable.	Prepare conceptual plan and cost estimate.  Provide bond for improvements.  Submit complete plans to construct intersection improvements to City.	Within 90 days of the effective date of the Development Agreement (DA).  Within 180 days of the effective date of the DA.  Within 180 days of the effective date of the DA.	Project Sponsor  Project Sponsor  Project Sponsor	City of Menlo Park Public Works Department (PW)  PW  PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
	Complete and submit application to Caltrans.	Within 30 days of City approval of plans.	Project Sponsor	PW, Caltrans
	Construct improvements.	Within 180 days of Caltrans approval	Project Sponsor	PW, Caltrans
<p><i>d. Bayfront Expressway &amp; University Avenue (#38)</i> The Project Sponsor shall be required to initiate design concepts through a Project Study Report (PSR), or other appropriate development document, for potential future grade separation at the intersection of Bayfront Expressway and University Avenue. Because the intersection would be under Caltrans jurisdiction, the Project Sponsor would be required to coordinate with Caltrans and the City. This potential mitigation is not under the control of the City, and the impact would remain significant and unavoidable.</p>	Confirm Caltrans oversight process requirements and scope of work	Within 90 days of the effective date of the Development Agreement	Project Sponsor	PW, Caltrans
	Complete PSR (or appropriate document) with City and Caltrans oversight	Prior to occupancy of Building 21	Project Sponsor	PW, Caltrans
<p><i>e. Bayfront Expressway &amp; Chilco Street (#40)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.</p>	Implement Mitigation Measure TRA-1.2.	See below	See below	See below
<p><i>f. Chilco Street &amp; Constitution Drive (#45)</i> The proposed mitigation for peak-hour impacts at the intersection of Chilco Street and Constitution Drive would provide the following elements to accommodate inbound a.m. and outbound p.m. traffic movements:</p> <ul style="list-style-type: none"> <li>• Installation of a traffic signal and signalized pedestrian crossings on all four legs of the intersection.</li> <li>• Provision of three southbound lanes on the one-block segment of</li> </ul>	Prepare conceptual plan and cost estimate.	Within 90 days of the effective date of the Development Agreement (DA).	Project Sponsor	PW
	Provide bond for improvements.	Within 180 days of the	Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>Chilco Street, between Bayfront Expressway and Constitution Drive, to include two southbound left-turn lanes to accommodate the volume of left-turning vehicles entering the Project site. In addition, during the a.m. peak hour, provision of a “split-phase” signal operation on Chilco Street is recommended.</p> <ul style="list-style-type: none"> <li>• Provision of a northbound left-turn lane on Chilco Street approaching Constitution Drive.</li> <li>• Provision of two outbound lanes on Chilco Street exiting the Project site.</li> </ul> <p>With these improvements, this impact would be reduced to a less-than-significant level.</p>	<p>Submit complete plans to construct intersection improvements to City.</p> <p>Construct improvements.</p>	<p>effective date of the DA.</p> <p>Within 180 days of the effective date of the DA.</p> <p>Prior to occupancy of Building 22.</p>	<p>Project Sponsor</p> <p>Project Sponsor</p>	<p>PW</p> <p>PW</p>
<p><i>g. University Avenue &amp; Adams Drive (#47)</i></p> <p>The impact under background plus-Project conditions would be significant and unavoidable. Installation of a traffic signal at this location would be recommended under 2040 cumulative conditions with the proposed General Plan. Therefore, if the proposed General Plan is adopted, this impact could be mitigated to less-than-significant levels (see Mitigation Measure TRA-13.1).</p>	<p>Implement Mitigation Measure TRA-13.1</p>	<p>See below</p>	<p>See below</p>	<p>See below</p>
<p><i>h. Jefferson Drive &amp; Constitution Drive (#50)</i></p> <p>Because this impact would be limited to affecting a side-street driveway that serves just 15 vehicle trips during the p.m. peak hour, this impact would be less than significant, and no mitigation is required.</p>	<p>N/A</p>	<p>N/A</p>	<p>N/A</p>	<p>N/A</p>
<p><i>i. University Avenue &amp; US 101 Southbound Ramps (#56)</i></p> <p>With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.</p>	<p>Implement Mitigation Measure TRA-1.2.</p>	<p>See below</p>	<p>See below</p>	<p>See below</p>
<p><i>j. University Avenue &amp; Woodland Avenue (#57)</i></p> <p>With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.</p>	<p>Implement Mitigation Measure TRA-1.2.</p>	<p>See below</p>	<p>See below</p>	<p>See below</p>
<p><i>k. Chilco Street &amp; Hamilton Avenue (#60)</i></p>	<p>Implement</p>	<p>See below</p>	<p>See below</p>	<p>See below</p>

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
Mitigation Measures TRA-1.2, TRA-3.1, and TRA-3.2 would reduce this impact, but not to a less-than-significant level. This impact would remain significant and unavoidable.	Mitigation Measure TRA-1.2, TRA-3.1, and TRA-3.2.			
<p><i>l. Bayfront Expressway &amp; Facebook Building 20 Entrance (#65)</i></p> <p>The impact would be reduced to less-than-significant levels by providing a two-lane westbound left-turn pocket at the adjacent intersection of Bayfront Expressway and the Building 21 entrance. However, the right-of-way along Bayfront Expressway is constrained by the wetlands located adjacent to the roadway; therefore, this mitigation measure may not be feasible. Alternatively, the Project Sponsor shall be required to conduct a micro-simulation evaluation as part of the proposal to install a new traffic signal at the proposed entrance to Building 21 and ensure that queues do not extend onto the Bayfront Expressway at the Building 20 or 21 intersections (see Mitigation Measure TRA-1.1m, below). The intersection is under the jurisdiction of Caltrans; therefore, the City cannot guarantee that this improvement would be implemented.</p> <p>Mitigation Measure TRA-1.2 (described below) would reduce net Project vehicle trip generation during both peak hours, but the increase in eastbound traffic on Bayfront Expressway between Chilco Street and Willow Road would still be anticipated to result in a significant impact at this intersection. Therefore, the impact would remain significant and unavoidable.</p>	Prepare microsimulation evaluation and submit to City for review. Prepare concept plan and cost estimate of proposed improvements.	Within 90 days of the effective date of the Development Agreement (DA).	Project Sponsor	City of Menlo Park Public Works Department (PW)
	Provide bond for improvements.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Submit microsimulation evaluation to Caltrans as part of proposal to install new traffic signal at Building 21.	Within 30 days of City approval of evaluation and plans.	Project Sponsor	PW, Caltrans
	Construct improvements.	Within 180 days of Caltrans approval	Project Sponsor	PW, Caltrans

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
	Implement Mitigation Measures TRA-3.1 and TRA-1.2.	See below	See below	See below
<p><i>m. Bayfront Expressway &amp; Proposed Building 21 Entrance (#66)</i>                      The proposed mitigation is the provision of a two-lane left-turn pocket for northbound vehicles that would enter Building 21 from Bayfront Expressway. However, the right-of-way along Bayfront Expressway is constrained by the wetlands located adjacent to the roadway; therefore, this mitigation measure may not be feasible. Alternatively, the Project Sponsor shall be required to conduct a micro-simulation evaluation as part of the proposal to install a new traffic signal at this location and ensure that queues do not extend onto Bayfront Expressway at either intersection (see Mitigation Measure TRA-1.1l, above) while maintaining an acceptable intersection LOS of D or better. With the proposed mitigation, if feasible, the impact would be less than significant during the a.m. peak hour.</p> <p>During the p.m. peak hour, implementation of Mitigation Measure TRA-1.2 would reduce the impact to less than significant levels. Because the feasibility of the a.m. peak-hour mitigation described above has not yet been confirmed, the impact would remain significant and unavoidable.</p>	Implement Mitigation Measures TRA-1.1l and TRA-1.2.	See above (TRA-1.1l) and below (TRA-1.2)	See above (TRA-1.1l) and below (TRA-1.2)	See above (TRA-1.1l) and below (TRA-1.2)
<p><i>TRA-1.2: Reduce the Peak-Hour Share of Vehicle Trips Allowable under the Trip Cap, for both the Project Site and Buildings 10-19 to no more than 50 Percent of Allowable Vehicle Trips During each 2-Hour Peak Commute Period.</i> The proposed mitigation would reduce the maximum number of allowable peak-hour vehicle trips to no more than 50 percent of the 2-hour peak-period vehicle trip cap for both the Project site and Buildings 10-19.</p>	Implement the trip cap monitoring and enforcement policy that applies to the Project.	Prior to occupancy of Building 21.	Project Sponsor	PW
	Amend the CDP	Prior to	Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
	and DA for Buildings 10-19 (also known as East Campus) to implement this measure.	occupancy of Building 21.		
<b><i>IMPACT BEING ADDRESSED: Impacts on Routes of Regional Significance under Background Plus Project Conditions. Some Routes of Regional Significance would operate at or below their LOS threshold with the addition of Project trips, and Project traffic would exceed the allowable 1 percent threshold resulting in significant and unavoidable impacts. (TRA-2)</i></b>				
Implement Mitigation Measure TRA-1.2, plus: <i>TRA-2.1: Implement Improvements to Routes of Regional Significance to Address Background Plus-Project Effects.</i> Providing additional travel lanes would increase segment capacity but would not be feasible on all segments given available right-of-way widths and both downstream and downstream capacity limitations on facilities such as US 101 and the Dumbarton Bridge. In addition, the routes are under the control of Caltrans and the City cannot guarantee mitigation. Therefore, these impacts would remain significant and unavoidable.	N/A	N/A	N/A	N/A
<b><i>IMPACT BEING ADDRESSED: Increase in Daily Traffic Volumes on Roadway Segments Under Background Plus-Project Conditions. Increases in daily traffic associated with the Project under near term plus-Project Conditions would result in increased ADT volumes on Project area roadway segments, resulting in significant and unavoidable impacts. (TRA-3)</i></b>				
<i>TRA-3.1: Provide Measures to Reduce Cut-Through Traffic in the Belle Haven Neighborhood via Chilco Street (South of the Dumbarton Rail Corridor), Newbridge Street, and Ivy Drive.</i> The Project Sponsor shall provide measures to prevent cut-through traffic, which could include prohibiting left-turns exiting the Project site via Chilco Street during the p.m. peak period. The provision of physical traffic calming measures could also be included, where such measures would not affect emergency access and/or transit service, subject to community and City approval. Because community members and other potentially affected stakeholders may be affected by such improvements, the Project Sponsor shall fund a Neighborhood Traffic Plan to identify appropriate measures for reducing	Coordinate with the City and local stakeholders to fund, develop and implement a Neighborhood Traffic Plan.	Funding provided within 90 days of effective date of DA.  Develop plan within 180 days of effective date of DA.	Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
cut-through traffic.		Implement measures prior to occupancy of Building 22.		
<i>TRA-3.2: Provide Multi-Modal improvements on study segments that would be potentially impacted by increased ADT. The Project Sponsor shall provide measures to improve mobility options via walking, bicycling, and transit, consistent with the City’s complete streets goals, as described in Mitigation Measures TRA-4.1 and TRA-5.1.</i>	Implement Mitigation Measures TRA-4.1 and TRA-5.1	See below	See below	See below
<b><i>IMPACT BEING ADDRESSED: Pedestrian Connections Under Background Plus-Project Conditions. The Project would result in a lack of adequate pedestrian connections to the area circulation system under background plus-Project conditions, resulting in a potentially significant impact. (TRA-4)</i></b>				
<p><i>TRA-4.1: Provide External Pedestrian Connections to the Area Circulation System and Adjacent Land Uses.</i></p> <p><i>a. Constitution Drive</i></p> <p>The Project Sponsor shall construct sidewalks along one side of Constitution Drive between Chilco Street and Chrysler Drive and pedestrian crosswalks and curb ramps at Chilco Street &amp; Constitution Drive and Jefferson Drive &amp; Constitution Drive.</p>	Prepare conceptual plan and cost estimate.	Within 90 days of the effective date of the Development Agreement (DA).	Project Sponsor	PW
	Provide bond for improvements.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Submit complete plans to construct intersection improvements to City.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Construct		Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
	improvements.	Prior to occupancy of Building 21.		
<b><i>IMPACT BEING ADDRESSED: Bicycle Connections Under Background Plus-Project Conditions. The Project would result in a lack of adequate bicycle connections to the area circulation system under background plus-Project conditions, resulting in potentially significant impacts. (TRA-5)</i></b>				
<i>TRA-5.1: Provide bicycle connections to the area circulation system and adjacent land uses.</i>	See below	See below	See below	See below
<p><i>a. Hamilton Avenue</i> The Project Sponsor shall install bicycle boulevard treatments on Hamilton Avenue between Chilco Street and the pedestrian/bicycle overcrossing of US 101, including stop-sign modifications, lane markings, signage, and wayfinding elements.</p>	Prepare conceptual plan and cost estimate.	Within 90 days of the effective date of the Development Agreement (DA).	Project Sponsor	PW
	Provide bond for improvements.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Submit complete plans to construct intersection improvements to City.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Construct improvements.	Prior to occupancy of Building 22.	Project Sponsor	PW
<i>b. Northbound Access to the Project Site for Bicyclists</i>	Prepare	Within 90 days	Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>The Project Sponsor shall provide facilities for northbound bicyclists to cross Willow Road and access the Project site, thereby minimizing vehicle/bicycle conflicts. Such facilities may include a two-stage left-turn queue box, or similar improvements, to accommodate northbound left-turn movements for bicyclists at the Willow Road/Hamilton Drive intersection from the curbside bicycle lane, in conjunction with a Class I pathway or similar improvements for northbound bicyclists to the travel on the west side of Willow Road between Hamilton Avenue and the Project site.</p>	conceptual plan and cost estimate.	of the effective date of the Development Agreement (DA).	Project Sponsor	PW
	Provide bond for improvements.	Within 180 days of the effective date of the DA.		
	Submit complete plans to construction intersection improvements to City.	Within 180 days of the effective date of the DA.	Project Sponsor	PW
	Complete and submit application to Caltrans.	Within 30 days of City approval of plans.	Project Sponsor	PW, Caltrans
	Construct improvements.	Prior to occupancy of Building 22.	Project Sponsor	PW, Caltrans
<p><b><i>IMPACT BEING ADDRESSED: Pedestrian and/or Bicycle/Vehicle Conflicts. The Project design would cause increased potential for pedestrian and/or bicycle/vehicle conflicts, resulting in potentially significant impacts. (TRA-6)</i></b></p>				
<p><i>TRA-6.1: Refine the Project Design to Minimize Conflicting Movements between Bicycles, Pedestrians, and Other Travel Modes within the Project Site. The Project Sponsor shall work to minimize conflicts to the satisfaction of the transportation manager prior to approval of the site</i></p>	Refine the Project site plan to satisfaction of City's	Prior to issuance of building permit for first	Project Sponsor	PW

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
plan for construction.	Transportation Manager	phase of Building 21		
<b><i>IMPACT BEING ADDRESSED: Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Existing General Plan Plus-Project Conditions. Increases in peak-hour vehicle traffic associated with the Project would result in increased delays during AM and PM peak hours causing significant and unavoidable impacts on the operation of study intersections under Cumulative 2040 Conditions with the Existing General Plan. (TRA-10)</i></b>				
<i>TRA-10.1: Provide Increased Traffic Capacity under Cumulative 2040 Existing General Plan plus-Project Conditions.</i>	See below	See below	See below	See below
<i>a. Sand Hill Road and I-280 Northbound Off-Ramp (#1)</i> With implementation of Mitigation Measure TRA-1.2, this cumulative impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>b. El Camino Real &amp; Glenwood Avenue (#25)</i> The provision of a dedicated right-turn lane on Glenwood Avenue, where it approaches El Camino Real, is identified in the City’s TIF program. The Project Sponsor’s payment of the TIF shall partially mitigate this impact. The provision of one additional through lane on Glenwood Avenue would be needed to improve LOS to an acceptable LOS of D and fully mitigate this impact. However, the provision of an additional through lane is not feasible given the right-of-way constraints. Therefore, this impact would be considered significant and unavoidable under cumulative 2040 existing General Plan plus-Project conditions.	Pay the TIF identified in the City’s TIF program.	Prior to issuance of building permit for first phase of each building.	Project Sponsor	PW
<i>c. El Camino Real &amp; Ravenswood Avenue-Menlo Avenue (#28)</i> Potential mitigation would be to provide a right-turn pocket on Menlo Avenue, where it approaches El Camino Real, and a third through lane on El Camino Real in both the northbound and southbound directions. These measures are identified in the City’s TIF program. The Project Sponsor’s payment of the TIF shall mitigate this impact. With implementation of this mitigation measure, the intersection would operate acceptably, and the impact would be reduced to a less-than-significant level.	Pay the TIF identified in the City’s TIF program.	Prior to issuance of building permit for first phase of each building.	Project Sponsor	PW
<i>d. Willow Road &amp; Hamilton Avenue (#36)</i> The Project impact was identified under background plus-Project	Implement	See above	See above	See above

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
conditions (see TRA-1.1b). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	TRA-1.1b.			
<i>e. Bayfront Expressway &amp; Willow Road (#37)</i> The Project impact was identified under background plus-Project conditions. As discussed in Mitigation Measure TRA-1.1c, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1c.	See above	See above	See above
<i>f. Bayfront Expressway &amp; University Avenue (#38)</i> The Project impact was identified under background plus-Project conditions. As discussed in Mitigation Measure TRA-1.1d, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1d.	See above	See above	See above
<i>g. Chilco Street &amp; Constitution Drive (#45)</i> This impact, identified under background plus-Project conditions, pertains to the design of the Project entrance, as described above in Mitigation Measure TRA-1.1f. With implementation of this mitigation measure, the intersection would operate acceptably, and this impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.1f.	See above	See above	See above
<i>h. Chrysler Drive &amp; Constitution Drive (#46)</i> With implementation of Mitigation Measure TRA-1.2, the cumulative impact at this location would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>i. University Avenue &amp; Adams Drive (#47)</i> The Project impact under cumulative 2040 existing General Plan plus-Project conditions would remain significant and unavoidable. Installation of a traffic signal at this location would be recommended under 2040 cumulative conditions with the proposed General Plan. Therefore, if the proposed General Plan is adopted, this impact could be mitigated to a less-than-significant level (see Mitigation Measure TRA-13.1i).	Implement Mitigation Measure TRA-13.1	See below	See below	See below
<i>j. University Avenue &amp; Bay Road (#51)</i>	Implement	See above	See above	See above

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.	Mitigation Measure TRA-1.2.			
<i>k. University Avenue &amp; Donohoe Street (#54)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced, but the increase in delay would remain significant and unavoidable.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>l. University Avenue &amp; US 101 Southbound Ramps (#56)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>m. University Avenue &amp; Woodland Avenue (#57)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>n. Chilco Street &amp; Hamilton Avenue (#60)</i> The Project impact was identified under background plus-Project conditions (see TRA-1.1k). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1k.	See above	See above	See above
<i>o. Bayfront Expressway &amp; Facebook Building 20 Entrance (#65)</i> The Project impact was identified under background plus-Project conditions (see TRA-1.1l). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1l.	See above	See above	See above
<i>p. Bayfront Expressway &amp; Proposed Building 21 Entrance (#66)</i> The Project impact was identified under background plus-Project conditions (see TRA-1.1m). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1m.	See above	See above	See above

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b><i>IMPACT BEING ADDRESSED: Impacts on Routes of Regional Significance Under Cumulative 2040 Existing General Plan Plus Project Conditions. Some Routes of Regional Significance would operate at or below their LOS threshold with the addition of Project trips, and Project traffic would exceed the allowable 1 percent threshold, resulting in significant and unavoidable impacts. (TRA-11)</i></b>				
Implement Mitigation Measure TRA-2.1.	See above	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Increase in Daily Traffic Volumes on Roadway Segments under Cumulative 2040 Existing General Plan Plus-Project Conditions. Increases in daily traffic under existing General Plan plus-Project conditions would result in increased ADT volumes on Project area roadway segments, resulting in significant and unavoidable impacts. (TRA-12)</i></b>				
Implement Mitigation Measures TRA-3.1 and TRA-3.2.	See above	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Peak-Hour Traffic Impacts at Intersections Under Cumulative 2040 Proposed General Plan Conditions. Increases in peak-hour vehicle traffic associated with the Project would contribute to increased delays during the a.m. and p.m. peak hours in 2040 under the proposed General Plan (ConnectMenlo), causing a significant and unavoidable impact on the operation of study intersections. (TRA-13)</i></b>				
<i>TRA-13.1: Increase Traffic Capacity under Cumulative 2040 Proposed General Plan Conditions.</i>	See below	See below	See below	See below
<i>a. Sand Hill Road &amp; I-280 Northbound Off-ramp (#1)</i> With implementation of Mitigation Measure TRA-1.2, this potential cumulative impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>b. El Camino Real &amp; Ravenswood Avenue-Menlo Avenue (#28)</i> With implementation of Mitigation Measure TRA-10.1c, this potential cumulative impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-10.1c.	See above	See above	See above
<i>c. Willow Road &amp; Hamilton Avenue (#36)</i> The Project impact was identified under background plus-Project conditions (see TRA-1.1b). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement Mitigation Measure TRA-1.1b.	See above	See above	See above
<i>d. Bayfront Expressway &amp; Willow Road (#37)</i> The Project impact was identified under background plus-Project conditions. As discussed in Mitigation Measure TRA-1.1c, no additional	Implement Mitigation Measure	See above	See above	See above

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	TRA-1.1c.			
<i>e. Bayfront Expressway &amp; University Avenue (#38)</i> The Project impact was identified under background plus-Project conditions. As discussed in Mitigation Measure TRA-1.1d, no additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement Mitigation Measure TRA-1.1d.	See above	See above	See above
<i>f. Chilco Street &amp; Constitution Drive (#45)</i> This impact, identified under background plus-Project conditions, pertains to the design of the Project entrance, as described above in Mitigation Measure TRA-1.1f. With implementation of this mitigation measure, the intersection would operate acceptably, and this impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.1f.	See above	See above	See above
<i>g. Chrysler Drive &amp; Constitution Drive (#46)</i> With implementation of Mitigation Measure TRA-1.2, the cumulative impact at this location would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2.	See above	See above	See above
<i>h. University Avenue &amp; Adams Drive (#47)</i> Signalization of this intersection should be included in the City’s TIF program. The Project Sponsor’s payment of the TIF shall mitigate this impact, and the impact would be less than significant.	Pay the TIF identified in the City’s TIF program.	Prior to issuance of building permit for first phase of Building 21	Project Sponsor	PW
<i>i. University Avenue &amp; Bay Road (#51)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.	Implement Mitigation Measure TRA-1.2	See above	See above	See above
<i>j. University Avenue &amp; Donohoe Street (#54)</i> With implementation of Mitigation Measure TRA-1.2, this impact would be reduced, but the increase in delay would remain significant and unavoidable.	Implement Mitigation Measure TRA-1.2	See above	See above	See above
<i>k. University Avenue &amp; US 101 Southbound Ramps (#56)</i>	Implement	See above	See above	See above

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
With implementation of Mitigation Measure TRA-1.2, this impact would be reduced to a less-than-significant level.	Mitigation Measure TRA-1.2			
<i>l. Chilco Street &amp; Hamilton Avenue (#60)</i> The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-1.1k). This impact would remain significant and unavoidable.	Implement TRA-1.1k.	See above	See above	See above
<i>m. Bayfront Expressway &amp; Facebook Building 20 Entrance (#65)</i> The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-1.1l). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1l.	See above	See above	See above
<i>n. Bayfront Expressway &amp; Proposed Building 21 Entrance (#66)</i> The Project impact was identified under background plus-Project conditions and cumulative 2040 existing General Plan plus-Project conditions (see Mitigation Measure TRA-1.1m). No additional feasible mitigation measures were identified to reduce this impact, which would remain significant and unavoidable.	Implement TRA-1.1m.	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Impacts on Routes of Regional Significance Under Cumulative 2040 Proposed General Plan Conditions. Some Routes of Regional Significance would operate at or below their LOS threshold with the addition of Project trips. However, Project traffic would exceed the allowable 1 percent threshold, resulting in significant and unavoidable impacts. (TRA-14)</i></b>				
Implement Mitigation Measure TRA-2.1.	See above	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Increase in Daily Traffic Volumes on Roadway Segments Under Cumulative 2040 Proposed General Plan Conditions. Increases in daily traffic associated with the Project under Cumulative 2040 Proposed General Plan Conditions would result in increased ADT volumes on Project area roadway segments resulting in significant and unavoidable impacts. (TRA-15)</i></b>				
Implement Mitigation Measure TRA-3.1 and TRA-3.2.	See above	See above	See above	See above
<b>AIR QUALITY</b>				
<b><i>IMPACT BEING ADDRESSED: Construction Criteria Air Pollutant Emissions. Construction activities at the Project site could result in the</i></b>				

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b><i>generation of regional criteria pollutant emissions during construction in excess of BAAQMD thresholds. (AQ-2a)</i></b>				
<p><i>AQ-2.1: Implement BAAQMD Basic Construction Mitigation Measures to Reduce Construction-Related Dust.</i> The Project Sponsor shall require all construction contractors to implement the basic construction mitigation measures recommended by BAAQMD to reduce fugitive dust emissions. Emission reduction measures shall include, at a minimum, the following measures. Additional measures may be identified by BAAQMD or contractor as appropriate.</p> <ul style="list-style-type: none"> <li>• All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day. Recycled water, to be purchased through advance arrangement with the City of Redwood City or the City of Palo Alto, shall be used to water all exposed surfaces.</li> <li>• All haul trucks transporting soil, sand, or other loose material offsite shall be covered.</li> <li>• All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.</li> <li>• All vehicle speeds on unpaved roads shall be limited to 15 mph.</li> <li>• All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.</li> <li>• A publicly visible sign shall be posted with the telephone number and name of the person to contact at the lead agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. BAAQMD’s phone number shall also be visible to ensure compliance with applicable regulations.</li> </ul>	Implement the basic construction mitigation measures recommended by BAAQMD to reduce fugitive dust emissions.	During construction	Project Sponsor and Project Contractor(s)	PW / CDD
<b><i>IMPACT BEING ADDRESSED: Operational Criteria Air Pollutant Emissions. Operational activities at the Project site could result in the generation of regional criteria pollutant emissions during operation in excess of BAAQMD thresholds. (AQ-2b)</i></b>				
<i>AQ-2.2: Offset NOX Emissions Generated during Project Operation that are above the BAAQMD NOx Average Daily Emission Threshold.</i> The Project	Develop offsite NOx mitigation	Prior to occupancy of	Project Sponsor	PW / CDD

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>Sponsor shall, prior to occupancy of the first building within the Project, enter into an agreement with the City to develop an alternative or complementary offsite mitigation program to offset operational NO<sub>x</sub> emissions to the level established by the BAAQMD thresholds for the years in which the Project’s emissions exceed the BAAQMD threshold. The offsite mitigation program shall require Project Sponsor to provide a one-time payment to the City to establish a program to fund emission reduction projects through grants and similar mechanisms within the City of Menlo Park. The amount of such payment shall be calculated based on then-current BAAQMD Carl Moyer Program cost-effectiveness limit multiplied by the emissions that exceed BAAQMD’s average daily threshold for each year that emissions exceed the threshold plus a five percent administrative fee to fund procurement of offsite emission reductions for the Project’s projected operational emissions.</p> <p>Potential projects shall be limited to those which will reduce emissions for each year in which the project’s emissions exceed the BAAQMD threshold through the end of 2025, which is when the Project’s operational emissions are projected to be below the average daily thresholds, including, but not limited to, the following:</p> <ul style="list-style-type: none"> <li>• Alternative fuel, low-emission school buses, transit buses, and other vehicles.</li> <li>• Diesel engine retrofits and repowers.</li> <li>• Bike Sharing Programs.</li> <li>• Electric vehicle charging stations and plug-ins.</li> </ul> <p>All offsite reductions must be quantifiable, verifiable, and enforceable. The Project Sponsor shall engage a qualified air quality expert to coordinate with the City to identify a list of potential projects eligible for funding. Emission reduction projects shall be funded so that the Project’s emissions are reduced each year until the end of 2025. The air quality expert retained by the Project Sponsor shall provide a report within one year of occupancy of the first building within the Project identifying the projects that were funded and associated NO<sub>x</sub> emissions expected to be realized for each year out until the end of 2025. Annual reporting of the</p>	<p>program and provide a one-time payment to the City to establish a program to fund emission reduction projects.</p>	<p>Building 21..</p>		

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<p>implementation of emissions reduction projects shall be required until the Project's emissions are less than the BAAQMD threshold without the offsets.</p> <p>If a sufficient number of emissions reduction projects are not identified to meet the required performance standards in the City of Menlo Park, Project Sponsor shall consult with a qualified air quality expert to ensure conformity is met through some other means of achieving the performance standard of achieving net zero operational emissions in excess of BAAQMD's average daily thresholds through 2025, including (but not limited to) payment of a one-time mitigation offset fee to BAAQMD's Strategic Incentives Division plus a five percent administrative fee to fund one or more emissions reduction projects within the San Francisco Bay Area Air Basin. Reporting for any emissions reduction projects outside the City shall be completed on the same schedule as indicated above for emission reduction projects in the City.</p> <p>If annual reports indicate that emission reductions do not adequately reduce project emissions to a level below the BAAQMD threshold for any year, then a penalty of 200 percent shall be imposed that will require the Project Sponsor to obtain an additional year of offsets based on the amount of emissions by which the Project's emissions exceed the BAAQMD threshold for the next following year (e.g., if the 2019 emissions exceed the threshold by five tons, then 10 tons of emissions must be provided by 2020).</p>				
<p><b><i>IMPACT BEING ADDRESSED: Result in a Cumulatively Considerable Net Increase of any Criteria Pollutant for which the Project Region is Nonattainment. The Project could result in the generation of criteria pollutant emissions that would result in a cumulatively considerable net increase. (AQ-3)</i></b></p>				
Implement Mitigation Measures AQ-2.1 and AQ-2.2	See above	See above	See above	See above
<p><b><i>IMPACT BEING ADDRESSED: Exposure of Existing Sensitive Receptors to Substantial Pollutant Concentrations During Construction. The Project would expose existing sensitive receptors to substantial pollutant concentrations during construction. (AQ-4a)</i></b></p>				
Implement Mitigation Measure AQ-2.1	See above	See above	See above	See above
<p><b><i>IMPACT BEING ADDRESSED: Cumulative Criteria Pollutants during Operation. Operational activities associated with the Project could</i></b></p>				

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<b><i>generate substantial ROG, NO<sub>x</sub>, and PM10 emissions in excess of BAAQMD thresholds. (C-AQ-2b)</i></b>				
Implement Mitigation Measure AQ-2.2	See above	See above	See above	See above
<b>GREENHOUSE GAS EMISSIONS</b>				
<b><i>IMPACT BEING ADDRESSED: Greenhouse Gas Emissions. The Project would not generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment. (GHG-1)</i></b>				
<i>GHG-1.1: Implement BAAQMD Best Management Practices for Construction. The Project Sponsor shall require all construction contractors to implement the BMPs recommended by BAAQMD to reduce GHG emissions. Emissions reduction measures shall include, at a minimum, the use of local building materials (at least 10 percent), the recycling and reuse of at least 50 percent of construction waste or demolition material, and the use of alternative-fuel vehicles for construction vehicles/equipment (at least 15 percent of the fleet).</i>	Implement the BMPs recommended by the BAAQMD to reduce GHG emissions.	During construction	Project Sponsor and Contractor(s)	PW / CDD
<b>NOISE</b>				
<b><i>IMPACT BEING ADDRESSED: Exposure to Excessive Noise Levels. The Project could expose persons to or generate noise levels in excess of standards established in the General Plan, noise ordinance or applicable standards of other agencies. (NOI-1)</i></b>				
<i>NOI-1.1: Implement Noise Control Measures to Reduce Construction Noise during Project Construction. The Project Sponsor shall submit a Construction Noise Plan for review and approval by the Planning and Building Divisions prior to the issuance of the demolition permit. The Project Sponsor shall comply with construction noise limits specified in Section 8.06 of the City of Menlo Park Municipal Code by implementing measures during demolition and construction of the Project. These measures may include, but are not limited to:</i>	Submit a Construction Noise Plan. Implement noise control measures to reduce construction noise during construction.	Submit Plan prior to the issuance building permit for first phase of each building. Implement Plan during construction	Project Sponsor and Contractor(s)	CDD

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>Friday.</p> <ul style="list-style-type: none"> <li>• Use best available noise control techniques (e.g., improved mufflers, equipment redesign, intake silencers, ducts, engine enclosures, acoustically attenuating shields or shrouds) on equipment and trucks used for Project construction wherever feasible.</li> <li>• Use hydraulically or electrically powered impact tools (e.g., pile drivers, jack hammers, pavement breakers, rock drills) used for Project construction wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, use an exhaust muffler on the compressed air exhaust; this muffler can lower noise levels from the exhaust by up to about 10 dBA. Use external jackets on the tools themselves where feasible. This could achieve a reduction of 5 dBA. Use quieter equipment, such as drills, rather than impact equipment whenever feasible.</li> <li>• Use “quiet” gasoline-powered compressors or electric compressors, and use electric rather than gasoline- or diesel-powered forklifts for small lifting to the extent feasible.</li> <li>• Locate stationary noise sources, such as temporary generators, as far from nearby receptors as possible; such sources shall be muffled and enclosed within temporary enclosures and shielded by barriers or other measures to the extent feasible.</li> <li>• Install temporary noise barriers (generally approximately 8 feet in height) around construction areas adjacent to sensitive receptors to reduce construction noise from equipment to acceptable levels. Specifically, the noise barriers shall reduce noise levels during the hours of 8:00 a.m. to 6:00 p.m. on weekdays to 85 dBA at a distance of 50 feet from the construction equipment. In addition, the noise barriers shall reduce overall construction noise to less than 60 dBA Leq, as measured at the applicable property lines of adjacent uses, during the hours of 7:00 a.m. to 8:00 a.m. and 6:00 p.m. to 10:00 p.m. weekdays and 7:00 a.m. to 10:00 p.m. on Saturdays. The noise barriers shall be installed unless an acoustical engineer submits documentation</li> </ul>				

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>that confirms that barriers are not necessary to achieve these attenuation levels or provides specific locations and heights to achieve the required attenuation.</p> <ul style="list-style-type: none"> <li>• Prohibit trucks from idling along streets serving the construction site.</li> <li>• Prior to any pile-driving activities, notify all surrounding property owners and occupants within 300 feet of the Project site, informing them of the estimated start date and duration.</li> <li>• Implement “quiet” pile-driving technology (e.g., vibratory pile driving or pre-drilled pile holes) where feasible, in consideration of geotechnical and structural requirements and conditions.</li> <li>• Monitor the effectiveness of noise attenuation measures by taking noise measurements during pile-driving activities to ensure compliance with the 85 dBA standard at 50 feet for construction equipment and during general construction occurring during non-exempted daytime hours to ensure compliance with the 60 dBA Leq daytime standard.</li> </ul>				
<p><i>NOI-1.2: Implement Noise Control Measures to Reduce HVAC Noise during Project Operation.</i> The Project Sponsor shall design the Project HVAC system to limit noise to the applicable standard at the property line of nearby noise-sensitive receptors. Measures that can implemented to achieve this include, but are not limited to:</p> <ul style="list-style-type: none"> <li>• Maximize the distance between HVAC systems and nearby sensitive receptors,</li> <li>• Provide enclosures around the HVAC units,</li> <li>• Incorporate local barriers around equipment, and</li> <li>• Utilize mufflers or silencers on HVAC systems.</li> </ul> <p>Prior to the issuance of building permits, the Project Sponsor shall prepare a report, identifying measures that will be implemented to ensure that exterior HVAC noise levels will comply with the following noise limits:</p> <ul style="list-style-type: none"> <li>• The 60 dBA Leq daytime and 50 dBA Leq nighttime noise standards for equipment located on the ground,</li> </ul>	<p>Design the Project HVAC system to limit noise to the applicable standard at the property line of nearby noise-sensitive receptors. Prepare a report documenting compliance.</p>	<p>Prior to the issuance of building permit for first phase of each building and document compliance prior to occupancy.</p>	<p>Project Sponsor</p>	<p>CDD</p>

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<ul style="list-style-type: none"> <li>The zoning ordinance limit of 50 dBA at a distance of 50 feet for roof-mounted equipment.</li> </ul>				
<p><i>NOI-1.3: Install Sound Enclosures around Emergency Generators.</i> The Project Sponsor shall reduce the sound level from the operating generators to a maximum sound level of less than the 60 dBA noise standard at nearby noise-sensitive land uses. Measures that could accomplish this standard include, but are not limited to:</p> <ul style="list-style-type: none"> <li>Installing sound enclosures around all emergency generators,</li> <li>Utilizing mufflers to reduce generator noise, and</li> <li>Utilizing equipment that meets this standard.</li> </ul> <p>Prior to the issuance of building permits, the Project Sponsor shall prepare a report, identifying measures that shall be implemented to ensure that exterior noise levels from emergency generators comply with the 60 dBA Leq daytime/nighttime noise standards.</p>	Install sound enclosures for emergency generators and prepare a report identifying measures that shall be implemented	Prior to the issuance of building permit for first phase of each building; install sound enclosures of other measures to implement Report and document compliance prior occupancy.	Project Sponsor and Contractor(s)	CDD
<p><i>NOI-1.4: Limit Generator Testing to Daytime Hours.</i> The Project Sponsor shall limit generator testing to between the hours of 8:00 a.m. and 6:00 p.m.</p>	Limit generator testing to daytime hours.	Ongoing during occupancy	Project Sponsor	CDD
<p><i>NOI-1.5: Design Enclosures around Mechanical Equipment Associated with the Recycled Water System to Limit Exterior Noise.</i> The Project Sponsor shall design the recycled water system such that noise generated by mechanical equipment complies with the City noise standards of 60 dBA Leq (daytime) and 50 dBA Leq (nighttime) at nearby residences. Measures that could accomplish this include, but are not limited to:</p> <ul style="list-style-type: none"> <li>Designing equipment room enclosures, access doors, and other equipment room openings to limit noise that could be transmitted to the exterior</li> <li>Utilizing mufflers to limit blower noise</li> </ul> <p>Prior to the issuance of building permits, the Project Sponsor shall</p>	Design the recycled water system to comply with the City noise standards at nearby residences and prepare a report documenting compliance.	Prior to the issuance of building permit for first phase and document compliance prior to occupancy for each building.	Project Sponsor and Contractor(s)	CDD

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
prepare a report, identifying measures that shall be implemented to ensure that exterior noise levels from the recycled water system comply with the daytime and nighttime noise standards.				
<b><i>IMPACT BEING ADDRESSED: Substantial Permanent Increase in Ambient Noise Levels. The Project would result in a permanent increase in ambient noise levels in the Project vicinity, above levels existing without the Project. (NOI-3)</i></b>				
Implement Mitigation Measures NOI-1.2 through NOI-1.5	See above	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Substantial Temporary or Periodic Increase in Ambient Noise Levels. The Project could result in a substantial temporary or periodic increase in ambient noise levels in the Project vicinity, above levels existing without the Project. (NOI-4)</i></b>				
Implement Mitigation Measure NOI-1.1	See above	See above	See above	See above
<b><i>IMPACT BEING ADDRESSED: Cumulative Exposure to Excessive Noise. The Project, in combination with other development within the city, could result in a substantial increase in exposure of persons to noise in excess of the standards established in the City General Plan or Municipal Code. The Project's contribution would be cumulatively significant. (C-NOI-1)</i></b>				
Implement Mitigation Measure NOI-1.1	See above	See above	See above	See above
<b>CULTURAL RESOURCES</b>				
<b><i>IMPACT BEING ADDRESSED: Impacts on Archaeological Resources. The Project has the potential to encounter and damage or destroy previously unknown subsurface archaeological resources during construction. (CUL-2)</i></b>				
<i>CUL-2.1: Perform Construction Monitoring, Evaluate Uncovered Archaeological Features, and Mitigate Potential Disturbance of Identified Significant Resources at the Project Site. Prior to demolition, excavation, grading, or other construction-related activities on the Project site, the Project Sponsor shall hire a qualified professional archaeologist (i.e., one who meets the Secretary of the Interior's professional qualifications for archaeology or one under the supervision of such a professional) to monitor, to the extent determined necessary by the archaeologist, Project-related earth-disturbing activities (e.g., grading, excavation, trenching). In the event that any prehistoric or historic-period subsurface archaeological features or deposits, including locally darkened soil (midden), that could conceal cultural deposits, animal bone, obsidian, and/or mortar are discovered during demolition/construction-related earthmoving activities, all ground-disturbing activity within 100 feet of the discovery</i>	Retain a qualified archeologist to monitor project-related earth-disturbing activities. Halt all ground-disturbing activity within 100 feet of any discovery of an archaeological feature.	Prior to issuance of a building permit for first phase of each building and during demolition, excavation, grading activities, and construction	Project Sponsor/ Qualified Archaeologist / and Native American representative	CDD

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<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>shall be halted immediately, and the Planning and Building Divisions shall be notified within 24 hours. The City shall consult with the Project archaeologist to assess the significance of the find. Impacts on any significant resources shall be mitigated to a less-than-significant level through data recovery or other methods determined adequate by the City that are consistent with the Secretary of the Interior's Standards for Archaeological Documentation. If Native American archaeological, ethnographic, or spiritual resources are discovered, all identification and treatment of the resources shall be conducted by a qualified archaeologist and Native American representatives who are approved by the local Native American community as scholars of the cultural traditions. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. When historic archaeological sites or historic architectural features are involved, all identification and treatment is to be carried out by historical archaeologists or architectural historians who meet the Secretary of the Interior's professional qualifications for archaeology and/or architectural history.</p>				
<p><b><i>IMPACT BEING ADDRESSED: Impacts on Paleontological Resources. The Project could destroy a unique paleontological resource or site or unique geologic feature. (CUL-3)</i></b></p>				
<p><i>CUL-3.1: Conduct Protocol and Procedures for Encountering Paleontological Resources.</i> Prior to the start of any subsurface excavations that would extend beyond previously disturbed soils, all construction forepersons and field supervisors shall receive training by a qualified professional paleontologist, as defined by the SVP, who is experienced in teaching non-specialists to ensure they recognize fossil materials and follow proper notification procedures in the event any such materials are uncovered during construction. Procedures to be conveyed to workers include halting construction within 50 feet of any potential fossil find and notifying a qualified paleontologist, who shall evaluate its significance. If a fossil is determined to be significant and avoidance is not feasible, the paleontologist shall develop and implement an excavation and salvage</p>	<p>Provide training by a qualified professional paleontologist to construction personnel. If paleontological materials are discovered, an excavation and salvage plan shall be developed and</p>	<p>Prior to issuance of a building permit for first phase of each building and during demolition, excavation, grading activities, and construction</p>	<p>Project Sponsor/ Qualified Paleontologist</p>	<p>CDD</p>

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plan in accordance with SVP standards. Construction work in these areas shall be halted or diverted to allow recovery of fossil remains in a timely manner. Fossil remains collected during the monitoring and salvage portion of the mitigation program shall be cleaned, repaired, sorted, and cataloged. Prepared fossils, along with copies of all pertinent field notes, photos, and maps, shall then be deposited in a scientific institution with paleontological collections. A final Paleontological Mitigation Plan Report shall be prepared that outlines the results of the mitigation program. The City shall be responsible for ensuring that the monitor’s recommendations regarding treatment and reporting are implemented.	construction in the affected area shall be halted.			
<b><i>IMPACT BEING ADDRESSED: Impacts on Human Remains. The Project has the potential to encounter or discover human remains during excavation or construction. (CUL-4)</i></b>				
<i>CUL-4.1: Comply with State Regulations Regarding the Discovery of Human Remains at the Project Site.</i> If human remains are discovered during any construction activities, all ground-disturbing activity within 50 feet of the remains shall be halted immediately, and the county coroner shall be notified immediately, according to Section 5097.98 of the State Public Resources Code and Section 7050.5 of California’s Health and Safety Code. Additionally, the Planning and Building Divisions shall be notified. If the remains are determined by the county coroner to be Native American, the NAHC shall be notified within 24 hours, and the guidelines of the NAHC shall be adhered to in the treatment and disposition of the remains. The Project Sponsor shall also retain a professional archaeologist with Native American burial experience to conduct a field investigation of the specific site and consult with the Most Likely Descendant, if any, identified by the NAHC. As necessary, the archaeologist may provide professional assistance to the Most Likely Descendant, including the excavation and removal of the human remains. The City of Menlo Park Community Development Department, Planning Division, shall be responsible for approval of recommended mitigation as it deems appropriate, taking account of the provisions of state law, as set forth in State CEQA Guidelines Section 15064.5(e) and Public Resources Code Section 5097.98. The applicant shall implement approved mitigation, to be	Halt ground-disturbing activities within 50 feet of discovered human remains if human remains are discovered during any construction activities. Notify the County Coroner. If remains are determined to be Native American, NAHC guidelines shall be followed and a qualified archaeologist shall determine	During construction	Project Sponsor/ Qualified Archeologist	CDD

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
verified by the Planning Division, before the resumption of ground-disturbing activities within 50 feet of where the remains were discovered.	the Most Likely Descendant.			
<b>BIOLOGICAL RESOURCES</b>				
<b><i>IMPACT BEING ADDRESSED: Indirect Impacts on Special-Status Species. The Project could result in increased predation of special-status bird and mammal species that inhabit nearby saltwater and brackish water marshes in the Don Edwards National Wildlife Refuge. (BIO-2)</i></b>				
<p><i>BIO-2.1: Install Bird Perching Deterrents on All New Buildings and Other Elevated Structures, Including the Bicycle/Pedestrian Bridge.</i> The Project Sponsor shall implement the following measures to protect special-status species from increased predation on the Project site:</p> <ul style="list-style-type: none"> <li>For all new buildings constructed on the Project site, as well as the bicycle/pedestrian bridge and northern bridge approaches, the Project Sponsor shall install bird deterrents along suitable perching sites to deter avian predators of special-status species that inhabit the adjacent salt marshes. Such deterrents may include one or more of the following: bird spikes, bird netting, an electric shock track, sound deterrents, or perching deterrents approved by CDFW and/or USFWS.</li> <li>Trees that are used for replacement landscaping, especially those planted on rooftops, shall consist of species that generally do not exceed 30 feet in height to limit the visibility of adjacent salt marshes to the north. These trees may include native or non-invasive nonnative ornamental species. Species with broad canopies are preferred because trees with tall, narrow canopies (e.g., palms or conifers) generally provide better hunting perches for raptors. Additionally, trees that are planted on the rooftops of the new buildings shall be located away from the edge of the roof and planted with a reduced line of sight to the Bay.</li> </ul>	Install bird deterrents along suitable perching sites on buildings. New trees shall consist of species that generally do not reach heights of greater than 30 feet.	Prior to occupancy of each building.	Project Sponsor	CDD
<b><i>IMPACT BEING ADDRESSED: Impacts on Native Wildlife Nursery Sites. The removal of buildings, trees, shrubs, or woody vegetation and the installation of new buildings and lighting could affect native migratory birds. (BIO-3)</i></b>				
Implement Mitigation Measure BIO-2.1, plus: <i>BIO-3.1: Conduct Pre-construction Surveys for Nesting Migratory Birds.</i> The	Avoid construction	During nesting season, no	Qualified Biologist /	CDD



<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<ul style="list-style-type: none"> <li>• A nest survey shall be required prior to implementation of Phase 1 and Phase 2 of the Project and when construction work stops at a portion of the site where suitable nesting habitat remains for more than 15 days. Additionally, at least one nest survey shall be conducted at the beginning of each year of Project implementation between February and May. As discussed in Chapter 2, <i>Project Description</i>, Project implementation will occur between 2016 and 2022. The need for additional surveys shall be determined by the qualified wildlife biologist and based on the results of the initial survey.</li> <li>• If the biologist finds active nests during the survey, he or she shall establish species-specific no-disturbance buffer zones for each nest with use of high-visibility fencing, flagging, or pin flags. No construction activities shall be allowed within the buffer zones. The size of the buffer shall be based on the species sensitivity to disturbance and planned work activities in the vicinity. The buffer shall remain in effect until the nest is no longer active.</li> <li>• If structure demolition activities cannot occur outside of the nesting season, the Project Sponsor or its contractor shall remove inactive nests from the structure to be demolished and install nest exclusion measures (i.e., fine mesh netting, panels, or metal projectors) outside of the nesting season. All exclusionary devices shall be monitored and maintained throughout the breeding season to ensure that they are successful in preventing the birds from accessing cavities or nest sites. No more than 3 days prior to building demolition activities, a qualified biologist shall conduct a pre-construction survey of all potential nesting habitat on the structure to be demolished and the surrounding areas for the presence of active nests. If active nests are found on the building or in the affected area, then demolition activities shall not proceed until the biologist verifies that all nests on the building are inactive.</li> <li>• After all surveys and/or nest deterrence activities are completed, the biologist shall complete a memorandum detailing the survey effort and results and submit the memorandum to the City within 7 days of</li> </ul>	<p>Conduct a nest survey where suitable nesting habitat remains for more than 15 days. Submit a memorandum after all surveys and/or nest deterrence activities are completed.</p>	<p>Prior to issuance of a building permit for the first phase of each building and at the beginning of each year of Project implementation between February and May. Submit memorandum within 7 days of survey completion.</p>		

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
survey completion.				
<p><i>BIO-3.2: Implement Bird-Safe Design Standards into Project Buildings and Lighting Design.</i> The Project Sponsor or its contractor shall implement the following measures to minimize hazards to birds:</p> <ul style="list-style-type: none"> <li>• Reduce large areas of transparent or reflective glass.</li> <li>• Locate water features, trees, and bird habitat away from building exteriors to reduce reflection.</li> <li>• Reduce or eliminate the visibility of landscaped areas behind glass.</li> <li>• Turn non-emergency lighting off at night, especially during bird migration season (February–May and August–November).</li> <li>• Include window coverings that adequately block light transmission from rooms where interior lighting is used at night and install motion sensors or controls to extinguish lights in unoccupied spaces.</li> <li>• Design and/or install lighting fixtures that minimize light pollution, including light trespass, over-illumination, glare, light clutter, and skyglow, while using bird-friendly colors for lighting when possible. San Francisco's Standards for Bird-safe Buildings document<sup>1</sup> provides a good overview of building design and lighting guidelines to minimize bird/building collisions.</li> <li>• Nighttime construction work near Pond R3 shall be avoided. If nighttime construction work cannot be avoided, lighting will be directed to the work area and away from habitat for the western snowy plover.</li> </ul>	Implement Bird-Safe Design Standards into building and lighting design.	Prior to issuance of building permit for building shell and duration of use of the building.	Project Sponsor	CDD
<b>HYDROLOGY AND WATER QUALITY</b>				
<b><i>IMPACT BEING ADDRESSED: Violation of Water Quality Standards or Waste Discharge Requirements. The Project could violate water quality standards or waste discharge requirements. (WQ-1)</i></b>				
<p><i>WQ-1.1: Implement Construction Dewatering Treatment (if necessary).</i> Dewatering treatment would be necessary if groundwater is encountered</p>	Implement construction	During	Project Sponsor / Project	CDD

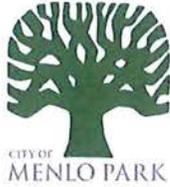
<sup>1</sup> City and County of San Francisco. 2011. *Standards for Bird-Safe Buildings*. San Francisco Planning Department. July 14. Available: <[http://www.sf-planning.org/ftp/files/publications\\_reports/bird\\_safe\\_bldgs/Standards%20for%20Bird%20Safe%20Buildings%20-%202011-30-11.pdf](http://www.sf-planning.org/ftp/files/publications_reports/bird_safe_bldgs/Standards%20for%20Bird%20Safe%20Buildings%20-%202011-30-11.pdf)>.

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>during excavation activities, dewatering is necessary to complete the Project, or the dewatered water is discharged to any storm drain or surface water body. Because there is potential for groundwater to be contaminated with VOC's or fuel products at the Project site, the Project Sponsor would be required to comply with the San Francisco Bay RWQCB's VOC and Fuel General Permit (Order No. R2-2012-0012).</p> <p>If dewatering activities require discharges into the storm drain system or other water bodies, the water shall be pumped to a tank and tested for water quality using grab samples and sent to a certified laboratory for analysis. If it is found that the water does not meet water quality standards, it should either be treated as necessary prior to discharge so that all applicable water quality objectives (as noted in Tables 3.10-1 and 3.10-2) are met or hauled offsite instead for treatment and disposal at an appropriate waste treatment facility that is permitted to receive such water. Water treatment methods shall be selected that achieve maximum removal of contaminants found in the groundwater and represent the best available technology that is economically achievable. Implemented methods may include the retention of dewatering effluent until particulate matter has settled before it is discharged, the use of infiltration areas, filtration, or other means. The contractor shall perform routine inspections of the construction area to verify that the water quality control measures are properly implemented and maintained, conduct visual observations of the water (i.e., check for odors, discoloration, or an oily sheen on groundwater), and perform other sampling and reporting activities prior to discharge. The final selection of water quality control measures shall be submitted in a report to the San Francisco Bay RWQCB for approval prior to construction. If the results from the groundwater laboratory do not meet water quality standards and the identified water treatment measures cannot ensure treatment that meets all standards for receiving water quality, then the water shall be hauled offsite instead for treatment and disposal of at an appropriate waste treatment facility that is permitted to receive such water.</p>	<p>dewatering treatment if groundwater is encountered.</p>	<p>construction</p>	<p>Contractor(s)</p>	
<p><b><i>IMPACT BEING ADDRESSED: Impacts from Flooding. The Project could expose people or structures to a significant risk of loss, injury, or death</i></b></p>				

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b><i>involving flooding, including flooding as a result of the failure of a levee or dam, but would not place structures within a 100-year flood hazard area. (WQ-5)</i></b>				
<i>WQ-5.1: Flood-Proofing of Project Underground Infrastructure.</i> Prior to or, at a minimum, concurrent with the issuance of the first construction activity permit at the Project site, and in connection with applicable FEMA requirements, the Project Sponsor shall ensure that the Project incorporates design features, including storm drains, sewers, and equipment facilities, that would flood-proof underground infrastructure, thereby allowing it to withstand hydrostatic forces and buoyancy from SLR changes in groundwater levels. Onsite recycled-water wetland treatment areas shall be located at grade, with underground tanks placed in elevated areas to provide protection from the 100-year BFE plus 16 inches.	Incorporate design features to flood-proof below-ground infrastructure.	Prior to, or concurrent with, the issuance of the first construction permit.	Project Sponsor	CDD
<i>WQ-5.2: Provide Adequate Stormflow Conveyance Capacity for Sea-Level Rise Conditions at the Project Site.</i> Prior to or, at a minimum, concurrent with the issuance of the first construction activity permit at the Project site, the Project Sponsor shall provide current documentation in the form of a technical report to ensure that, as a result of Project design features, the storm drain system’s existing conveyance capacity is not constricted by SLR at the outlets, including the offsite Chrysler pump station, as a result of the Project design.	Incorporate design features to ensure that storm drain system conveyance capacity is not constricted by sea level rise.	Prior to, or concurrent with, the issuance of the first construction permit	Project Sponsor	CDD
<b><i>IMPACT BEING ADDRESSED: Cumulative Hydrology and Water Quality Impacts. The Project, in combination with other foreseeable development in the vicinity, could contribute considerably to cumulative impacts on water quality, groundwater recharge and supplies, storm drain capacity, or current flooding. (C-WQ-1)</i></b>				
Implement Mitigation Measure WQ-1.1	See above	See above	See above	See above
<b>HAZARDS AND HAZARDOUS MATERIALS</b>				
<b><i>IMPACT BEING ADDRESSED: Upset and Accident Conditions Involving Hazardous Materials. The Project could create a potentially significant hazard to human health and/or the environment involving the release of hazardous materials. (HAZ-2)</i></b>				
<i>HAZ-2.1: Soil and Groundwater Management.</i> Soil Management Plans that cover the entire Project site shall be prepared and implemented. These Soil Management Plans shall, as appropriate, incorporate the analytical	Prepare and implement soil	Prior to and during	Project Sponsor	CDD/DTSC

<b>FACEBOOK CAMPUS EXPANSION PROJECT MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
results from the most recent groundwater monitoring event and soil investigations and include protocols for managing both known and potentially undocumented residual soil and groundwater contamination that may be encountered during Project construction, including naturally occurring asbestos. The Soil Management Plans shall include dust control measures that describe how construction and grading operations will minimize dust emissions and ensure that no equipment or operations will emit visible dust across the property line. Although naturally occurring asbestos has not been detected in the vicinity of Buildings 307-309, in accordance with CARB’s Asbestos ATCM for Construction, Grading, Quarrying, and Surface Mining Operations, if naturally occurring asbestos is encountered during construction, then dust control measures must meet the requirements of an ADMP approved by the BAAQMD. These Soil Management Plans shall be approved by DTSC and implemented during Project construction.	management plans.	construction		
<i>HAZ-2.2: Additional Site Investigation.</i> If required by DTSC, additional site investigations shall be performed to delineate the source and extent of contamination on the Project site. At DTSC’s discretion, these investigations may be incorporated into the Soil Management Plans required by DTSC for the Project site. The analytical results shall be compared to risk-based human health screening levels approved by DTSC. The site investigation(s) shall be prepared and evaluated by a licensed professional, and a technical report summarizing the field activities, results, and conclusion shall be submitted to DTSC for review and approval prior to issuance of building permits.	If required, conduct additional site investigations to delineate the source and extent of contamination and prepare a report.	Prior to issuance of building permits	Project Sponsor	CDD/DTSC
<i>HAZ-2.3: Remedial Action.</i> According to the results of additional site investigations (if any), the Project Sponsor shall coordinate with DTSC to select and implement remedial actions (as necessary) to protect future site users from conditions that could pose an unacceptable health risk. Remedial measures may include, but are not limited to, source removal of contaminated materials, in-situ treatment, engineering controls, and/or modification of institutional controls described in the existing LUC for the Project site. Remedial actions shall be implemented prior to building	Coordinate with DTSC to select and implement remedial actions (as necessary).	During implementation of the Soil Management Plans	Project Sponsor	CDD/DTSC

<b>FACEBOOK CAMPUS EXPANSION PROJECT                      MITIGATION MONITORING AND REPORTING PROGRAM</b>				
<b>Mitigation Measures</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
occupancy. At DTSC's discretion, remedial actions may be completed during implementation of the Soil Management Plans required by DTSC for the Project site.				



# NOTICE OF PREPARATION ENVIRONMENTAL IMPACT REPORT FACEBOOK CAMPUS EXPANSION PROJECT CITY OF MENLO PARK

**Date:** June 18, 2015

<b>To:</b>	State Clearinghouse State Responsible Agencies State Trustee Agencies Other Public Agencies Interested Organizations	<b>From:</b>	Kyle Perata Associate Planner City of Menlo Park 701 Laurel Street Menlo Park, CA 94025
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**Subject:** Notice of Preparation (NOP) of the Environmental Impact Report for the Facebook Campus Expansion Project

**Lead Agency:** City of Menlo Park Planning Division

**Project Title:** Facebook Campus Expansion Project

**Project Area:** City of Menlo Park

Notice is hereby given that the City of Menlo Park (City) will be the lead agency and will prepare an environmental impact report (EIR) for the Facebook Campus Expansion Project (Project). The EIR will address the Project's potential physical environmental effects on each of the environmental topics outlined in the California Environmental Quality Act (CEQA). The City of Menlo Park is requesting comments on the scope and content of this EIR.

A scoping session will be held as part of the Planning Commission meeting on **July 13, 2015, at 7 p.m.** at the Menlo Park City Council Chambers, located at 701 Laurel Street, Menlo Park, CA 94025. The scoping session is part of the EIR scoping process and when the City solicits input from the public and other agencies on specific topics they believe should be addressed in the environmental analysis. The focus of the scoping meeting will be the content to be studied in the EIR. Written comments on the scope of the EIR may also be sent to:

**Kyle Perata, Associate Planner**  
City of Menlo Park  
Community Development Department, Planning Division  
701 Laurel Street  
Menlo Park, CA 94025  
kperata@menlopark.org  
Phone: 650.330.6721  
Fax: 650.327.1653

Comments on the NOP are due no later than the close of the NOP review period at 5:00 p.m. on **Monday, July 20, 2015**. However, we would appreciate your response at the earliest possible date. Please send your written comments to Kyle Perata at the address shown above or email to

ktperata@menlopark.org with "Facebook Campus Expansion Project EIR" as the subject. Public agencies providing comments are asked to include a contact person for the agency.

**PROJECT LOCATION AND EXISTING CONDITIONS:** The 58-acre Project site encompasses the existing TE Connectivity (TE) campus at 300–309 Constitution Drive in the city of Menlo Park. The Project site, which includes assessor's parcel number (APN) 055-260-250, is zoned M-2 (General Industrial) and M-2-X (General Industrial, Conditional Development). The site is designated as Limited Industry under the City's General Plan and can be built out to approximately 1.142 million square feet (sf) of office uses under the allowable 0.45 floor area ratio (FAR), and up to approximately 1.396 million square feet or 0.55 FAR for other general industrial uses. The Project site is bounded by Bayfront Expressway/State Route (SR) 84 to the north, Facebook Building 20 to the east, and Chilco Street to the west and south. A portion of the Project site abuts the Dumbarton Rail Corridor to the south. Figure 1 depicts the location of the Project site.

Currently, there are 10 buildings containing industrial, warehouse, office, and research and development (R&D) uses at the Project site, totaling approximately 1.02 million sf, as well as 1,690 parking spaces. TE used and continues to use the site primarily for industrial activities and Pentair Thermal Controls leases one of the on-site buildings. In December 2014, the Planning Commission approved a conditional use permit to convert an existing 184,460-square-foot warehouse building located on the southwestern portion of the Project site for Facebook office uses. The conversion will result in an approximately 4,330 square feet reduction for a new building square footage of approximately 180,108 square feet. The renovation of this building (Building 23) is expected to be completed in the first quarter of 2016 and, while it is located on the Project site, it is not part of the Project.

**PROJECT DESCRIPTION:** The proposed Facebook Campus Expansion Project includes the demolition of buildings at the site (Building 23 would not be demolished) and the construction of two new office buildings (Building 21 and Building 22), encompassing approximately 967,000 sf (a net increase of approximately 127,000 sf at the Project site). Figure 2 shows the conceptual site plan. The proposed Building 21 would contain approximately 513,000 sf of office and event uses and be located on the eastern portion of the Project site. The event space would be utilized for internal Facebook events and have the capacity to accommodate approximately 2,000 people. Building 21 would be constructed during the first phase of development, and Building 22 would be constructed as the second phase of development. The proposed Building 22 would include approximately 450,000 sf of office uses and would be located on the western portion of the Project site. Both buildings would be constructed over surface parking that would contain approximately 3,800 parking spaces. The buildings would be connected to each other and the existing Building 20 east of the Project site via enclosed bridges. The buildings would have an architectural style, height, and massing similar to that of Facebook Building 20. Maximum building heights would be approximately 75 feet.

The Project would also include the potential for a 200-room limited-service hotel with approximately 175,000 sf of space (Building 24) in the northwestern portion of the Project site. The hotel, which would be located near the corner of Chilco Street and Bayfront Expressway, could be constructed in a future phase and would bring the total area of new development at the Project site to approximately 1.14 million sf, with a total FAR not to exceed 0.55 (including existing Building 23). The proposed office GFA would be consistent with the existing City General Plan and M-2 General Industrial Zoning District, which allows office uses at a FAR of up to 0.45 and the comprehensive project including the hotel would not exceed 0.55 FAR, which is consistent with the City General Plan. However, rezoning the entire site from M-2 and M-2-X to M-2-X would be required to exceed the maximum 35-foot height limit and a Zoning Ordinance Text Amendment would be required to accommodate the proposed hotel.

Access to the Project site is proposed from Constitution Drive and a new signalized intersection on Bayfront Expressway at the mid-point of the site. A vehicular connection on the east end of the site to the existing Building 20 could also be constructed. The Project may include a limit on the number of daily or

peak period vehicle trips to and from the site. The Project would be organized around a publicly accessible open space and a bicycle/pedestrian corridor that would run north-south through the middle of the site. The Project would also include construction of a new bicycle/pedestrian bridge over Bayfront Expressway to allow for access to the Bay Trail and Bedwell Bayfront Park from the Project site and the Belle Haven neighborhood. The perimeter of the Project site would have a landscaped buffer that would include bicycle/pedestrian pathways that would be separated from the vehicle access roads. The on-site paths would connect the proposed office buildings to Building 20 on the east and Facebook Buildings 10–19 on the north side of Bayfront Expressway.

**PROJECT APPROVALS:** The following approvals would be required by the City under the Project:

- Rezone from M-2 to M-2-X
- Conditional Development Permit
- Zoning Ordinance Text Amendment
- Development Agreement
- Heritage Tree Removal Permits
- Below-Market-Rate Housing Agreement
- Lot Line Adjustment
- Environmental Review
- Fiscal Impact Analysis

**RESPONSIBLE AGENCIES:** The agencies listed below are expected to review the draft EIR to evaluate the Project:

- Bay Area Air Quality Management District (BAAQMD)
- California Department of Transportation (Caltrans)
- California Regional Water Quality Control Board (RWQCB)/San Mateo Countywide Water Pollution Prevention Program
- City/County Association of Governments (C/CAG)
- San Mateo County Transportation Authority (SMCTA)
- Department of Toxic Substance Control (DTSC)
- Menlo Park Fire Protection District
- San Mateo County Environmental Health Division
- West Bay Sanitary District

**INTRODUCTION TO EIR:** The purpose of an EIR is to inform decision-makers and the general public of the environmental effects of a proposed project. The EIR process is intended to provide environmental information for evaluating a proposed project and its potential to cause significant effects on the environment, examine methods of reducing adverse environmental impacts, and identify alternatives to a proposed project. The Facebook Campus Expansion Project EIR will be prepared and processed in accordance with CEQA and the State CEQA Guidelines. The EIR will include the following:

- Summary of the Project and its potential environmental effects
- Description of the Project
- Description of the existing environmental setting, potential environmental impacts of the Project, and mitigation measures to reduce significant environmental effects of the Project
- Alternatives to the Project

- Cumulative impacts
- CEQA conclusions

**PROBABLE ENVIRONMENTAL EFFECTS:** The EIR will analyze whether the Project would have significant environmental impacts in the following areas:

- |                                   |                                  |
|-----------------------------------|----------------------------------|
| • Aesthetics                      | • Hydrology and Water Quality    |
| • Air Quality                     | • Land Use and Planning Policy   |
| • Biological Resources            | • Noise                          |
| • Cultural Resources              | • Population and Housing         |
| • Geology and Soils               | • Public Services and Recreation |
| • Greenhouse Gas Emissions        | • Utilities                      |
| • Hazards and Hazardous Materials | • Transportation and Traffic     |

To help prepare several of these sections and analyze the impacts, a transportation study will be prepared. The transportation study will focus on intersections, residential and non-residential roadway segments, and Routes of Regional Significance. The transportation study for the Project and the ConnectMenlo General Plan update will be coordinated to ensure consistency and address both the near-term and long-term transportation needs and impacts of both projects. It is currently envisioned that a single, concurrent transportation analysis will be conducted to evaluate the Project and the ConnectMenlo General Plan update.

**ENVIRONMENTAL EFFECTS NOT LIKELY TO REQUIRE FURTHER ANALYSIS:** The Project is not anticipated to result in significant environmental effects in the following areas:

- Agricultural or Forestry Resources
- Mineral Resources

The Project site is fully developed in an urbanized area and located near SR 84 and US 101. As such, agricultural and mineral resources do not exist on the site, and a detailed analysis of these topics will not be included in the EIR.

**ALTERNATIVES:** Based on the significance conclusions determined in the EIR, alternatives to the Project will be analyzed to reduce identified impacts. Section 15126.6(e) of the State CEQA Guidelines requires the evaluation of a No-Project Alternative. Other alternatives may be considered during preparation of the EIR and will comply with the State CEQA Guidelines, which call for a "range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project."

**EIR PROCESS:** Following the close of the NOP comment period, a draft EIR will be prepared that will consider all NOP comments. In accordance with State CEQA Guidelines Section 15105(a), the draft EIR will be released for public review and comment for the required 45-day review period. Following the close of the 45-day public review period, the City will prepare a final EIR, which will include responses to all substantive comments received on the draft EIR. The draft EIR and final EIR will be considered by the Planning Commission and City Council in making the decision to certify the EIR and approve or deny the Project.

  
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 Kyle Perata, Associate Planner  
 City of Menlo Park

June 18, 2015  
 Date





## **Attachment 3**

### **Facebook Campus Expansion Project Master Plan**



**STAFF REPORT**

**City Council**

**Meeting Date:**

**6/6/2017**

**Staff Report Number:**

**17-133-CC**

**Informational Item:**

**Update on proposed revisions to the approved Facebook Campus Expansion Project at 301-309 Constitution Drive**

**Recommendation**

This is an informational item and no action is required.

**Policy Issues**

The proposed conditional development permit (CDP) amendment will require the City Council to consider the merits of the project, including project consistency with the City's current General Plan, Municipal Code, and other adopted policies and programs. The Council will also need to consider the proposed modified development standards and project phasing in the requested CDP amendment along with the environmental review associated with the proposed changes.

**Background**

**Site Location**

The subject site is located at 301-309 Constitution Drive. Using Bayfront Expressway in an east to west orientation at the subject site and Willow Road in a north to south orientation, the subject site extends from the corner of Chilco Street and Bayfront Expressway east toward Building 20, located at 1 Facebook Way, near the intersection of Willow Road and Bayfront Expressway. Chilco Street wraps around the western side and a portion of the southern side of the property. The campus is adjacent to Bayfront Expressway on the north across from the former salt ponds. A location map identifying the entire Facebook West Campus is included as Attachment A.

**Project History**

On February 7, 2017, Hibiscus Properties LLC, on behalf of Facebook, submitted an application for an amendment to the previously approved CDP for the Campus Expansion Project located at 301-309 Constitution Drive. The Facebook Campus Expansion Project included two new office buildings totaling 962,400 square feet, a 200-room limited service hotel, publicly accessible open space, and bicycle and pedestrian bridge and was approved by the City Council on November 1, 2016. The approved project was expected to be constructed in two phases: Phase 1 (Building 21) and Phase 2 (Building 22 and Hotel). The public open space and bicycle and pedestrian bridge would be completed in Phase 2 but the permitting process for the bridge was required to be initiated by the applicant in Phase 1. The applicant subsequently began construction on Phase 1 (Building 21) in December 2016. Due to the existing tenant (TE Connectivity) remaining at the site longer than anticipated, the applicant submitted an application for a revised project for Phase 2 and staff is currently reviewing the application and preparing the associated environmental analysis. The applicant submitted a project description letter (Attachment B) that describes

the proposed revisions in more detail.

## Analysis

Buildings at 301-306 Constitution Drive and the chemical transfer facility (CTF) building, also located on site, would need to be demolished to allow for the construction of Building 22, based on the previously approved site plan associated with the approved CDP. Therefore, TE Connectivity would need to completely vacate the site to allow for the construction of Building 22. Facebook has stated that its current lease agreement with TE Connectivity extends to September 2022, with options for TE to leave prior to the end of the current lease agreement (potentially in 2020). Since TE Connectivity could be at the site through September 2022, Facebook has modified the project site plan to allow for TE Building 305 to continue to be located on site, while allowing for the concurrent construction and occupancy of Building 22. The project plans are included in Attachment C. The proposed revised project includes the following modifications from the previously approved CDP:

- Modify the design of Building 22 to encompass a four-story building of approximately 449,500 square feet of gross floor area (maximum approved under previous CDP) with a reduced building footprint;
- Relocate the surface parking beneath Building 22 into a stand-alone eight-story parking garage structure;
- Increase the maximum height (as measured from finished grade) of the parking garage structure from 75 feet in height to approximately 83 feet for the safety railing and vehicle screening, with the parking deck limited to a maximum of 75 feet in height;
- Increase in the height of Building 21 to approximately 87 feet to allow for skylights to exceed the 75 foot height limit;
- Construct new electric vehicle charging facilities (encompassing approximately 97,500 square feet of site area) for Facebook's fleet of electric shuttle buses and trams;
- Increase the amount of private landscaped open space by approximately five acres after the demolition of Building 305;
- Retain Building 305 and associated manufacturing operations in an interim phase prior to TE's lease expiration;
- Reduce the required parking for Building 22 and 23 by approximately 20 spaces during the interim phase;
- Increase the size of the publicly accessible open space, which includes the plaza, emergency vehicle access lanes, pedestrian/bike pathways, and bio-treatment areas; and
- Modify the timeline for delivery of the full extent of the publicly accessible open space to accommodate the retention of Building 305 in the interim phase.

Section 6 (Modifications) of the previously approved CDP sets up the review process for modifications to the approved project. The review process includes four distinct scenarios based on the extent of the proposed project revisions outlined in the table below.

Table 1		
Section	Title	Acting Body
6.1.1	Substantially Consistent Modifications	Planning Division Staff
6.1.2	Minor Modifications	Planning Division Staff with notification to Planning Commission and subject to Commissioner request for additional review
6.1.3	Major Modifications	Planning Commission
6.1.4	Design Review	Planning Commission; Limited to review of architectural review of Building 22 and Hotel, provided project plans consistent with CDP
6.1.5	CDP Amendments	City Council, with review and recommendation by Planning Commission

The proposed revisions to the project would result in material modifications to the conditions of approval, modifications of allowed uses (parking garage structure and on-site recharging facility and vehicle storage), the relaxation of some development standards, and would generally not be consistent with the project description in the CDP. Therefore the proposed revised project would require a CDP amendment, as set forth in Section 6.1.5. In addition to amending the CDP, the Development Agreement (DA) for the project may also need to be revised to ensure that the DA is consistent with the amended CDP. Further, the DA may need to be revised to ensure that the expected timing for economic benefits negotiated with the previously approved project are realized. As staff continues to review the project, modifications to additional land use entitlements, such as the heritage tree removal permits and Below Market Rate (BMR) Housing Agreement, may also need to be revised.

**Current Status and Planning Commission Review**

Upon receipt of the application, staff began its review of the project, including the applicable environmental analysis. Since an EIR was certified for the project, the City contracted with the same consultant (ICF International) to conduct a consistency analysis between the proposed revised project and the certified EIR. That analysis is underway and in the preliminary stages.

As part of the initial review for the project, planning staff scheduled a study session for the proposed revised project with the Planning Commission. On May 22, 2017 the Planning Commission reviewed the project at a study session, which provided an opportunity for the applicant to present the revised project to the Commission, for members of the public to comment on the proposed revised project, and for the Commission to ask questions and provide feedback to the applicant and staff. In general, the Planning Commission generally voiced support for the overall design of Building 22, and the modification to the timing of the demolition of Building 305. However, the Planning Commission provided guidance to the applicant on the design of the garage. The Planning Commission requested that the applicant provide modulation of the garage structure’s façade, evaluate alternatives for the exterior materials, and explore options for lowering the height of the garage and reducing the massing of the garage structure, including the potential splitting of

the garage structure into two structures with a shared ramp and including a below grade parking level. Further, the Commission provided additional guidance on the need to provide more articulation on the northern (Bayfront Expressway) façade of Building 22 and for the applicant to explore options to reduce the massing and height of the mechanical equipment enclosures. In addition, the applicant's proposed mechanical screening does not meet the requirements of the zoning ordinance with regard to opacity and the Commission expressed a desire for the massing of the mechanical screening to be reduced. The Commission also noted that the preliminary design of the public open space may not effectively draw people into the full space and should be modified. The Commission did also acknowledge that the proposed revised project would delay the anticipated timeline for delivery of the hotel and its associated revenue to the City. Depending on when TE vacates the site, the hotel may not be operational until mid-2022 or mid-2024 according to the applicant.

### **Next Steps**

City staff is reviewing the project plans, associated documents, and managing the environmental review process. The applicant will consider the Planning Commission's comments and work to modify the plans to address its comments accordingly. Once all the required information is submitted by the applicant and deemed complete, and the environmental analysis is finished, the project will be scheduled for a Planning Commission meeting and ultimately for a future City Council meeting. If the environmental review consistency analysis determines that the revised project is consistent with the certified EIR, an addendum to the certified EIR will be prepared for the consideration of the Planning Commission and City Council.

At this time, the environmental review is anticipated to be complete in late summer 2017, with the entitlement hearings taking place in the fall of 2017.

### **Impact on City Resources**

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

### **Environmental Review**

An EIR was prepared for the previously approved project. The proposed revised project is being evaluated for consistency with the previously certified EIR. If the proposed revised project is consistent with the certified EIR, then an addendum to the EIR will be prepared for review and consideration of the Planning Commission and City Council as part of the overall project review process.

### **Public Notice**

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

### **Attachments**

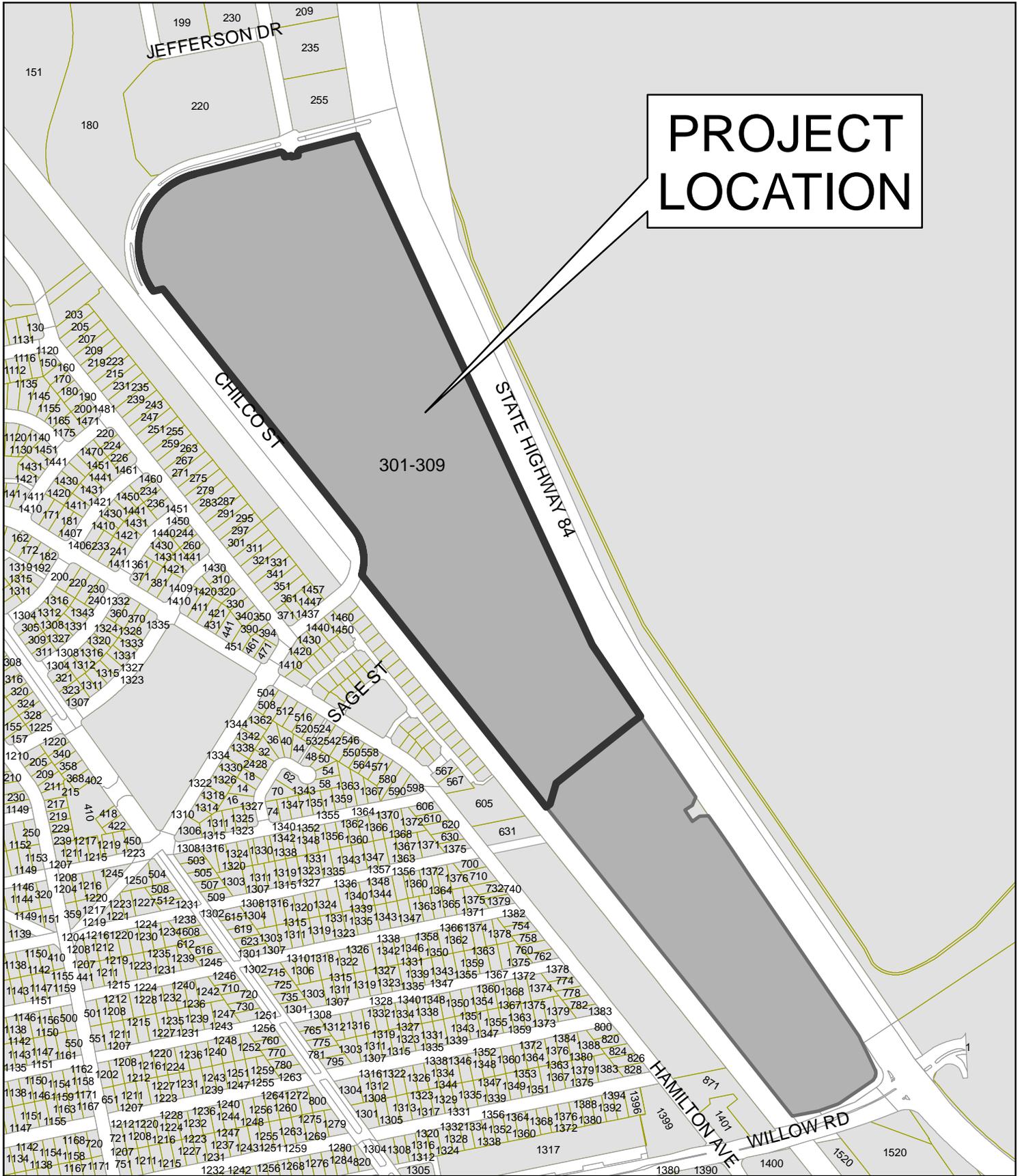
- A. Location Map
- B. Project Description Letter
- C. Project Plans

Report prepared by:  
Kyle Perata, Senior Planner

Staff Report #: 17-133-CC

Report reviewed by:  
Mark Muenzer, Assistant Community Development Director

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**PROJECT  
LOCATION**

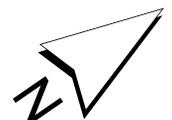
301-309

# CITY OF MENLO PARK

## LOCATION MAP

### 301-309 CONSTITUTION DRIVE & 1 FACEBOOK WAY

DRAWN: TAS CHECKED: KTP DATE: 05/22/17 SCALE: 1" = 300' SHEET: 1



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**Facebook Campus Expansion Project (Revised 4-25-17)****Building 22 Design Review and Request for Conditional Development Permit Amendment****Preliminary Project Description****I. Introduction**

Facebook is requesting design review for Building 22, as well as corresponding revisions to the site and phasing plan for the Facebook Campus Expansion Project. These changes generally include (i) shifting the parking program from surface parking beneath Building 22 into a stand-alone parking structure, (ii) reducing the footprint of Building 22, (iii) creating additional landscape reserve space, and (iv) installing new recharging facilities for Facebook's fleet of shuttle buses and trams. No material changes are proposed to the hotel, which has not yet been designed and construction of which is still anticipated to commence after TE vacates the site.

The revised program reflects a refined architectural design for Building 22 and a site plan that was designed to stay within the scope of the Environmental Impact Report (EIR) and CDP in order to avoid any significant new impacts or any substantial increase in the severity of previously identified impacts. Facebook is not requesting any new uses or square footage. None of the requested changes would affect Facebook's financial or other obligations under the CDP or Development Agreement.

As described below and based on preliminary conversations with City staff, Facebook believes that the proposed changes may require amendments to the Amended and Restated Conditional Development Permit Amendment for the Project.

As of April 27, 2017, this project description has been revised to include the following:

- Facebook will commit to demolishing Building 305 within twelve months of the date that TE vacates Building 305 (subject to receipt of all applicable permits and approvals, including any permits and approvals from state or federal agencies). In addition, Facebook agrees not to provide TE with any additional renewal or extension rights above and beyond what is in TE's existing lease (i.e., September 2022 will remain the "outside expiration date" for TE's lease).
- Facebook anticipates that there will be no net increase in the number of workers and visitors on-site during the interim period when Building 22 and Building 305 could both be occupied (which is anticipated to be no more than three years). TE currently has approximately 110 workers within Building 305 working in 8-hour shifts, 24 hours a day (for a total of approximately 330 employees). While this existing condition could remain through September 2022 if TE exercises its renewal right, at the latest, the number of TE workers on-site is anticipated to be less than the number of workers and guests anticipated to be present at the hotel.

**II. Background**

In November 2016, the City Council approved the Facebook Campus Expansion project. An EIR was prepared and certified to analyze the environmental impacts from the project pursuant to the

California Environmental Quality Act (CEQA). The EIR analyzed the overall effects of the project and identified mitigation measures to mitigate the project's significant impacts where feasible.

The project approvals included a Development Agreement, the CDP, a zoning ordinance text amendment (to accommodate the proposed hotel), a lot line adjustment, heritage tree removal permits, and a below-market rate housing agreement, among other approvals (collectively, the Approvals). The City Council also adopted CEQA Findings and a Mitigation Monitoring and Reporting Program for the project, as well as a Statement of Overriding Considerations concluding that the project's substantial benefits outweigh its environmental effects.

The project as described in the EIR contains two office buildings (Buildings 21 and 22) and a 200 room hotel, as well as a pedestrian and bicycle bridge across the Bayfront Expressway and a new, approximately two-acre publicly accessible plaza and open space. As described in the EIR's project description and on the City's website for the project, the project would be phased in over time as the existing tenants (Pentair and TE Connectivity) vacate the site.

In November 2016, the Planning Commission and City Council approved Building 21 through the City's design review process, concurrently with their approval of the project. Facebook commenced construction of Building 21 in December 2016, and anticipates completing Building 21 in mid-2018.

#### **A. The Approved Project**

The approved project as described in the EIR and the Approvals involves two office buildings, comprising approximately 962,000 gsf of office and amenity uses, and a 200 room hotel to be constructed on a portion of a 58 acre site located at 300 to 309 Constitution Drive within the City of Menlo Park. The project is subject to a site-wide trip cap to limit the number of peak hour and daily trips to and from the site.

Pursuant to the CDP and the Approvals, development on the site is limited to a .45 FAR for office uses and a maximum of .55 FAR for all uses (including the hotel), a building height limit of 75', and a minimum requirement of 3,533 parking spaces for the two new office buildings, hotel, and Building 23.

##### **1. Building 21**

Building 21 will be located in the eastern portion of the Project site and contain approximately 512,900 gsf of office and event uses. Building 21 will be a multi-story building on a podium structure above an at-grade parking lot. Facebook commenced construction of Building 21 in December 2016 after City approval in November 2016.

No changes are proposed to Building 21, with the exception of the location of the proposed open-air bridge connecting Building 21 and Building 22.

##### **2. Building 22**

Building 22 will be located in the northwestern portion of the Project site and contain approximately 449,500 gsf of office and event uses, and be approximately 75 feet in height. At the time the EIR was

prepared and as described in the Approvals, Building 22 had not yet been designed and it was contemplated that design review for Building 22 would occur in the future.

As described in the EIR, Building 22 was anticipated to be similar to Building 21. It was to include a ground level that had multiple lobbies and parking spaces for approximately 1,294 vehicles, a first floor accommodating office and amenity space, and potentially a mezzanine level. Useable open space would be provided on the roof, which would include landscaped areas, walking paths, and HVAC equipment, similar to Building 21. An outdoor terraced area was also anticipated to be located adjacent to the food court/dining area on the south side of Building 22. The food court would be separated from the main level by the outdoor terraced area, which would allow for outdoor dining. It was also anticipated that Building 22 would be connected to Building 21 through an open-air bridge.

In late 2016, Facebook commenced the design process for Building 22. After reviewing more than fifty different design schemes prepared by Gehry Partners, Facebook selected a preferred design. The revised design for Building 22 is further described below, including changes from the conceptual design that was evaluated in the EIR.

### **3. Hotel**

The project includes a 200-room hotel that could be developed as part of a future phase near the corner of Chilco Street and SR 84. Although the hotel had yet to be designed, the EIR studied a maximum hotel envelope that provided approximately 73,200 gsf of hotel and support space, approximately 1,800 gsf of office space, approximately 13,700 gsf of amenities, and 86,100 gsf of circulation, wall, structure, and stair space. Included in the amenities would be food and beverage areas for the public, multi-function space, a fitness room, a pool, and deck areas. The hotel was also assumed to be approximately 75 feet in height.

No changes are proposed to the hotel, which still has not yet been designed and would be subject to a future design review process. Facebook has also been actively meeting with potential hotel developers/partners and expects to select a preferred developer/partner within the next 12-18 months.

### **4. Publicly Accessible Pedestrian and Bicycle Bridge; Publicly Accessible Plaza**

As part of the Approvals, Facebook committed to constructing a new publicly accessible pedestrian and bicycle bridge across the Bayfront Expressway and a two-acre publicly accessible park for passive recreational uses and community events.

No changes are proposed to the publicly accessible pedestrian and bicycle bridge or the public park in connection with the current application. However, the overall scope and geographic limits of the public park have been refined in consultation with City staff, and the updated plans have significantly expanded the usable footprint of the public park by adding an additional pedestrian path and passive recreation space into the area occupied by the original footprint of Building 22.

## **B. Project Phasing and Schedule**

### **1. MPK 21 (Phase 1)**

Phase 1 involves construction of Building 21 as well as the permitting and construction of the pedestrian and bicycle bridge. Consistent with the schedule identified in the EIR, grading and utility work for Building 21 began in late fall 2016, and foundation permits were issued in December 2016. The permitting process for the pedestrian and bicycle bridge is well underway. No changes are proposed with respect to construction of Phase 1 of the project.

### **2. MPK 22 and Hotel (Phases 2 and 3)**

As described in the EIR and contemplated in the Approvals, construction of the project will be phased to allow existing tenants to continue operating. The construction schedule analyzed in the EIR contemplated that construction of Building 22 would start in early 2018 with demolition of Buildings 301-306 and the CTF, and that construction of the hotel would commence in early 2019. This schedule was proposed by Facebook in order to ensure the most conservative environmental analysis possible under a scenario in which the existing tenants on the site vacate prior to the expiration of their leases. Although Pentair has vacated the site, TE continues to occupy buildings 302, 303/304/306, 305, and the CTF, all of which are located within the western portion of the site. Level 10 construction currently occupies Building 301, but is anticipated to move into temporary construction trailer facilities in mid-2017.

TE's lease rights for Buildings 302, 303, 304, 305 and 306 and the CTF expires in September 2019. TE has one option to extend the term of its lease (as to any or all of the buildings) for an additional 3 years (i.e., until September 2022). TE also has the right to terminate its lease early (as to any or all of the buildings). If TE extends its lease for Building 305, there is also an incentive for TE to terminate its lease early (in September 2020); if TE elects not to exercise this early termination option and vacate Building 305 in September 2020, it will forego this incentive payment. At this time, it is anticipated that TE will vacate buildings 302, 303/304/306, and the CTF by mid-2017 and move into alternative facilities elsewhere in the Bay Area. However, TE is not anticipated to vacate Building 305 until later (i.e., in September 2020 – assuming TE elects to take advantage of its incentive option - or September 2022 at the latest). Facebook has had discussions with TE regarding a potential early termination of the lease of Building 305, too, but as of this time it does not appear that TE will surrender that building early as it remains necessary for TE's operations.

Because it now appears that TE may remain in Building 305 until September 2020 (or 2022 at the very latest), Facebook has made modifications to the site plan for Phase 2 and refined the design for Building 22 to allow construction to occur while Building 305 remains occupied. Accordingly, the anticipated construction phasing schedule is as follows:

#### **Phase 2**

Demolition of Buildings 301, 302, 303/304/306 and the CTF, would occur during Phase 2. It is anticipated that demolition would begin in mid-2017. Construction of the parking structure would start in late 2017 with grading and utility work followed immediately by the foundations and structure. The parking structure is anticipated to be complete in early 2019. Construction of Building 22 would start in mid-2018 with grading and utility

work. Foundations would start in mid-2018, construction of the core and shell would start in late 2018, and tenant improvements would start in mid-2019. Construction of Building 22 is expected to be complete by late 2019.

### **Phase 3**

Phase 3 would begin upon demolition of Building 305 which is anticipated to occur in late 2020 or late 2022 depending on when TE vacates the site.

If TE vacates Building 305 in September 2020, then construction of the hotel could start in late 2020 with demolition of the surface parking. Grading and utility work would start in early 2021, foundations would start in mid-2021, and construction of the core and shell would start in late 2021. Construction of the hotel would take approximately 18 months, with full buildout by mid-2022. Completion of the open space improvements and recharging facilities is anticipated to take approximately 12 months would be completed by late 2021.

If TE vacates Building 305 in September 2022, then construction of the hotel could start in late 2022 with demolition of the surface parking. Grading and utility work would start in early 2023, foundations would start in mid-2023, and construction of the core and shell would start in late 2023. Construction of the hotel would take approximately 18 months, with full buildout by mid-2024. Completion of the open space improvements and recharging facilities would be completed by late 2023.

### **3. Other Timing Considerations under the Project Approvals.**

Neither the CDP nor the DA requires a specific phasing schedule (e.g., that demolition of Building 305 precede construction of Building 22). Section 4.4 (Timing) of the DA expressly states that “. . . no moratorium or other limitation affecting the development and occupancy of the Project or the rate, timing or sequencing thereof shall apply to the Project.” The only requirement with respect to TE’s lease occurs under the DA, which provides that Facebook’s transient occupancy tax guarantee payment obligation is triggered two years after the “TE Vacation Date.” That term is defined as the date lease agreement between Facebook and Tyco Electronics Corporation has been terminated and TE has vacated all buildings leased by TE on the Property, which is September 2022 (assuming TE exercises its extension option) unless TE agrees to terminate the lease and vacate the buildings early.

However, the City has expressed some concern about further delays that might arise if TE were able to extend the term of its lease beyond what is now permitted under the lease and a desire for more certainty surrounding the ultimate demolition of Building 305. In response to that concern, Facebook will commit to demolishing Building 305 within twelve months of the date that TE vacates Building 305 (subject to receipt of all applicable permits and approvals, including any permits and approvals from the Department of Toxic Substances Control and/or the U.S. Environmental Protection Agency). In addition, Facebook agrees not to renew or extend TE’s lease beyond the outside expiration date of September 2022 (assuming that TE exercises its existing renewal option and foregoes its incentive to vacate in 2020).

### **III. Building 22 Design and Changes to the Project**

In late 2016, Facebook requested that Gehry Partners undertake a reevaluation of the conceptual drawings for Building 22. Gehry Partners and Facebook determined that the footprint of the overall building could be reduced in size without altering the overall envelope or impact of the project, such that Building 22 would remain well within the previously approved footprints and square footage envelopes while accommodating TE's continued use of Building 305 through the end of its lease.

The revised project conforms to the permitted FAR limits, setback requirements, building coverage and minimum open space requirements, maximum height limit of 75', and total number of parking spaces permitted. In addition, the revised project does not require any change in the permitted uses, density or intensity of uses, provisions for the reservation or dedication of land, restrictions or requirements relating to subsequent discretionary actions, any monetary obligations of Facebook, or any conditions or covenants limiting or restricting the use of the site.

The only change with respect to the overall site calculations is that total building coverage (at full buildout) will be reduced from approximately 1,311,977 square feet (52% of the site) to 1,019,293 square feet (approx. 40% of the site). During the interim phase where Building 305 remains on-site, total building coverage will be approximately 1,216,530 square feet (approximately 48% of the site).

With respect to FAR limits, construction of Building 22 would not exceed the site's .45 FAR requirement for office uses. While Building 305 remains, total FAR on-site would remain below the site's .55 FAR limit. As contemplated in the Project Approvals, including the Development Agreement, construction of the hotel could commence after TE fully vacates the site, which would ensure that at no point in time would the site's FAR exceed 0.55.

#### **1. Building 22 Design Narrative**

The design for Building 22 contemplates a 75' tall, four-story office building with an adjacent parking structure to the west. Access to the building will be provided through lobbies that are located at the east, center, and west ends of the building. A bus and tram stop/terminal will be located north of the building. Pedestrian bridges will connect the west lobbies to the parking structure. The design approach aims to provide a highly functional office building for Facebook while respecting the characteristics of the adjacent neighborhood. The office program includes a variety of conference meeting rooms, offices, food service venues, and extensive support spaces, consistent with the program analyzed for Building 22 in the EIR. It is the design intent to bring as much natural light as possible into the office work spaces, including through the use of an interior atrium space that would extend throughout the building, and provide easy access to the outdoors. The office space is planned to consist of open floor plans totaling approximately 449,500 sf., also consistent with the CDP and EIR. The building is oriented to face the Belle Haven neighborhood south of the site, and aims to enhance the local environment with California native vegetation.

In addition, Facebook is anticipating requesting clarifications in the CDP to permit architectural skylights that would partially extend beyond the 75' foot height limit, which may entail corresponding revisions in the CDP. The current design anticipates that the skylights would not extend higher than the proposed rooftop mechanical equipment, and would not create additional visual obstruction.

Facebook therefore requests that this design feature be included as part of the City's design review process.

## **2. Parking Structure**

As discussed above, parking would be consolidated in a new 8-story, 75' tall parking structure (measured from average finished grade to the roof deck pursuant to Section 2.5 of the CDP), with rooftop railing and screening, elevator hoistways, and a solar canopy that would extend above the height limit as permitted under the CDP. The parking structure is currently anticipated to accommodate approximately 1,736 spaces, which conforms to the CDP's parking requirements.

With respect to the hotel, no change in the parking configuration is anticipated. Approximately 245 parking spaces would be provided at grade below the hotel and would be available for hotel employees and guests, consistent with the analysis provided in the EIR.

## **3. Intensity of Use**

No change in the intensity of the uses on-site is anticipated. While the existing conditions at Building 305 could remain for some limited duration while Building 22 is occupied, the total number of workers associated with Building 305 is well below the projected number of workers, visitors and guests for the hotel, and the site would remain subject to the peak hour and daily trip caps established in the CDP that apply to the entire site (including the existing buildings).

## **4. Site Access and Circulation**

No changes are proposed with respect to site access. Circulation on the western portion of the site would be refined to reflect the consolidation of parking for the office buildings in a new parking structure. Preliminary analysis from Fehr & Peers has confirmed that the proposed circulation would not create queuing issues or modify the conclusions reached in the traffic analysis conducted for the EIR. Truck access would continue to be provided through a controlled driveway on Chilco Street at Constitution Drive, and would serve TE's interim use of Building 305 so long as it occupies the premises.

## **5. Landscaping, Site Design and Open Space**

Compared to the proposed site plan evaluated in the EIR, the refined site plan would increase the amount of landscaping and other pervious materials on-site, and result in additional natural areas including stormwater treatment areas. The additional landscaped area would provide passive recreational space for workers, as well as provide flexibility and reserve space for potential future uses. No reduction in the amount of replacement trees is sought.

## **6. Final Site Plan post-Building 305 Demolition**

Although the scope of improvements that would be installed after Building 305 is demolished are still undergoing refinement, the current proposal is to replace the existing asphalt parking lots with additional landscaping, a shuttle and tram drop-off area, as well as recharging facilities for Facebook's shuttle buses and trams in an area previously identified as a surface parking lot. The unenclosed facility

would be screened and accommodate approximately 50 shuttle buses and up to 23 trams in the area located east of Building 23.

#### **IV. Modifications to the CDP**

Section 6 of the CDP addresses permitted modifications to the approved project plans and identifies four different types of modifications that are permitted, each of which follows a distinct approval process.

##### **A. “Substantially Consistent Modifications”**

Under Section 6.1.1 of the CDP, “substantially consistent modifications” to the project may be approved by the Community Development Director based on a determination that the proposed modifications are in substantial compliance with and/or substantially consistent with the Project Plans and the Project Approvals. Substantially consistent modifications are generally not visible to the public and do not affect permitted uses, density or intensity of use, restrictions and requirements relating to subsequent discretionary actions, monetary obligations, or material modifications to the conditions of approval.

##### **B. “Minor Modifications”**

Under Section 6.1.2 of the CDP, “minor modifications” to the approved plans may be approved by the Community Development Director (subject to Planning Commission review). Minor Modifications are similar to substantially consistent modifications, except that Minor Modifications are generally visible to the public and result in minor exterior changes to the Project aesthetics.

##### **C. “Major Modifications”**

Under Section 6.1.3 of the CDP, “major modifications” are defined as:

“[C]hanges or modifications to the Project that are not in substantial compliance with and/or substantially consistent with the Project Plans and Project Approvals. Major modifications include, but are not limited to, significant changes to the exterior appearance of the buildings or appearance of the Property, and changes to the Project Plans, which are determined by the Community Development Director (in his/her reasonable discretion) to not be in substantial compliance with and/or substantially consistent with the Project Plans and Project Approvals.”

Major modifications are subject to Planning Commission review and approval, based on a determination that the proposed modifications are compatible with other building and design elements or onsite/offsite improvements of the approved CDP and will not have an adverse impact on safety or the character and aesthetics of the site.

##### **D. Modifications that Require Council Approval and CDP Amendment**

Lastly, Section 6.1.5 of the CDP addresses three types of changes which would require a public amendments to the CDP by the City Council. These three types of changes which require Council approval include:

- Revisions to the project which involve relaxation of the development standards identified in Section 2;
- Material changes to the uses identified in Section 3; or
- Material modifications to the conditions of approval identified in 7 (Trip Cap), 9 (General Project Conditions), 10 (Undercrossing Improvements), 11 (Bicycle and Pedestrian Bridge), 12 (Public Open Space), 13 (On-Site Recycled Water), 14 (Access Parcel) or 15 (Mitigations Carried Forward from Building 20 Approval).

**E. The Changes to the Site Plan and Incorporation of a Parking Structure Constitute Conditional Development Permit Amendment to the CDP.**

As described above, Facebook is requesting several changes to the approved project plans,<sup>1</sup> including (i) the consolidation of surface parking for Buildings 22 and 23 into a parking structure, (ii) a reduction in the building footprint for Building 22 and a change in design from a one-story structure located on a podium above surface parking to a 4-story building (with no change in height), (iii) the addition of a shuttle, bus and tram recharging facility, and (iv) a revised site and circulation plan. These modifications involve significant changes to the exterior appearance of the buildings and the appearance of the property, as well as certain schematic changes to the overall project (specifically, the inclusion of a new parking structure and a dedicated space for recharging electric bus and tram vehicles).

As set forth above, the design for Building 22 and the proposed changes to the project have been planned to conform to the development standards in the CDP and the development envelope studied in the EIR (with the exception of the parking structure). No changes in the trip cap or permitted uses (or intensity) are requested. Facebook is also not requesting any increase in square footage nor the relaxation of any development standards in the CDP. Finally, the proposed changes to the project would result in an equally compelling design scheme and no adverse impacts on health or safety.

No revisions to relax the development standards in Section 2 of the CDP are sought, and Facebook is not seeking any material changes to the uses identified in Section 3. Facebook is also not seeking any material modifications to the conditions of approval. However, based on conversations with City staff, the proposed changes may require amending the following provisions of the CDP:

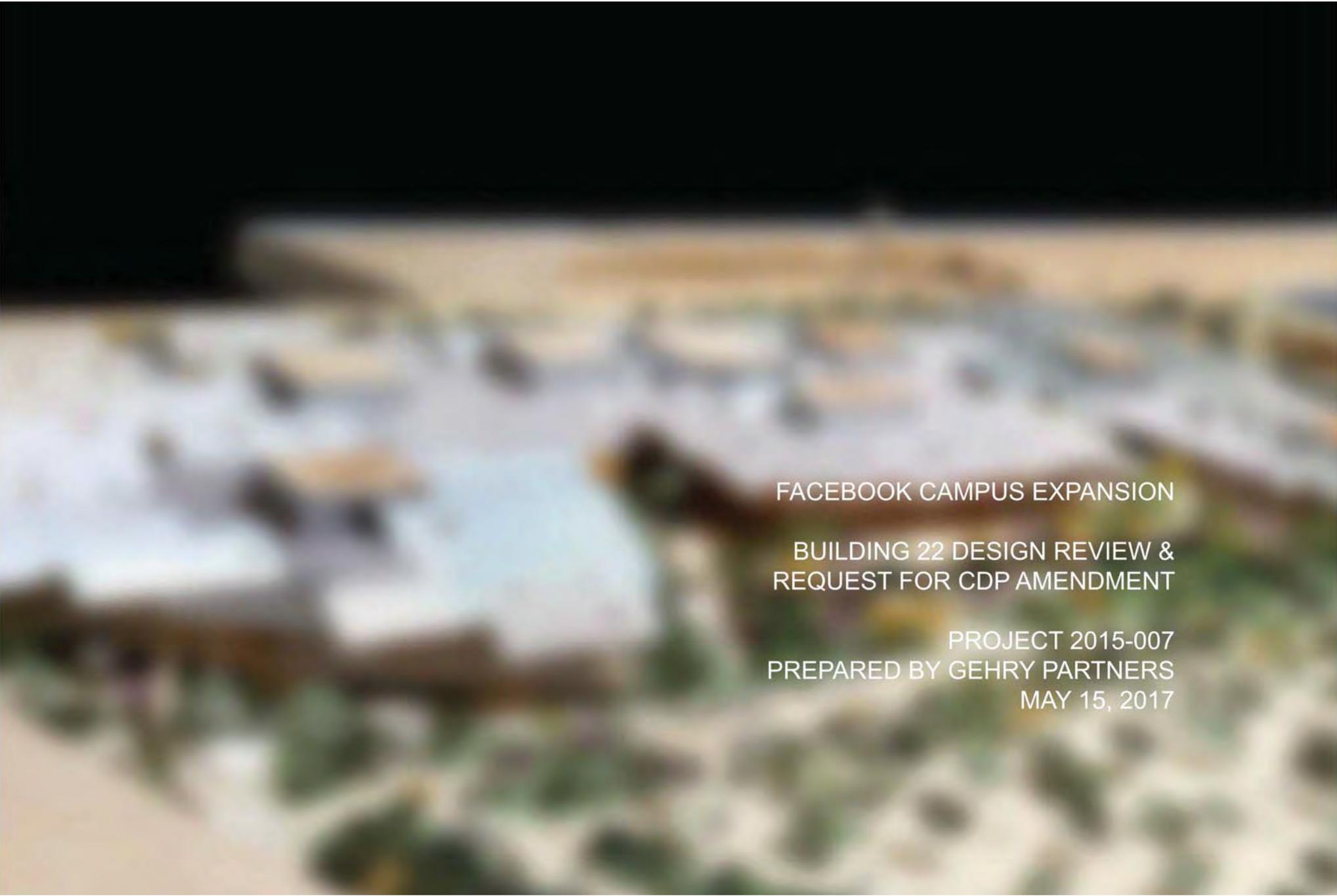
- **Section 1 (General Information):** revise the general description of the project to include references to a parking structure and the proposed bus/tram electric recharging space; clarify

<sup>1</sup> The CDP defines “Project Plans” as the “plans submitted by Gehry Partners, LLC dated September 20, 2016 consisting of 127 plan sheets, recommended for approval to the City Council by the Planning Commission on September 26, 2016 (Project Plans), and approved by the City Council on November 1, 2016, except as modified by the conditions contained herein and in accordance with Section 6 (Modifications) of [the CDP].”

that the existing structures on-site may continue to be occupied pending redevelopment of the site.

- **Section 2 (Development Standards):** No changes to the development standards, but amend the description of the project to make explicit reference to a parking structure and the proposed bus/tram electric recharging space. In addition, Facebook is requesting an additional exclusion from the building height limits to accommodate architectural skylights (provided that they extend no higher than any rooftop mechanical equipment). It may also be necessary to clarify that perimeter safety railings on the top level of the parking structure are permitted to exceed the height limit.
- **Section 3 (Uses):** Clarify that permitted uses include existing uses on-site (i.e., occupancy of the existing buildings by tenants prior to redevelopment), and that a bus/tram electric recharging space is an ancillary use.
- **Section 7 (Trip Cap):** Conforming changes to clarify that the trip cap applies to the entire TE Site, consistent with the existing Trip Cap Monitoring and Enforcement Policy.
- **Section 9 (Project Specific Conditions):** Technical changes to clarify that certain conditions apply to each “phase” of development as opposed to each “building,” and proposed changes to permit the partial use of Tier 2 and/or Tier 3 pile rigs modified with diesel particulate filters (with all remaining equipment to remain Tier 4), with no material difference in air quality emissions. The City previously approved the use of modified Tier 2 and Tier 3 pile rigs for Building 21 as a “substantially consistent modification” to the CDP after confirming that no material change in air quality emissions would occur; this analysis was peer reviewed by the City’s independent consultant, ICF.

In addition, to the extent that Building 305 will remain occupied by TE for an interim period while Buildings 21 and 22 are constructed and occupied, Building 305’s use is considered a non-conforming use and is thus permitted under the City’s zoning rules. To the extent that the phasing of demolition and building permits differs from the chronology contemplated in the EIR, Section 8 of the CDP gives the City Building Official the authority to determine the sequencing of building permits and sub-phases for each building/phase of construction. Nonetheless, clarifying revisions may be appropriate to clarify that Building 305 may remain occupied for the duration of TE’s lease term while other phases of the project are constructed.

An aerial photograph of a large, multi-colored building complex, likely a campus expansion. The building features a central courtyard area with a light blue and white color scheme. The surrounding area is a mix of greenery and paved surfaces. The image is slightly blurred, emphasizing the overall layout and color palette of the building.

FACEBOOK CAMPUS EXPANSION

BUILDING 22 DESIGN REVIEW &  
REQUEST FOR CDP AMENDMENT

PROJECT 2015-007  
PREPARED BY GEHRY PARTNERS  
MAY 15, 2017

# ***FACEBOOK CAMPUS EXPANSION***

*Prepared By:*  
*Gehry Partners, LLP*

***BUILDING 22 DESIGN REVIEW &  
REQUEST FOR CDP AMENDMENT***  
*MAY 15, 2017*

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A1-02	PROPOSED REGIONAL PLAN INTERIM PHASE	1"=300'
A1-03	PROPOSED REGIONAL PLAN FINAL PHASE	1"=300'
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A3-06	BUILDING 21 ELEVATIONS	1"=25'-0"
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A3-08	BUILDING 22 NORTH & SOUTH ELEVATIONS	1"=50'-0"
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A3-15	PARKING STRUCTURE EAST BUILDING ELEVATION	1/16"=1'-0"
A3-16	PARKING STRUCTURE WEST BUILDING ELEVATION	1/16"=1'-0"
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T1-11	INTERIM PHASE EMPLOYEE INTRA CAMPUS TRAM	AS NOTED
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<b>L1</b>	<b>LANDSCAPE PLANS</b>	
L1-01	PROPOSED LANDSCAPE PLAN INTERIM PHASE	AS NOTED
L1-02	PROPOSED LANDSCAPE PLAN FINAL PHASE	AS NOTED

	Previously Approved Project	MPK 22 Design Review & CDP Amendment Interim Phase	MPK 22 Design Review & CDP Amendment Final Phase
SITE AREA TOTAL (SF)	3,503,608	3,504,041	3,504,041
MPK20 Site AREA (SF)	963,680	959,565	959,565
TE SITE AREA (SF)	2,539,928	2,544,476	2,544,476
Building Coverage (SF)	1,838,665	1,743,136	1,545,961
Building Coverage (%)	52%	50%	44%
MPK20 (GFA)	433,555	433,555	433,555
MPK21 (GFA)	512,900	512,900	512,900
MPK22 (GFA)	449,500	449,500	449,500
MPK23 (GFA)	180,108	180,108	180,108
BLDG 305 (GFA)		289,718	
Hotel (GFA)	174,800		174,800
Total Office (GFA)	1,576,063	1,576,063	1,576,063
Office FAR	0.45	0.45	0.45
Total Mixed Use (GFA)	1,750,863	1,865,781	1,750,863
Mixed Use FAR	0.50	0.53	0.50
Parking Stalls			
MPK21	1,476	1,495	1,495
MPK22	1,294	0	0
MPK23	518	57	57
Parking Garage		1,736	1,736
Hotel	245		245
Total Parking Stalls	3,533	3,288	3,533

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1 HENDERSON WAY  
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

TITLE  
PROJECT DATA

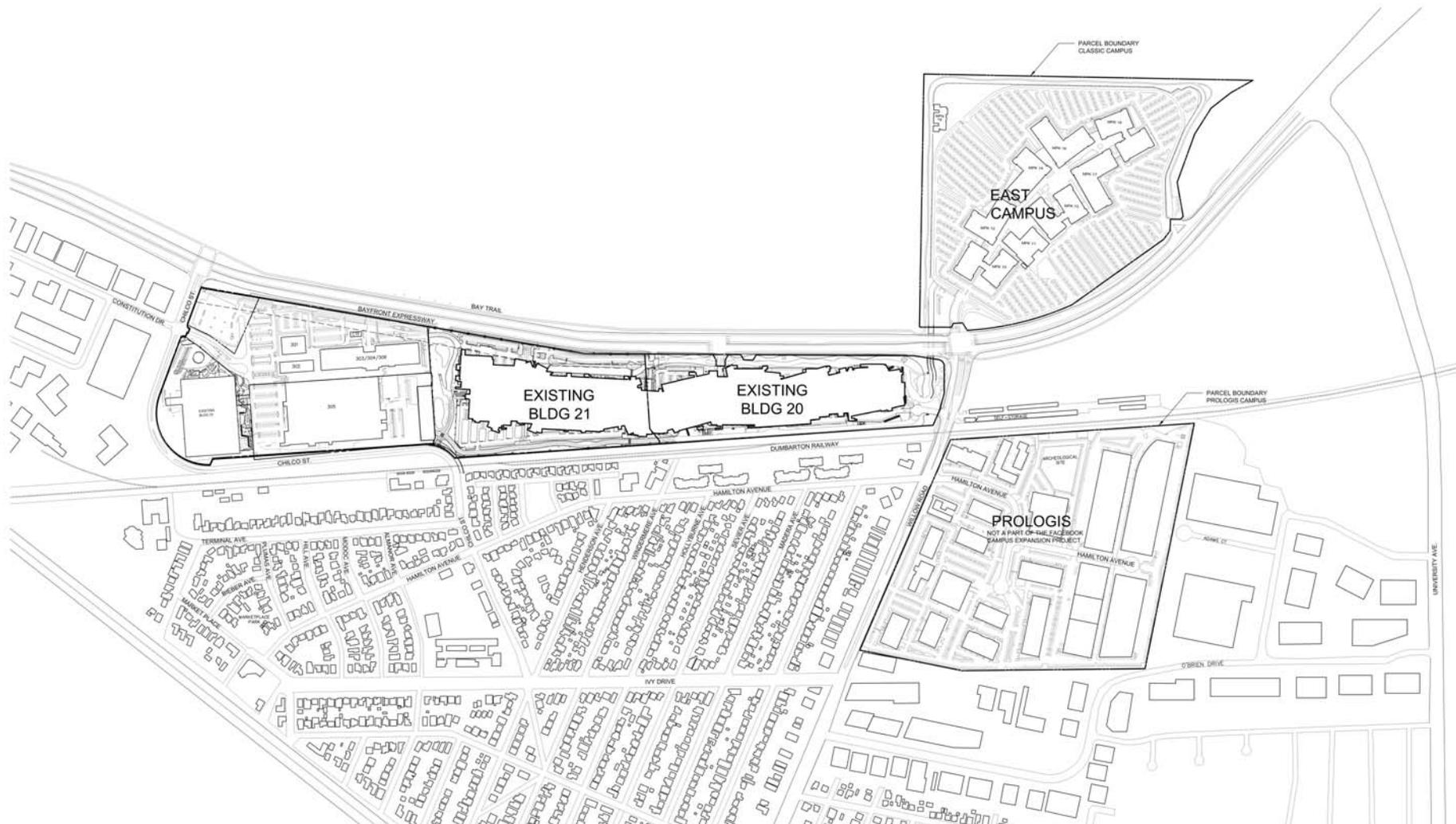
PROJECT NUMBER  
2015-007  
SCALE

SHEET NUMBER

A0-01

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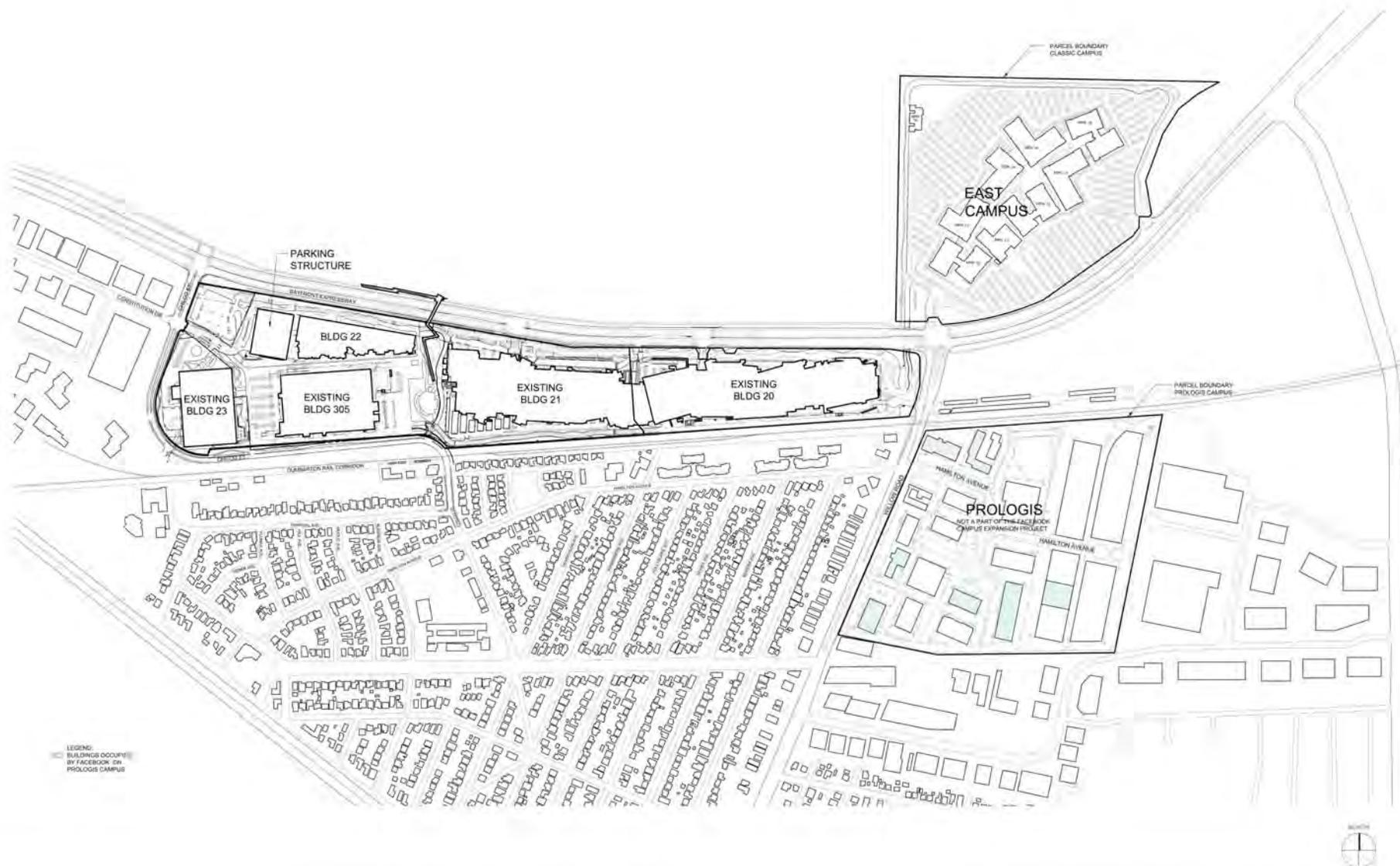
**FACEBOOK**  
 OWNER  
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 Facebook Building 21, 22 & Hotel Site  
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TITLE  
**EXISTING REGIONAL PLAN**

PROJECT NUMBER  
 2015-007  
 SCALE  
 1" = 300'  
 MAY 15, 2017  
11x17 SHEET SIZE: N.T.S.  
 ORIGINAL SHEET SIZE: 24" x 36"

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**A1-01**  
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**PROPOSED REGIONAL PLAN**  
 INTERIM PHASE

PROJECT NUMBER  
 2015-007

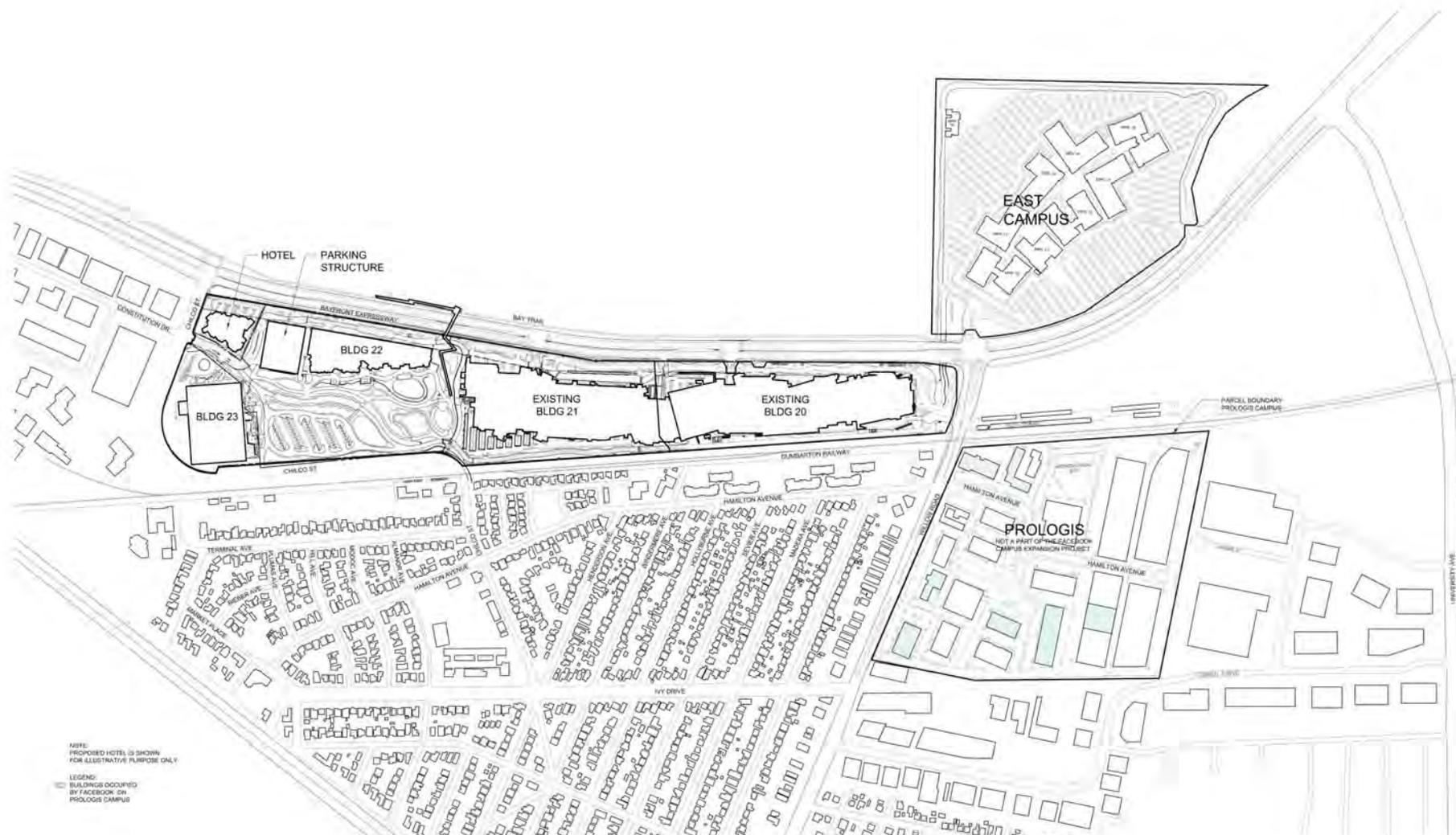
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DATE  
 MAY 15, 2017

DRAWING NO.  
 1718

SHEET NO.  
 147 OF 147

SHEET NUMBER  
**A1-02**



NOTE:  
PROPOSED HOTEL IS SHOWN  
FOR ILLUSTRATIVE PURPOSE ONLY

LEGEND:  
BUILDINGS OCCUPIED  
BY FACEBOOK ON  
PROLOGIS CAMPUS

GEHRY PARTNERS, LLP  
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1001 BAYVIEW AVENUE  
SUITE 1000  
COLUMBIA, SC 29912  
(803) 733-1000

FACEBOOK  
OWNER  
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WASHINGTON, DC 20036

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Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

PROPOSED REGIONAL PLAN  
FINAL PHASE

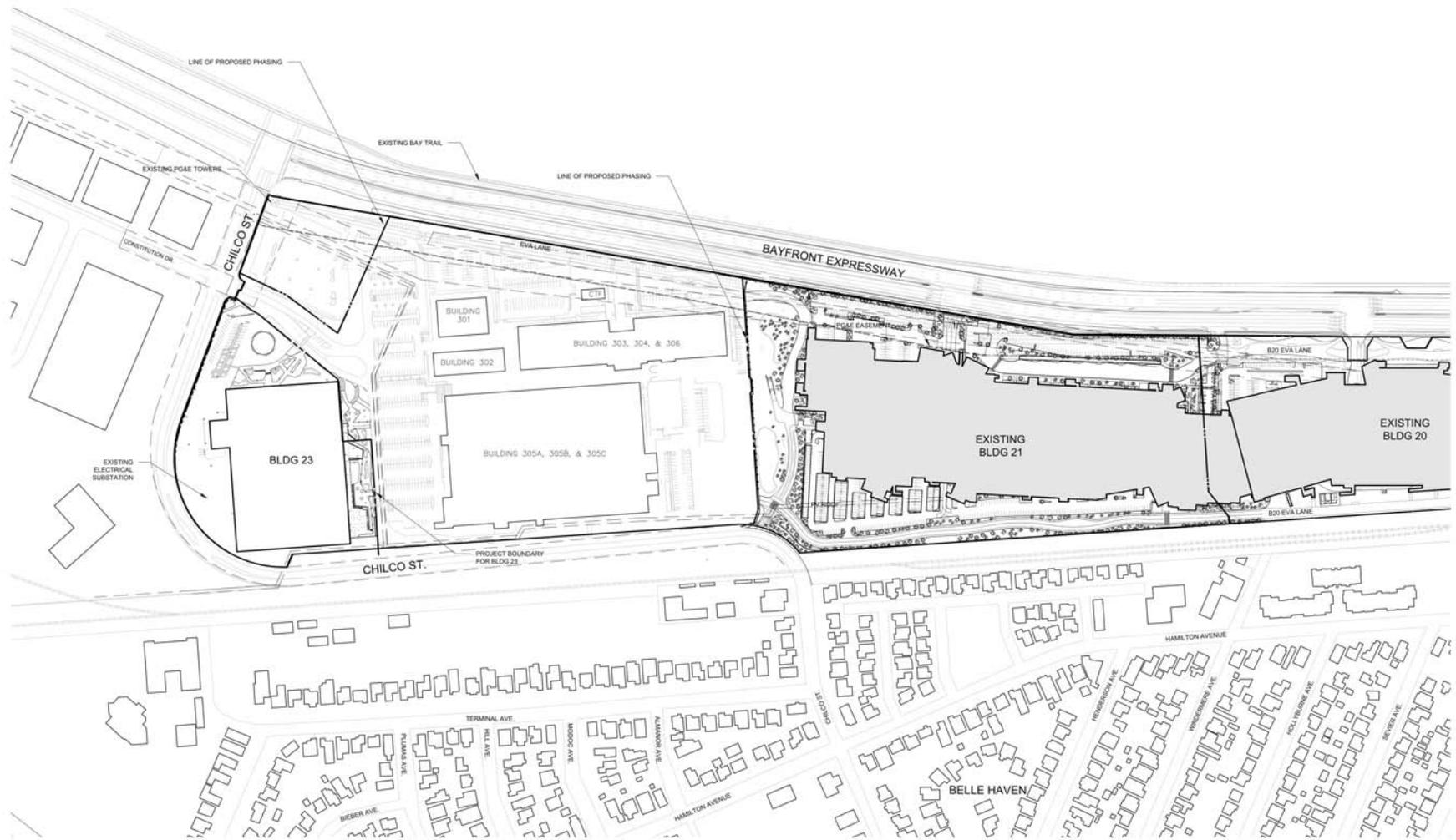
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2015-007

SCALE:  
1" = 300'

MAY 15, 2017

DATE PLOTTED: 5/15/17  
PLOTTER: HP PLOTTER 3477 44" x 36"

PROJECT NUMBER:  
A1-03



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Facebook Campus Expansion  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

TITLE  
 EXISTING SITE PLAN

PROJECT NUMBER  
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 MAY 15, 2017  
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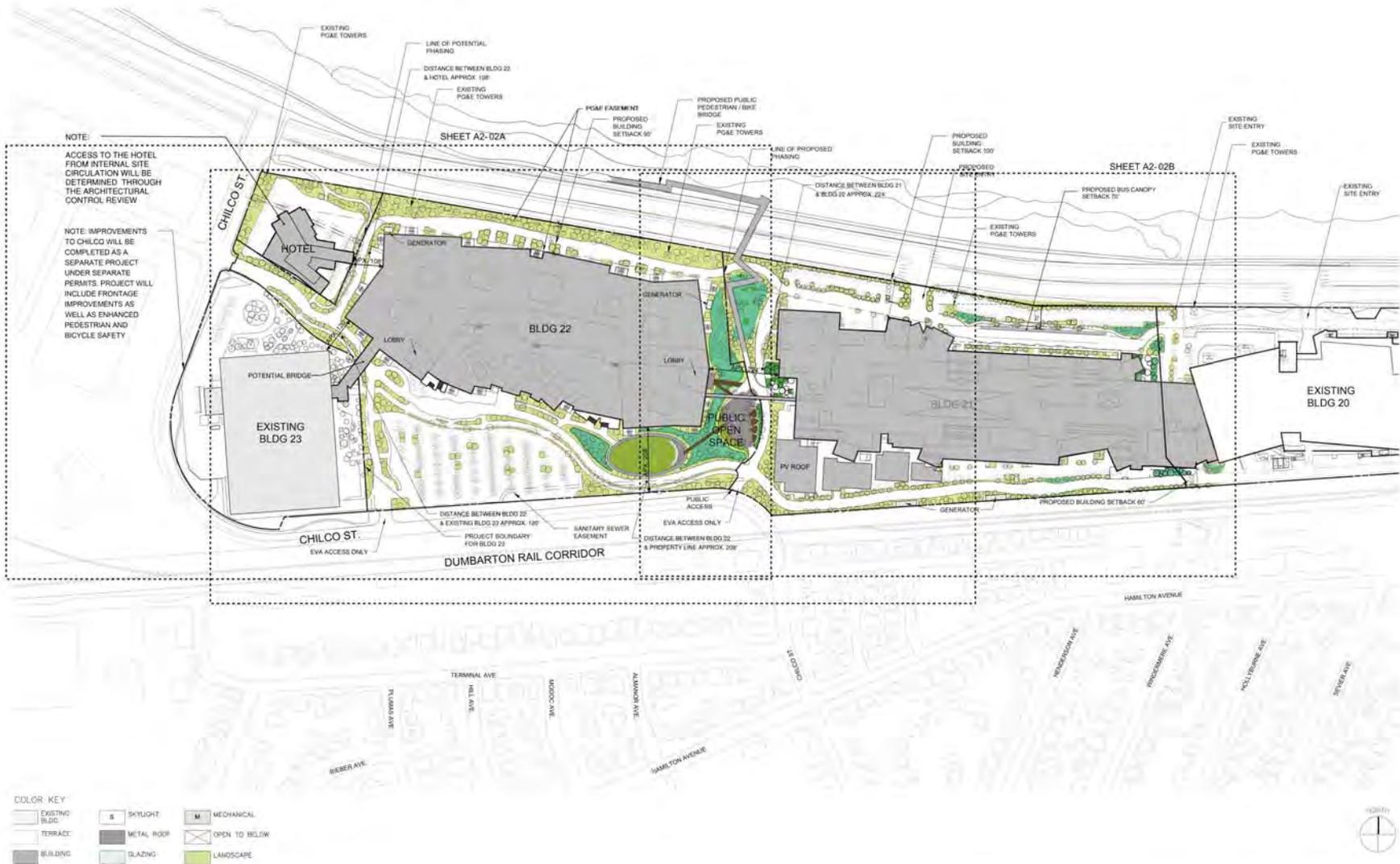
SHEET NUMBER

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# Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

PREVIOUSLY PROPOSED SITE PLAN | A1-14

SCALE : 1"= 150'  
 11X17 SCALE IS 1"=300'  
 SEPTEMBER 02, 2016



1 LEVEL 1  
1/32" = 1'-0"

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 LOS ANGELES, CALIFORNIA 90068  
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 OWNER  
 1 HACKER WAY  
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**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

TITLE  
**BUILDING 22**  
**FLOOR PLAN LEVEL 01**  
  
 PROJECT NUMBER  
 2015-007  
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 MAY 15, 2017  
THIS SHEET SIZE: N.T.S.  
 ORIGINAL SHEET SIZE: 34" x 36"

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LEGEND  
 ATRIUM  
 TERRACE

1 LEVEL 2  
 1/32" = 1'-0"

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 OAKLAND, CALIFORNIA 94612  
 (415) 435-3000

FACEBOOK  
 OWNER  
 1700 CALIFORNIA AVENUE  
 MENLO PARK, CALIFORNIA 94025

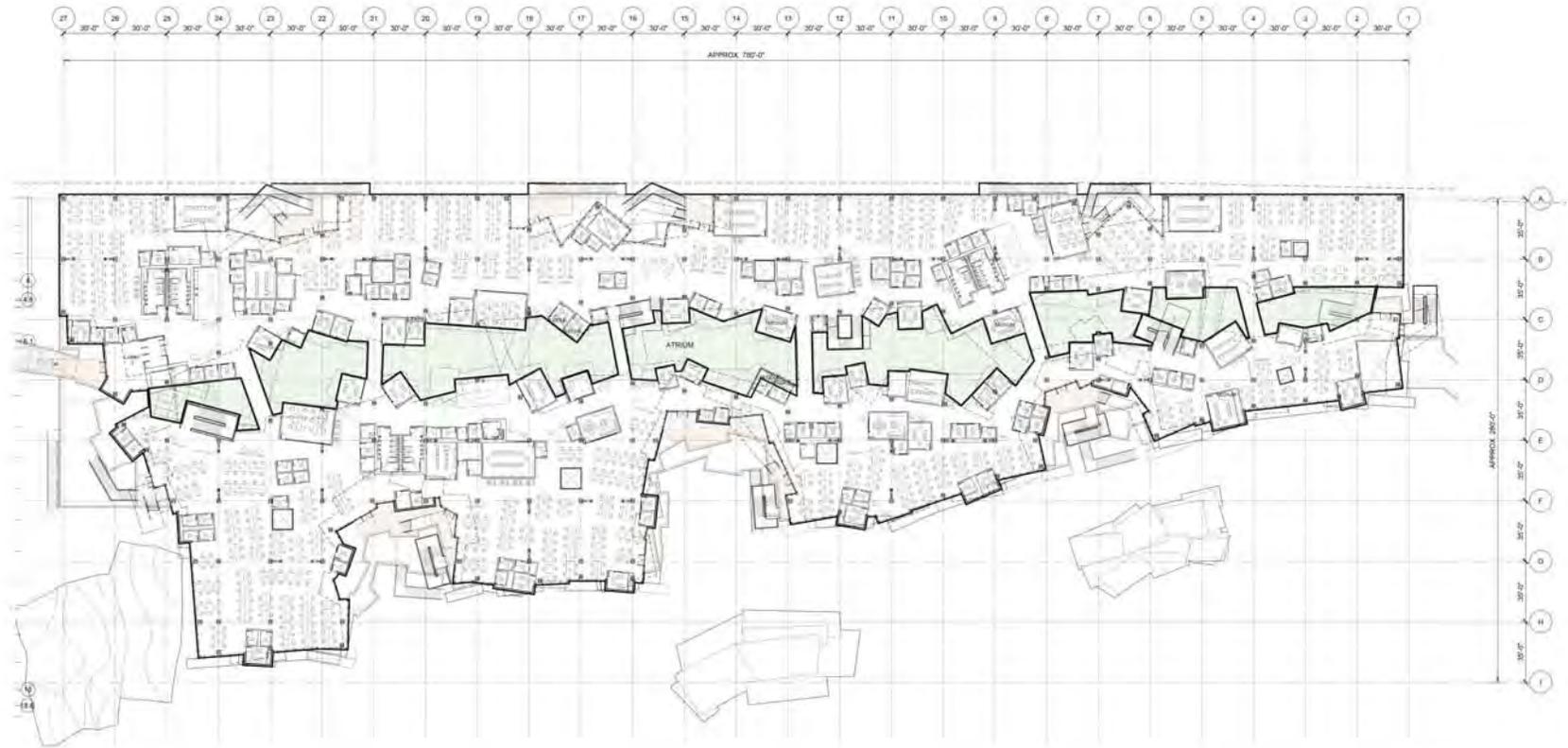
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Facebook Campus Expansion  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

BUILDING 22  
 FLOOR PLAN LEVEL 02

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 2015-007  
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 DATE:  
 MAY 15, 2017  
 TITLE SHEET SIZE: 34" x 46"  
 DRAWING SHEET NO.: 34" x 46"

SHEET NUMBER:  
 A2-02  
 DRAWING APPROVED BY:



LEGEND  
 ATRIUM  
 TERRACE

1 LEVEL 3  
 1/32" = 1'-0"

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EXHIBIT/DATE

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BUILDING 22  
 LEVEL 03 FLOOR PLAN

PROJECT NUMBER:  
 2015-007  
 SCALE:  
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 DATE:  
 MAY 15, 2017  
 11x17 SHEET SIZE: 11" x 17"  
 DRAWING SHEET NO.: 347 of 347

SHEET NUMBER:  
 A2-03

DESIGN APPROVED: [Signature]



LEGEND  
 ATRIUM  
 TERRACE

1 LEVEL 4  
 1/32" = 1'-0"

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 SUTHERLAND, CALIFORNIA 90589  
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 MENLO PARK, CALIFORNIA 94025

EXHIBIT/DATE

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 Facebook Building 21, 22 & Hotel Site  
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BUILDING 22  
 LEVEL 04 FLOOR PLAN

PROJECT NUMBER  
 2015-007

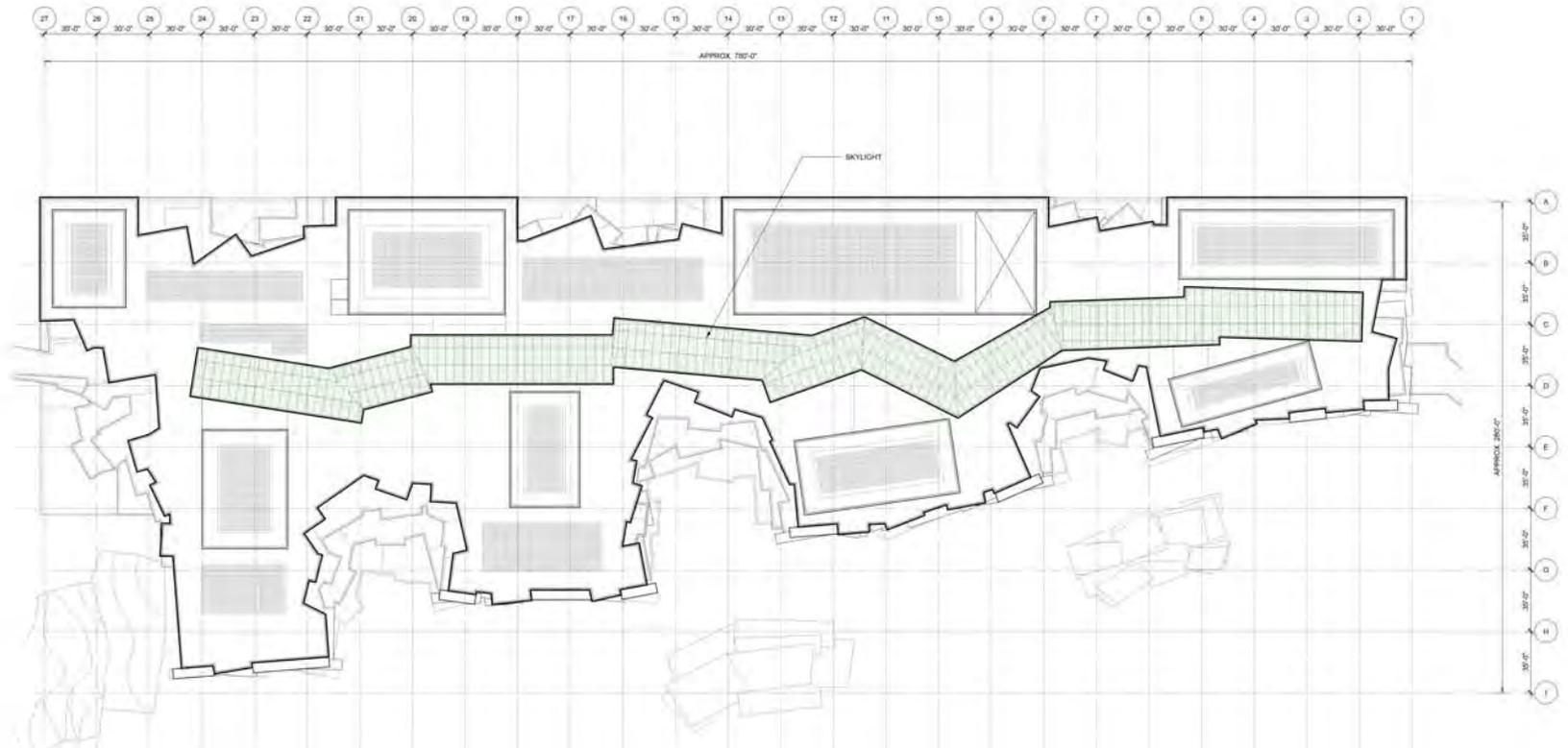
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MAY 15, 2017

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SHEET NUMBER  
 A2-04

DESIGN APPROVED: [Signature]



1 ROOF  
1/32" = 1'-0"

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(916) 482-3042

FACEBOOK  
OWNER  
17000 HWY 1  
MENLO PARK, CALIFORNIA 94025

DISPERSED

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
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BUILDING 22  
ROOF PLAN

PROJECT NUMBER:  
2015-007

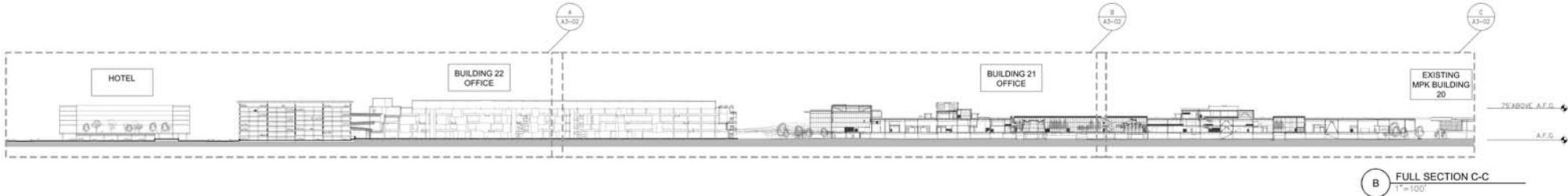
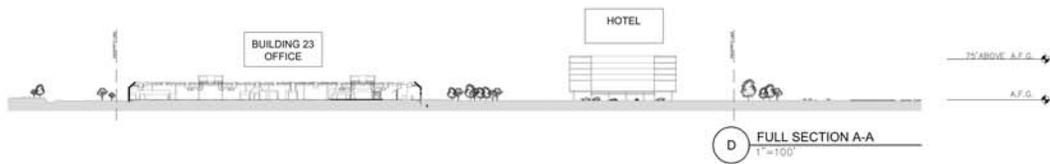
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MAY 15, 2017

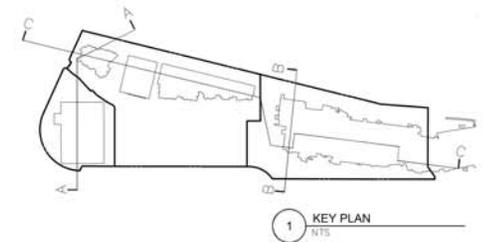
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SHEET NUMBER:  
A2-05

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- NOTES:**
1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY
  2. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT



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FACEBOOK  
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MENLO PARK, CALIFORNIA 94025

CONSULTANTS

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

TITLE  
BUILDING 21, 22 & HOTEL  
SITE SECTIONS

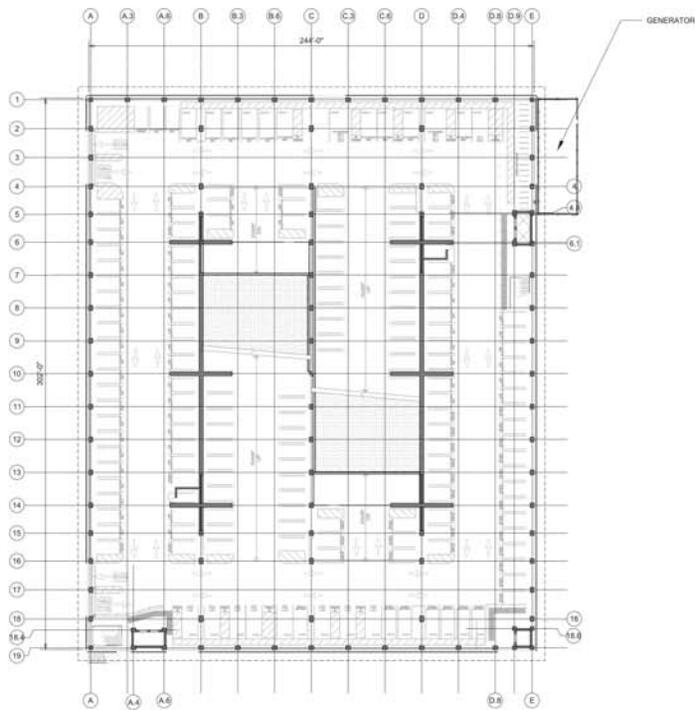
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MAY 15, 2017  
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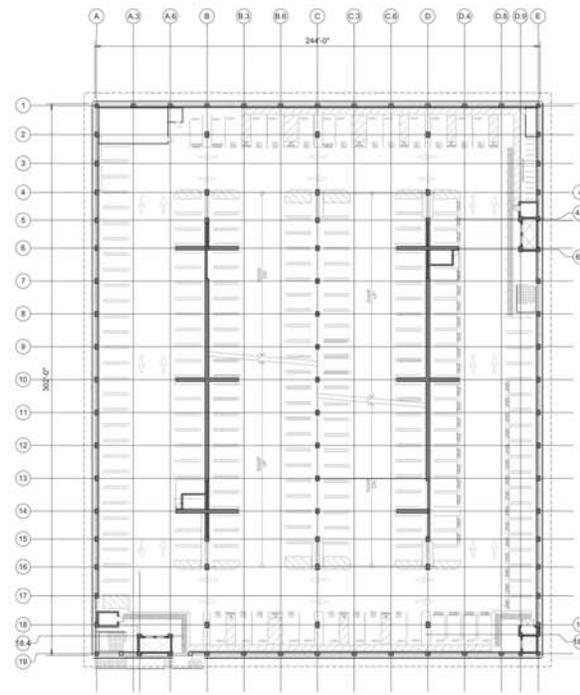
SHEET NUMBER

A3-01

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1 LEVEL 1  
1/32"=1'-0"



2 LEVEL 2  
1/32"=1'-0"

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FACEBOOK  
OWNER  
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MENLO PARK, CALIFORNIA 94025

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TITLE  
LEVEL 1 & 2  
MPK-22

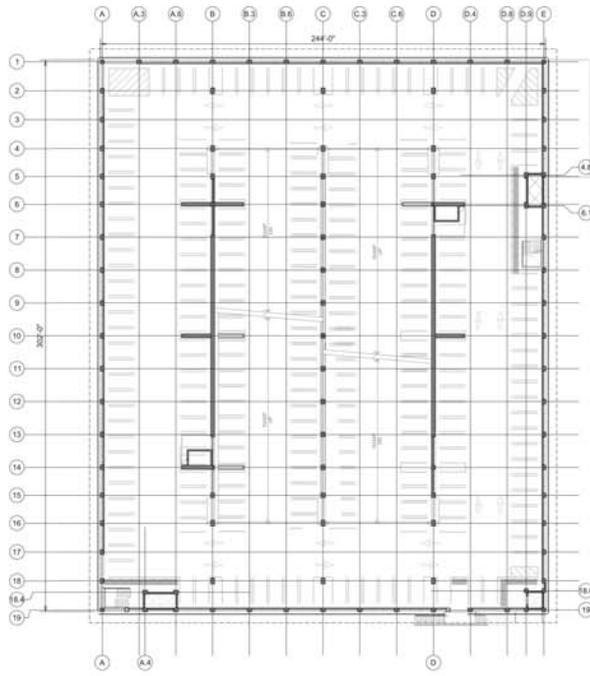
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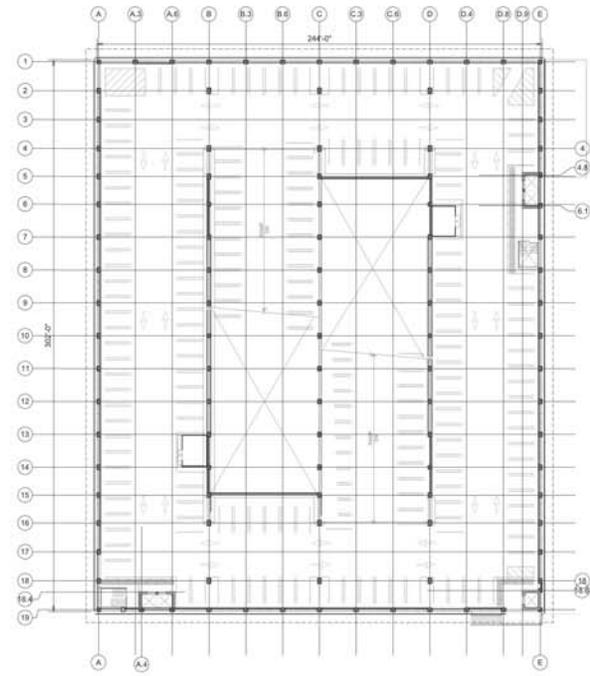
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1 TYP. FLOOR 3-7  
1/32" = 1'-0"



2 LEVEL 8  
1/32" = 1'-0"

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12911 BELMONT STREET  
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(310) 452-3000

CONSULTANTS

FACEBOOK  
OWNER  
1 HAYDEN WAY  
MENLO PARK, CALIFORNIA 94025

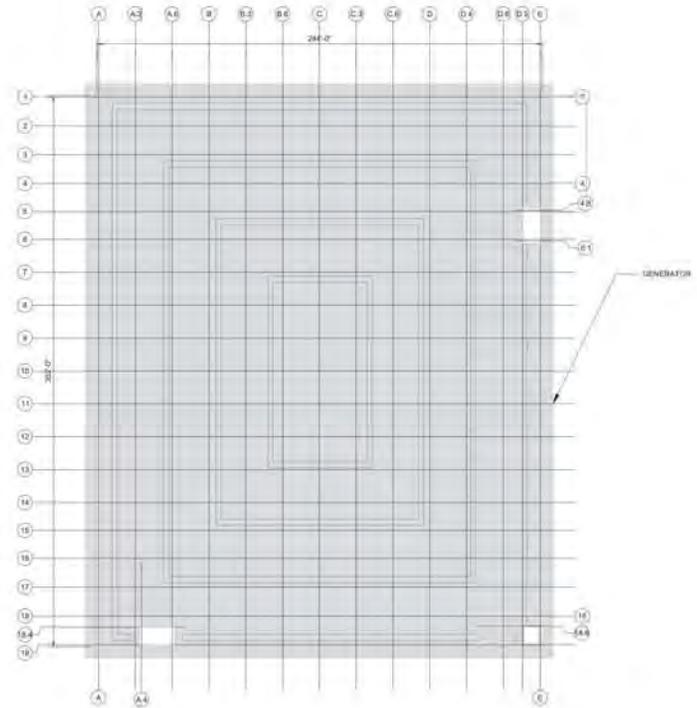
Facebook Campus Expansion  
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TITLE  
LEVEL 3 - 7  
MPK-22

PROJECT NUMBER  
2015-007  
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SHEET NUMBER  
A2-11

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1 ROOF  
17'-0" x 17'-0"

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200 WEST WASHINGTON STREET  
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415.774.3000

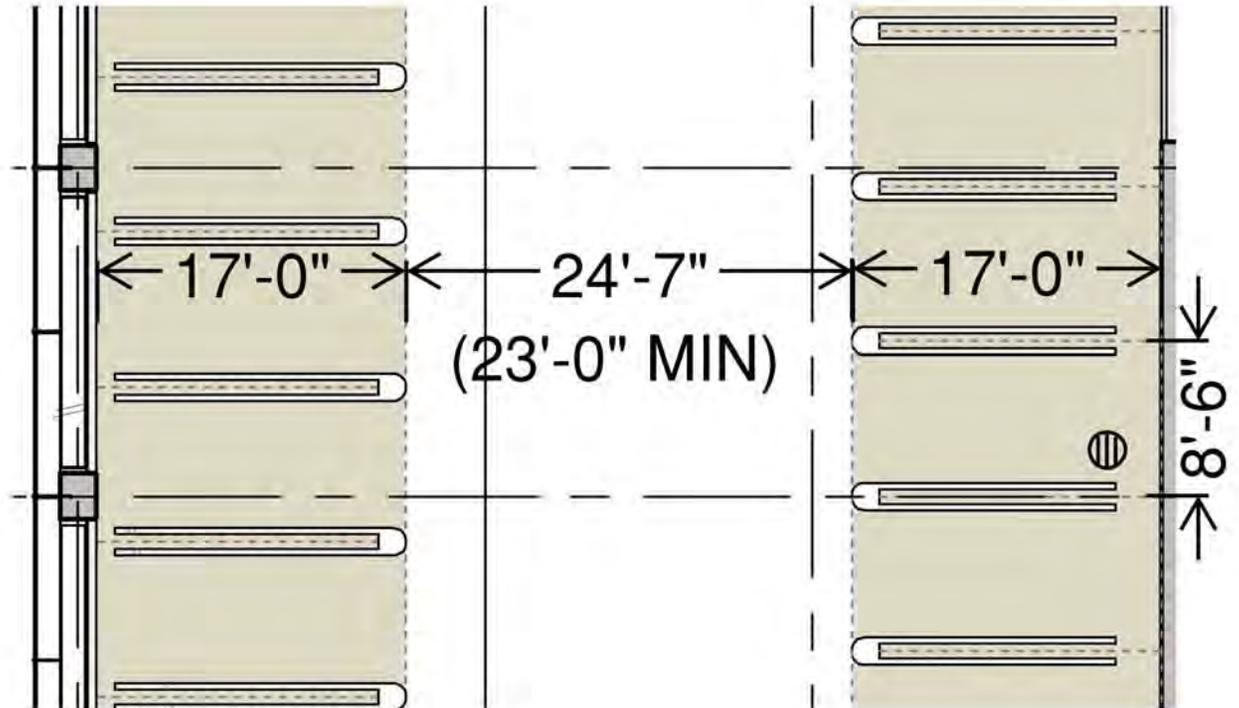
FACEBOOK  
OWNER  
1700 CALIFORNIA STREET  
SAN FRANCISCO, CALIFORNIA 94102

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ROOF  
MPK-22

PROJECT NUMBER  
2015-007  
SCALE  
1/32" = 1'-0"  
DATE  
MAY 15, 2017  
DRAWN BY: MPK  
CHECKED BY: JCB

PROJECT NUMBER  
A2-12  
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CONSULTANT

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 1600 AVENUE AVENUE  
 MENLO PARK, CA 94025

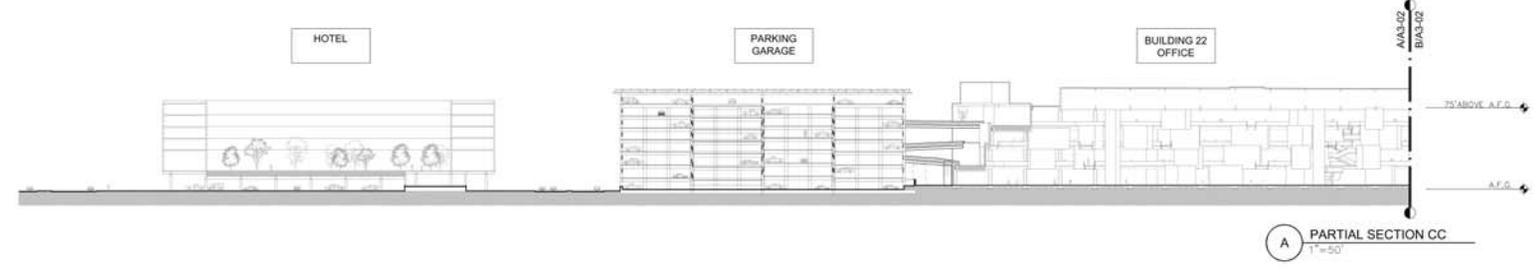
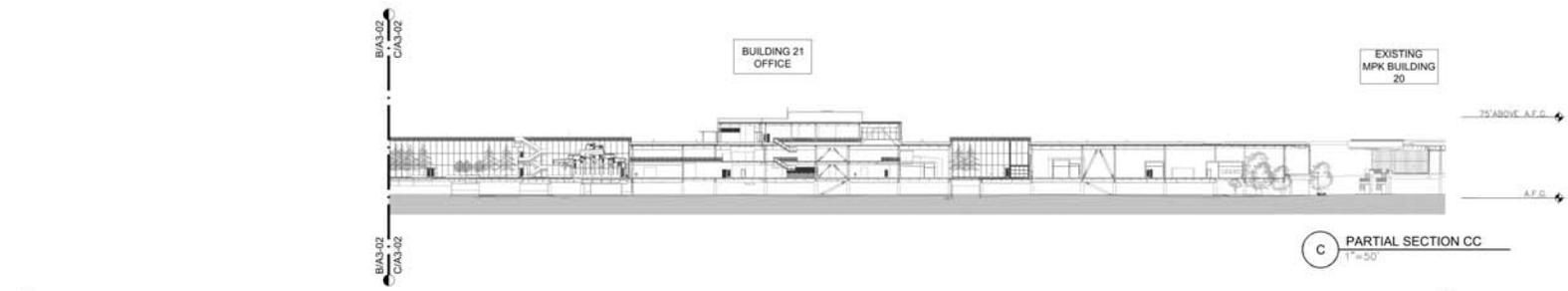
Facebook Campus Expansion  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

DETAIL OF TYPICAL  
 PARKING SPACE

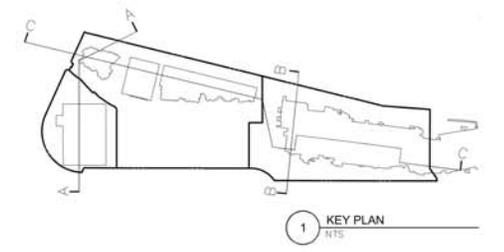
PROJECT NUMBER  
 2015-007  
 DATE  
 MAY 15, 2017  
 (11) SHEET 602 OF 615  
 (GRAPHIC SHEET 602 OF 347 OF 357)

SHEET NUMBER  
 A2-13

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**NOTES:**  
 1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY.  
 2. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT.



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 LOS ANGELES, CALIFORNIA 90008  
 (213) 482-3000

**FACEBOOK**  
 OWNER  
 1. ANDREX Bldg  
 WILCO PARK, CALIFORNIA 94225

CONSULTANTS

**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

**BUILDING 21, 22 & HOTEL**  
 SITE SECTION

PROJECT NUMBER  
 2015-007

SCALE  
 AS NOTED

MAY 15, 2017

HAFT SHEET SIZE: A1A  
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER  
**A3-02**

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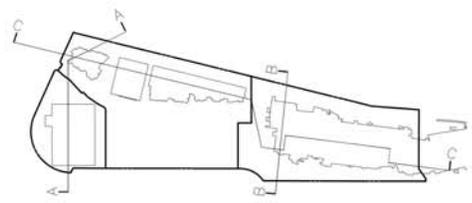


**B** FULL SECTION B-B  
1"=50'



**A** FULL SECTION A-A  
1"=50'

**NOTES:**  
 1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY.  
 2. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT.

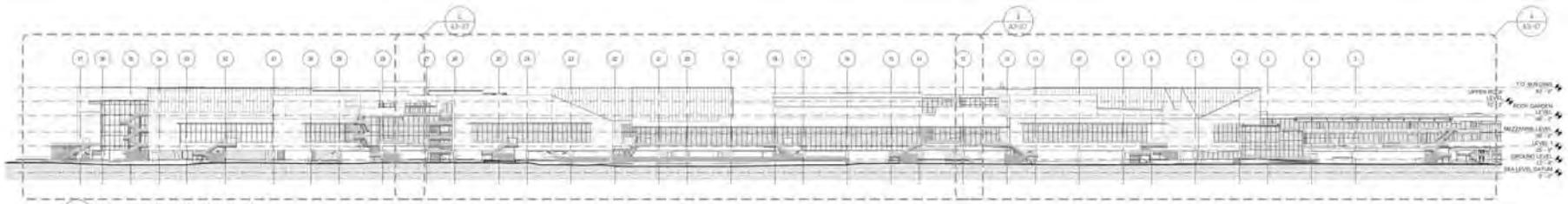


**1** KEY PLAN  
N.T.S.

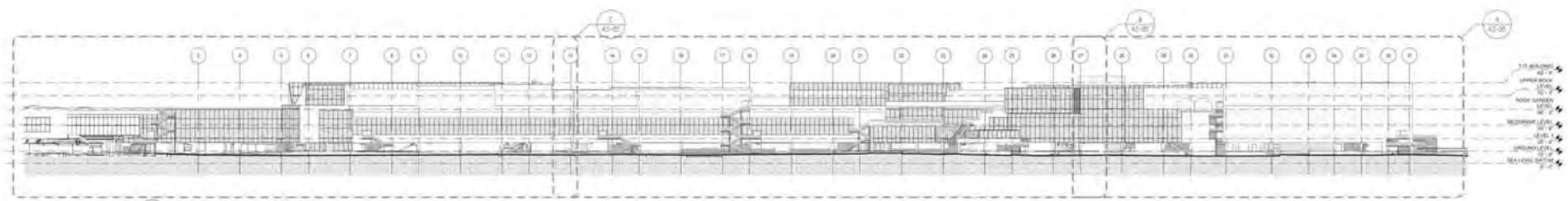
<p><b>GEHRY PARTNERS, LLP</b>          ARCHITECT          12541 BELTWAY STREET          LOS ANGELES, CALIFORNIA 90068          (310) 482-3000</p> <p><b>FACEBOOK</b>          OWNER          1 HAWKES WAY          MENLO PARK, CALIFORNIA 94025</p>	<p>CONULTANTS</p> <p><b>Facebook Campus Expansion</b>          Facebook Building 21, 22 &amp; Hotel Site          300-309 Constitution Drive</p>	<p>TITLE</p> <p><b>BUILDING 21,22 &amp; HOTEL</b>  <b>SITE SECTIONS</b></p>	<p>PROJECT NUMBER          2015-007</p> <p>SCALE          AS NOTED</p> <p>MAY 15, 2017</p> <p>1:117 SHEET NO. N.T.S.          ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p><b>A3-03</b></p> <p>© GEHRY PARTNERS, LLP</p>
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**NOTES:**

1. BUILDING 21 ELEVATIONS ARE PREVIOUSLY APPROVED
2. THESE ELEVATIONS ARE SHOWN FOR CONTEXT ONLY AND ARE NOT PART OF THE PROJECT.

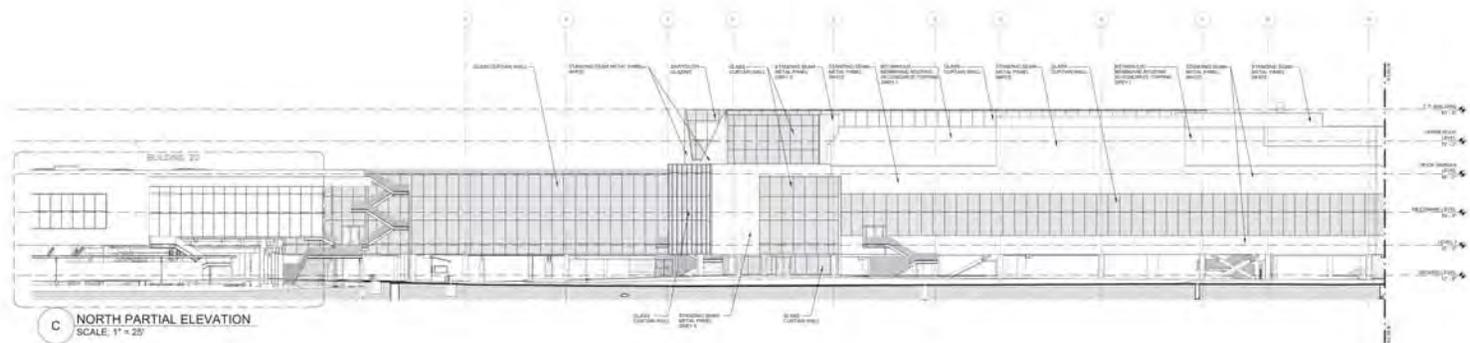


**B SOUTH ELEVATION KEY PLAN**  
SCALE: 1" = 50'

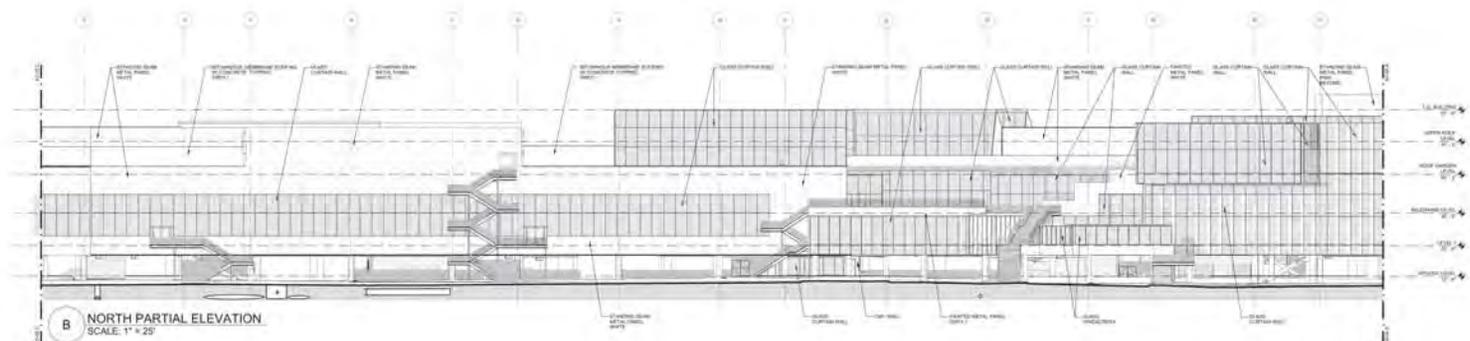


**A NORTH ELEVATION KEY PLAN**  
SCALE: 1" = 50'

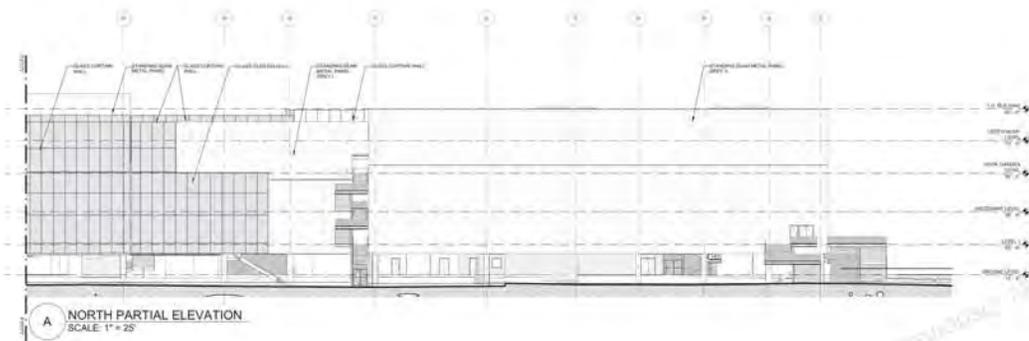
<p><b>GEHRY PARTNERS, LLP</b> ARCHITECT 3251 RIVERFRONT BLVD #100 DUBLIN, CALIFORNIA 94568 (925) 485-2500</p> <p><b>FACEBOOK</b> OWNER 1700 K STREET NW MENLO PARK, CALIFORNIA 94025</p>	<p><b>Facebook Campus Expansion</b> Facebook Building 21, 22 &amp; Hotel Site 300-309 Constitution Drive</p>	<p><b>MPK21</b> NORTH &amp; SOUTH BUILDING ELEVATIONS</p>	<p>PROJECT NUMBER 2015-007</p> <p>DATE 1" = 50'</p> <p>MAY 15, 2017</p> <p>DATE PLOTTED: 05/15/17 PLOTTER: HP DesignJet 2400</p>	<p>SHEET NUMBER <b>A3-04</b></p>
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C NORTH PARTIAL ELEVATION  
SCALE: 1" = 25'



B NORTH PARTIAL ELEVATION  
SCALE: 1" = 25'



A NORTH PARTIAL ELEVATION  
SCALE: 1" = 25'

**NOTES:**  
 1. BUILDING 21 ELEVATIONS ARE PREVIOUSLY APPROVED  
 2. THESE ELEVATIONS ARE SHOWN FOR CONTEXT ONLY AND ARE NOT PART OF THE PRODUCT

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**FACEBOOK**  
 OWNER  
 1 HAWKES WAY  
 MENLO PARK, CALIFORNIA 94025

**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

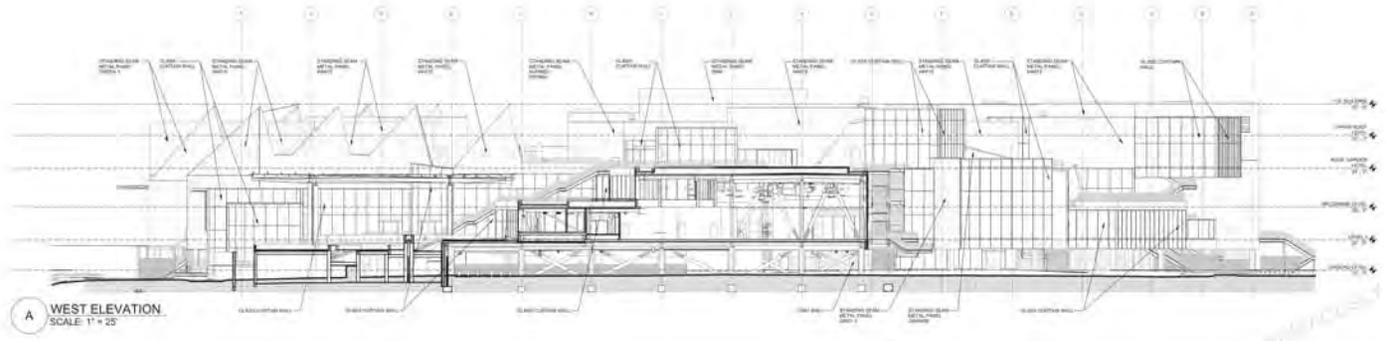
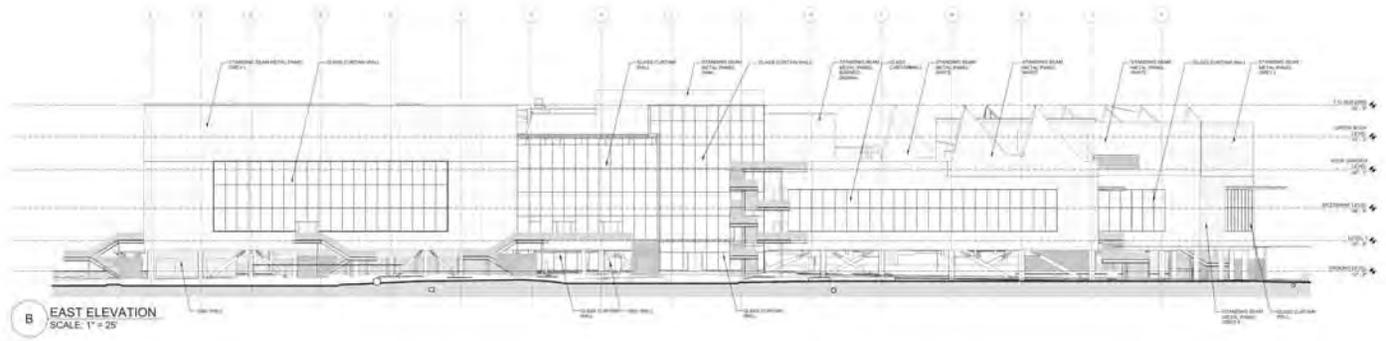
**MPK21**  
 NORTH  
 BUILDING ELEVATIONS

PROJECT NUMBER:  
 2015-007  
 SCALE:  
 1" = 25'  
 DATE:  
 02/20/17

**A3-05**

**NOTES:**

1. SUBMITTAL 21 ELEVATIONS ARE PREVIOUSLY APPROVED
2. THESE ELEVATIONS ARE SHOWN FOR CONTEXT ONLY AND ARE NOT PART OF THE PROJECT



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FACEBOOK  
OWNER  
170015 8th  
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

MPK21  
EAST & WEST  
BUILDING ELEVATIONS

PROJECT NUMBER  
2015-007

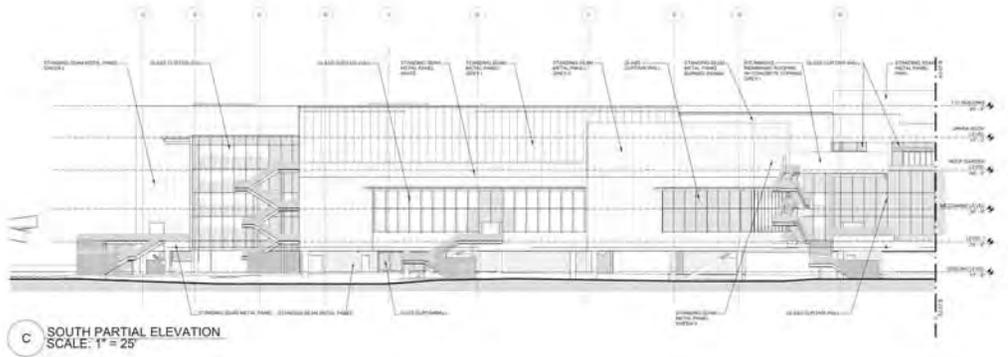
SCALE  
1" = 25'

MAY 15, 2017

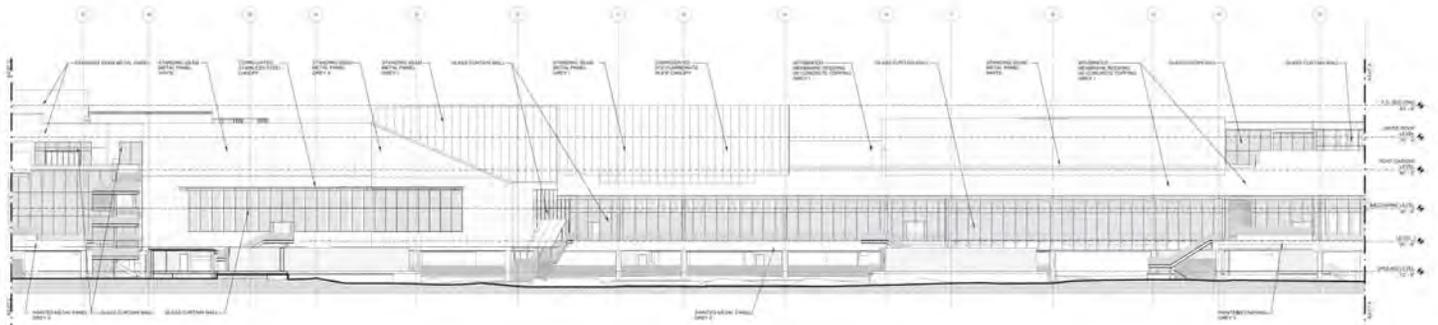
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11130000 SHEET 005 - 013

SHEET NUMBER  
A3-06

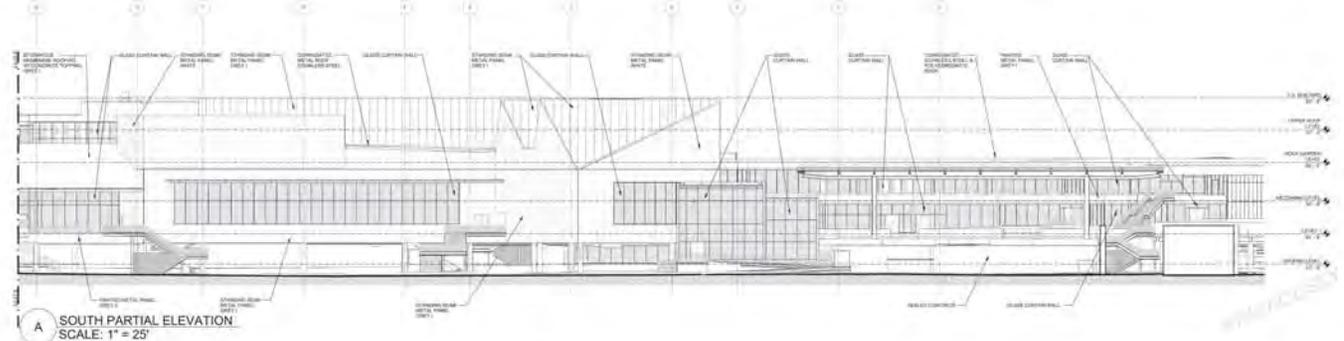
**NOTES:**  
 1. BUILDING 21 ELEVATIONS ARE PREVIOUSLY APPROVED.  
 1. THESE ELEVATIONS ARE SHOWN FOR CONTEXT ONLY AND ARE NOT PART OF THE PROJECT.



**C SOUTH PARTIAL ELEVATION**  
 SCALE: 1" = 25'



**B SOUTH PARTIAL ELEVATION**  
 SCALE: 1" = 25'



**A SOUTH PARTIAL ELEVATION**  
 SCALE: 1" = 25'

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 ARCHITECT  
 1234 BAYVIEW STREET  
 OAKLAND, CALIFORNIA 94612  
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**FACEBOOK**  
 OWNER  
 1700 K STREET, NW  
 WASHINGTON, DC 20005

**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

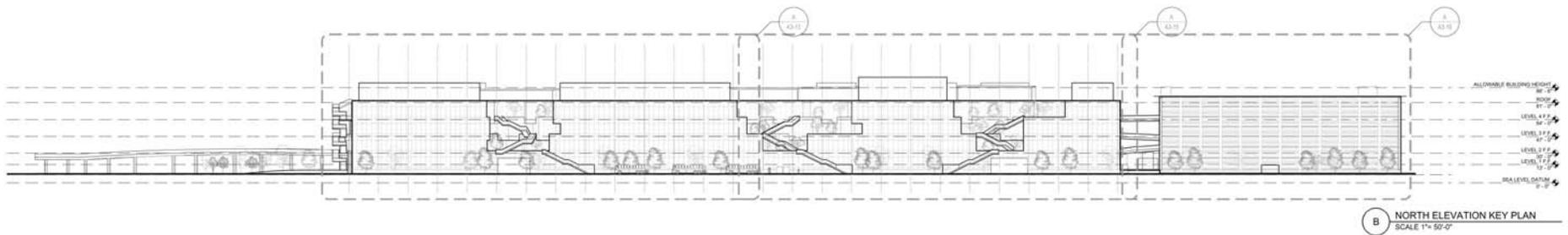
**MPK21**  
 SOUTH  
 BUILDING ELEVATIONS

PROJECT NUMBER  
 2015-007

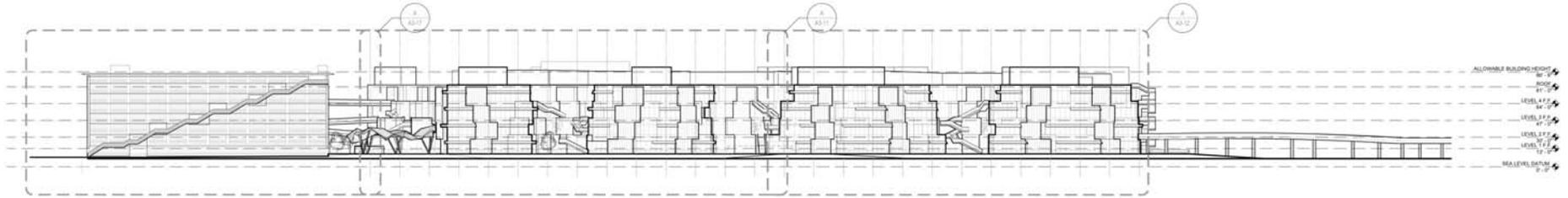
SCALE  
 1" = 25'

DATE  
 07/14/15

HEET NUMBER  
**A3-07**

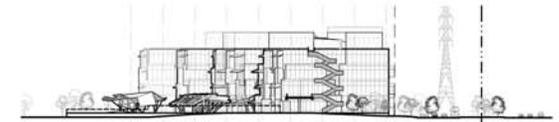


**B** NORTH ELEVATION KEY PLAN  
SCALE 1"=50'-0"

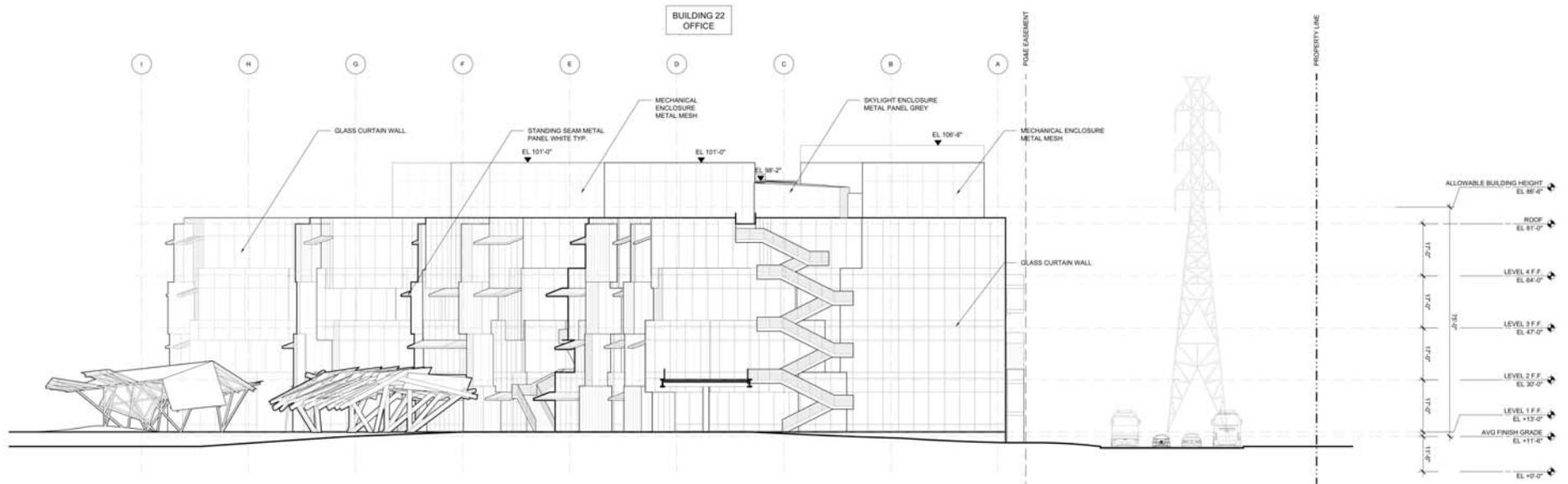


**A** SOUTH ELEVATION KEY PLAN  
SCALE 1"=50'-0"

<p><b>GEHRY PARTNERS, LLP</b> ARCHITECT 12511 BELTWAY STREET LOS ANGELES, CALIFORNIA 90068 (310) 482-3000</p> <p><b>FACEBOOK</b> OWNER 17000 WILLOW DRIVE MENLO PARK, CALIFORNIA 94025</p>	<p>CONSULTANTS</p> <p><b>Facebook Campus Expansion</b> Facebook Building 21, 22 &amp; Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p><b>MPK22</b> NORTH AND SOUTH BUILDING ELEVATIONS</p>	<p>PROJECT NUMBER</p> <p>2015-007</p> <p>SCALE</p> <p>1"=50'</p> <p>MAY 15, 2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p><b>A3-08</b></p> <p>© GEHRY PARTNERS, LLP</p>
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**B EAST ELEVATION WITH LANDSCAPE**  
1/64" = 1'-0"



**A EAST ELEVATION**  
1" = 16'-0"

**NOTES:**

1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK.

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LOS ANGELES, CALIFORNIA 90068  
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CONSULTANTS

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1 HAWKES WAY  
MENLO PARK, CALIFORNIA 94025

**Facebook Campus Expansion**  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

TITLE  
**MPK22**  
**EAST BUILDING ELEVATION**

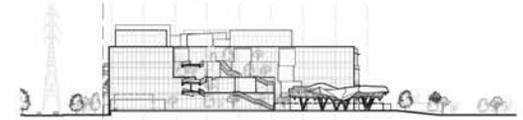
PROJECT NUMBER  
2015-007  
SCALE  
1/16" = 1'-0"

SHEET NUMBER

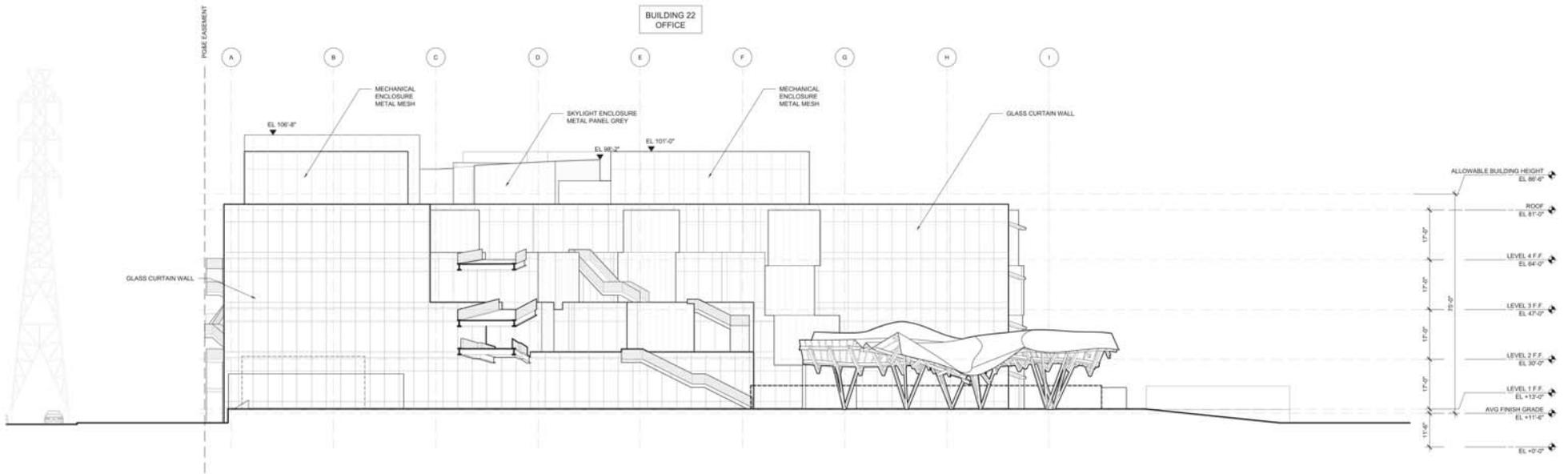
**A3-09**

MAY 15, 2017  
1/4" = 1'-0" SHEET SIZE: N.T.S.  
ORIGINAL SHEET SIZE: 24" x 36"

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**B** WEST ELEVATION WITH LANDSCAPE  
1/64" = 1'-0"



**A** WEST ELEVATION  
1/16" = 1'-0"

1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK.

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**FACEBOOK**  
OWNER  
1 HACKER WAY  
MENLO PARK, CALIFORNIA 94025

CONSULTANTS

**Facebook Campus Expansion**  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

TITLE  
**MPK22**  
WEST BUILDING ELEVATION

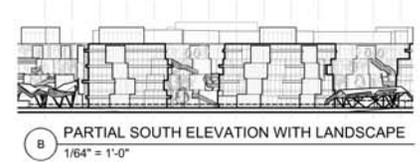
PROJECT NUMBER  
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SCALE  
1/16" = 1'-0"

SHEET NUMBER

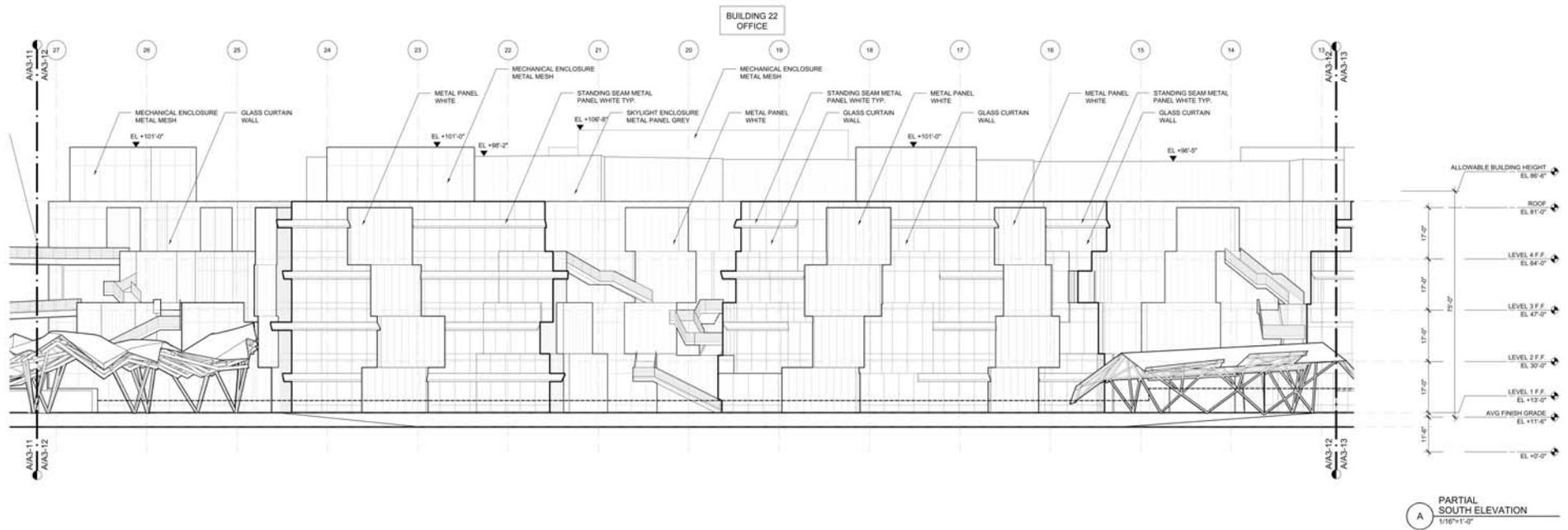
**A3-10**

MAY 15, 2017  
1/4" = 1'-0" SHEET SIZE: N.T.S.  
ORIGINAL SHEET SIZE: 24" x 36"

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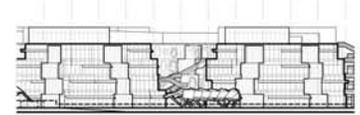
**B** PARTIAL SOUTH ELEVATION WITH LANDSCAPE  
1/64" = 1'-0"



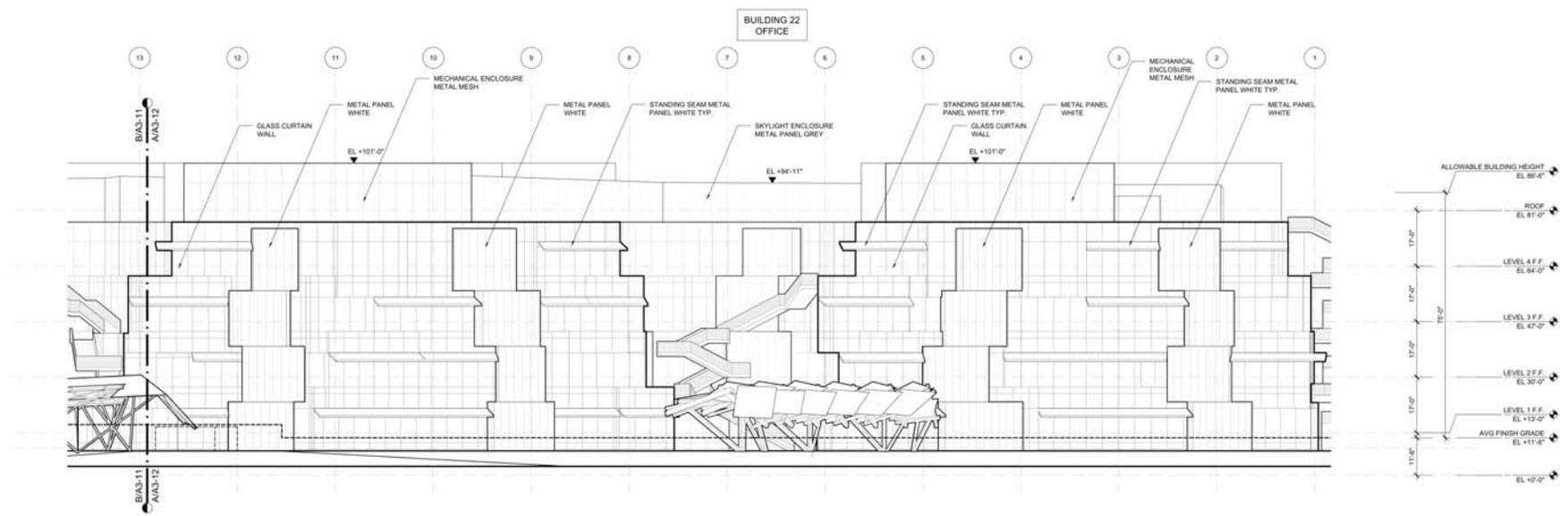
**A** PARTIAL SOUTH ELEVATION  
1/16" = 1'-0"

1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK.

<p><b>GEHRY PARTNERS, LLP</b> ARCHITECT 12541 BELTLINE STREET LOS ANGELES, CALIFORNIA 90068 (310) 485-2000</p> <p><b>FACEBOOK</b> OWNER 17000 NE 28TH AVE MENLO PARK, CALIFORNIA 94025</p>	<p>CONSULTANTS</p> <p><b>Facebook Campus Expansion</b> Facebook Building 21, 22 &amp; Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p><b>MPK22</b> <b>SOUTH</b> <b>BUILDING ELEVATIONS</b></p>	<p>PROJECT NUMBER 2015-007</p> <p>SCALE 1/16" = 1'-0"</p> <p>MAY 15, 2017</p> <p>PLAT SHEET SIZE: N.T.S. DRAWING SHEET SIZE: 34" x 36"</p>	<p>SHEET NUMBER</p> <p><b>A3-11</b></p> <p>© GEHRY PARTNERS, LLP</p>
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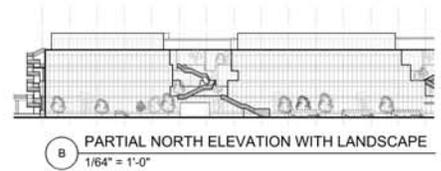
**B** PARTIAL SOUTH ELEVATION WITH LANDSCAPE  
1/64" = 1'-0"



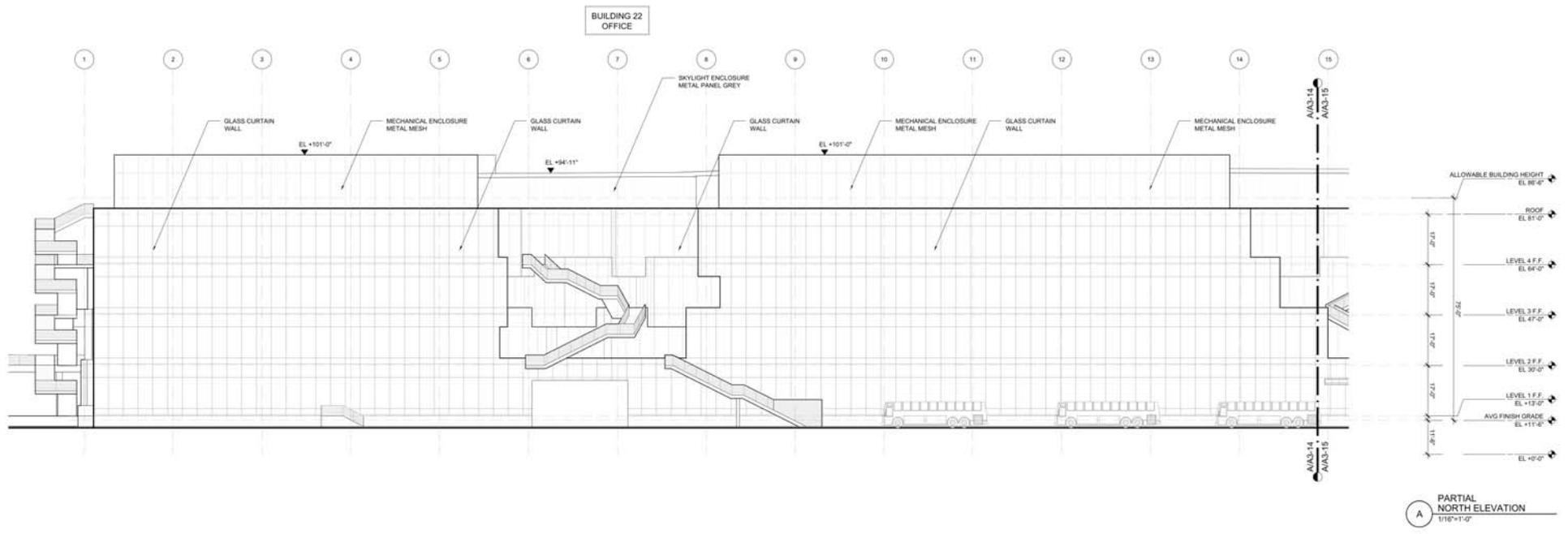
**A** PARTIAL SOUTH ELEVATION  
1/16"=1'-0"

1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT  
2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK.

<p><b>GEHRY PARTNERS, LLP</b> ARCHITECT 12541 BELTWOOD STREET LOS ANGELES, CALIFORNIA 90068 (310) 482-2000</p> <p><b>FACEBOOK</b> OWNER 17000 WOODLAND AVENUE MENLO PARK, CALIFORNIA 94025</p>	<p>CONSULTANTS</p> <p><b>Facebook Campus Expansion</b> Facebook Building 21, 22 &amp; Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p><b>MPK22</b> <b>SOUTH</b> <b>BUILDING ELEVATIONS</b></p>	<p>PROJECT NUMBER</p> <p>2015-007</p> <p>SCALE</p> <p>1/16" = 1'-0"</p> <p>MAY 15, 2017</p> <p>PLAT SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 34" x 36"</p>	<p>SHEET NUMBER</p> <p><b>A3-12</b></p> <p>© GEHRY PARTNERS, LLP</p>
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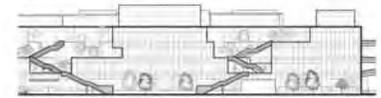
**B** PARTIAL NORTH ELEVATION WITH LANDSCAPE  
1/64" = 1'-0"



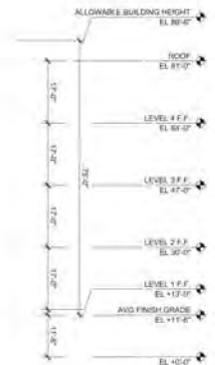
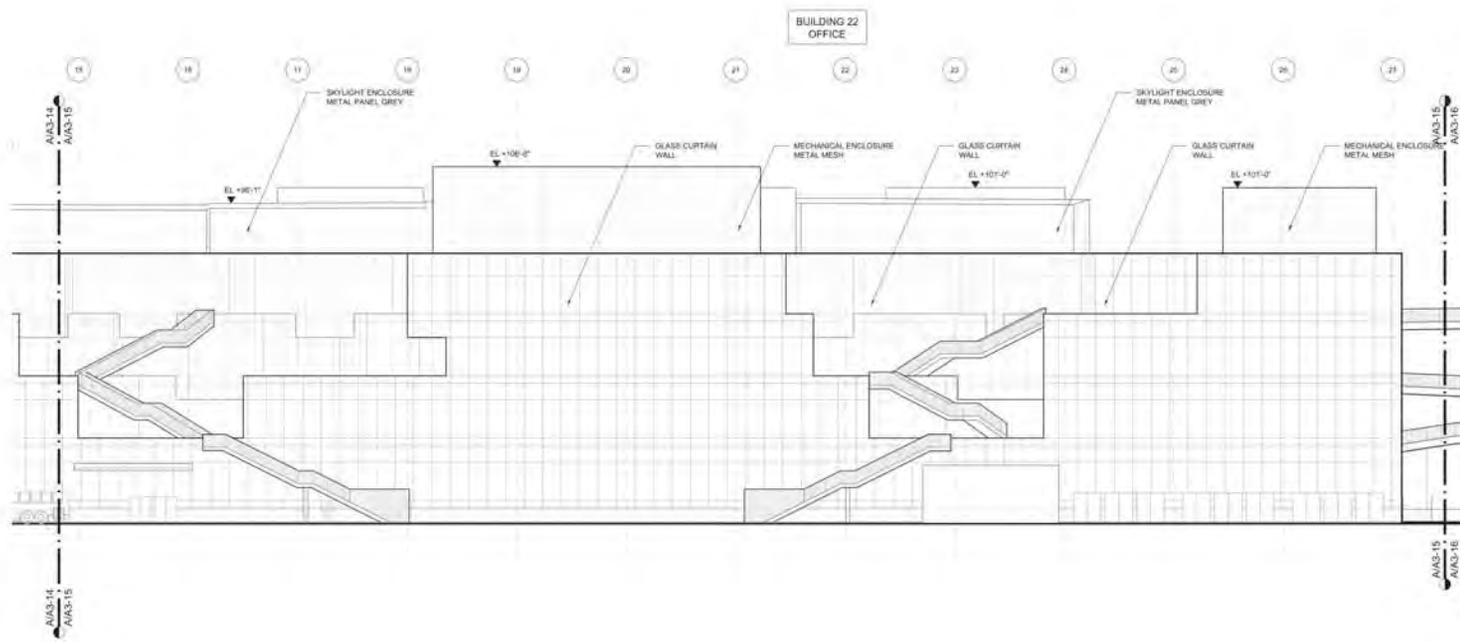
**A** PARTIAL NORTH ELEVATION  
1/16" = 1'-0"

1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK.

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B PARTIAL NORTH ELEVATION WITH LANDSCAPE  
1/64" = 1'-0"



A PARTIAL NORTH ELEVATION  
1/16" = 1'-0"

1 NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT  
2 LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF MENLO PARK

**GEHRY PARTNERS, LLP**  
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**FACEBOOK**  
OWNER  
1300 CALIFORNIA STREET  
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

MPK22  
NORTH  
BUILDING ELEVATIONS

PROJECT NUMBER  
2015-057  
SCALE  
1/16" = 1'-0"  
DATE  
MAY 15, 2017  
DRAWN BY  
T.M. / J.M. / J.P.  
CHECKED BY  
J.M. / J.P. / J.M.

PROJECT NUMBER  
A3-14



NORTH WEST VIEW INTERIM PHASE



NORTH WEST VIEW FINAL PHASE



SOUTH WEST VIEW INTERIM PHASE



SOUTH WEST VIEW FINAL PHASE



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SAN JOSE, CA 95110  
(408) 501-7000

FACEBOOK  
OWNER  
1 FALCON WAY  
MENLO PARK, CALIFORNIA 94025

CONTRACT NO.

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

TITLE  
PARKING STRUCTURE  
EAST  
BUILDING ELEVATION  
(YEAR 3)

PROJECT NUMBER  
2015-007  
SCALE  
1/16" = 1'-0"  
MAY 15, 2017  
THIS SHEET SIZE: 8 1/2" x 11"  
ORIGINAL SHEET SIZE: 34" x 48"

SHEET NUMBER  
A3-15  
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OWNER  
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MENLO PARK, CALIFORNIA 94025

CONSULTANT

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

PARKING STRUCTURE  
WEST  
BUILDING ELEVATION  
(YEAR 3)

PROJECT NUMBER  
2015-007

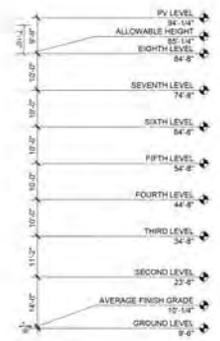
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MAY 15, 2017

1/17 SHEET SIZE: 8.5" x 11"  
ORIGINAL SHEET SIZE: 34" x 36"

SHEET NUMBER  
A3-16

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**A** PARTIAL SOUTH ELEVATION  
1/16"=1'-0"

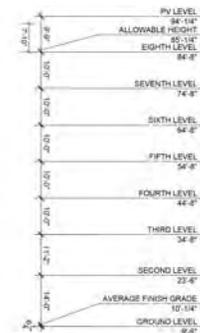
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ARCHITECT  
208 GATEWAY PL #300  
SAN JOSE, CA 95110  
FACEBOOK  
OWNER  
7 FOLKLORE BLVD  
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

MPK22  
SOUTH GARAGE ELEVATIONS  
(YEAR 3)

PROJECT NUMBER  
2015-007  
SCALE  
1/16" = 1'-0"  
MAY 15, 2017

SHEET NUMBER  
A3-17



A PARTIAL NORTH ELEVATION  
1/16"=1'-0"

WATRY DESIGN, INC.  
ARCHITECT  
200 SATELVILLE PL, #100  
SAN JOSE, CA 95110  
(408) 550-7000

FACEBOOK  
OWNER  
1 HAYDEN WAY  
MENLO PARK, CALIFORNIA 94025

DATE/REVISIONS

Facebook Campus Expansion  
Facebook Building 21, 22 & Hotel Site  
300-309 Constitution Drive

MPK22  
NORTH  
BUILDING ELEVATIONS  
(YEAR 3)

PROJECT NUMBER  
2015-007

SCALE  
1/16" = 1'-0"

MAY 15, 2017

TITLE SHEET NO. 3.13  
DRAWING SHEET NO. 34" x 36"

SHEET NUMBER

A3-18

© 2017 PARSONS LLP



KEY

	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 23	57
3 TOTAL	1,552



**GEHRY PARTNERS, LLP**  
 ARCHITECT  
 1201 BAYVIEW BLVD  
 SUITE 1000 | CLEVELAND, OH 44115  
 (216) 462-1000

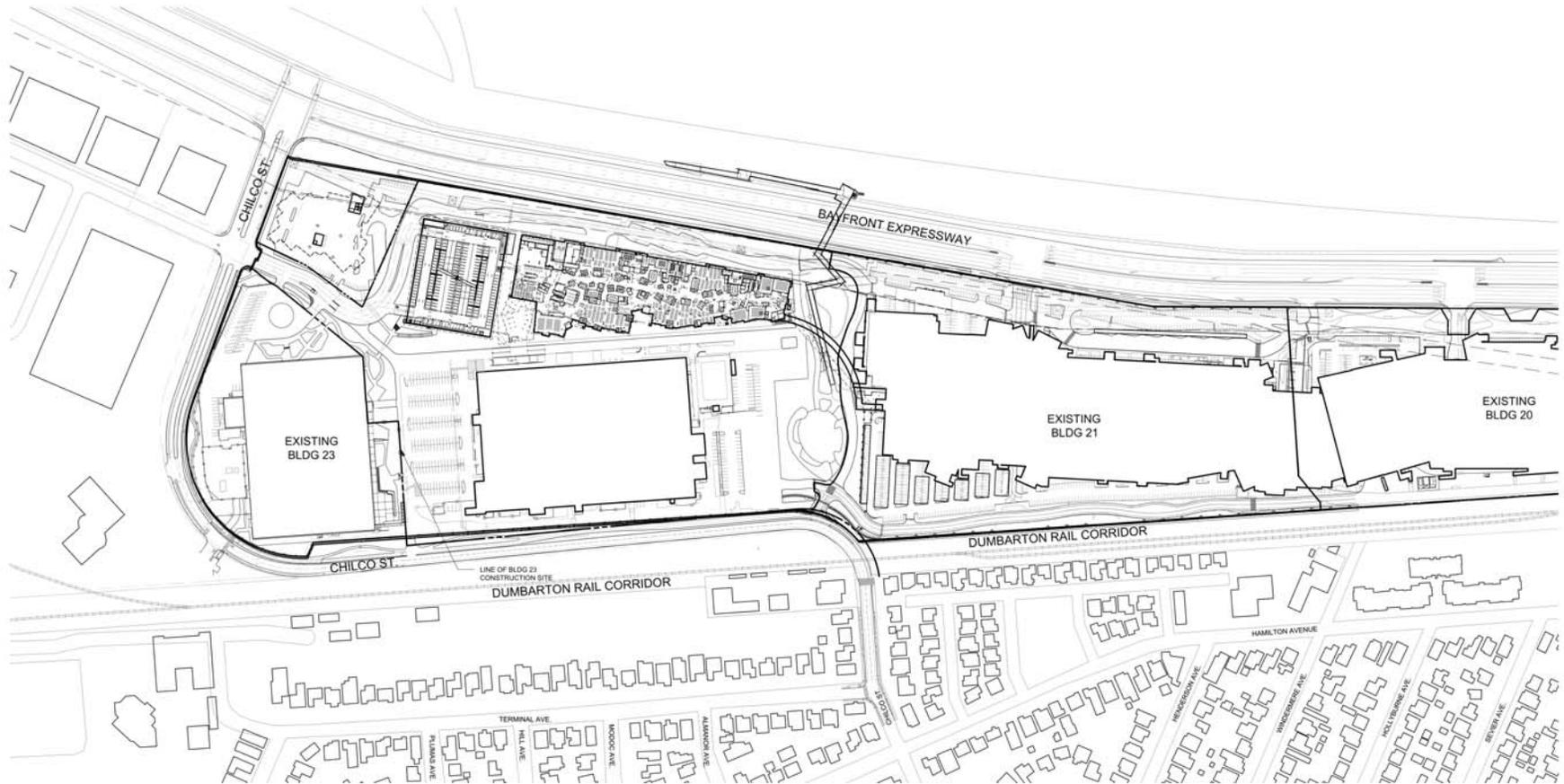
**FACEBOOK**  
 OWNER  
 1700 BAYVIEW BLVD  
 CLEVELAND, OH 44115

**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

**PARKING PHASING 1**  
 MPK21 BUILT  
 MPK22 UNDER CONSTRUCTION

PROJECT NUMBER  
 2015-007  
 SCALE  
 1" = 150'  
 DATE  
 MAY 15, 2017  
 11.11.17 08:11 AM '17  
 (SHEET 3427 OF 347)

SHEET NUMBER  
**A4-01**



KEY

	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 22	0
3 BUILDING 23	57
4 PARKING GARAGE	1,736
TOTAL	3,288



GEHRY PARTNERS, LLP  
 ARCHITECT  
 12511 BELLAIR STREET  
 LOS ANGELES, CALIFORNIA 90038  
 (310) 482-3000

CONSULTANTS

FACEBOOK  
 OWNER  
 17001 WILSON BLVD  
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

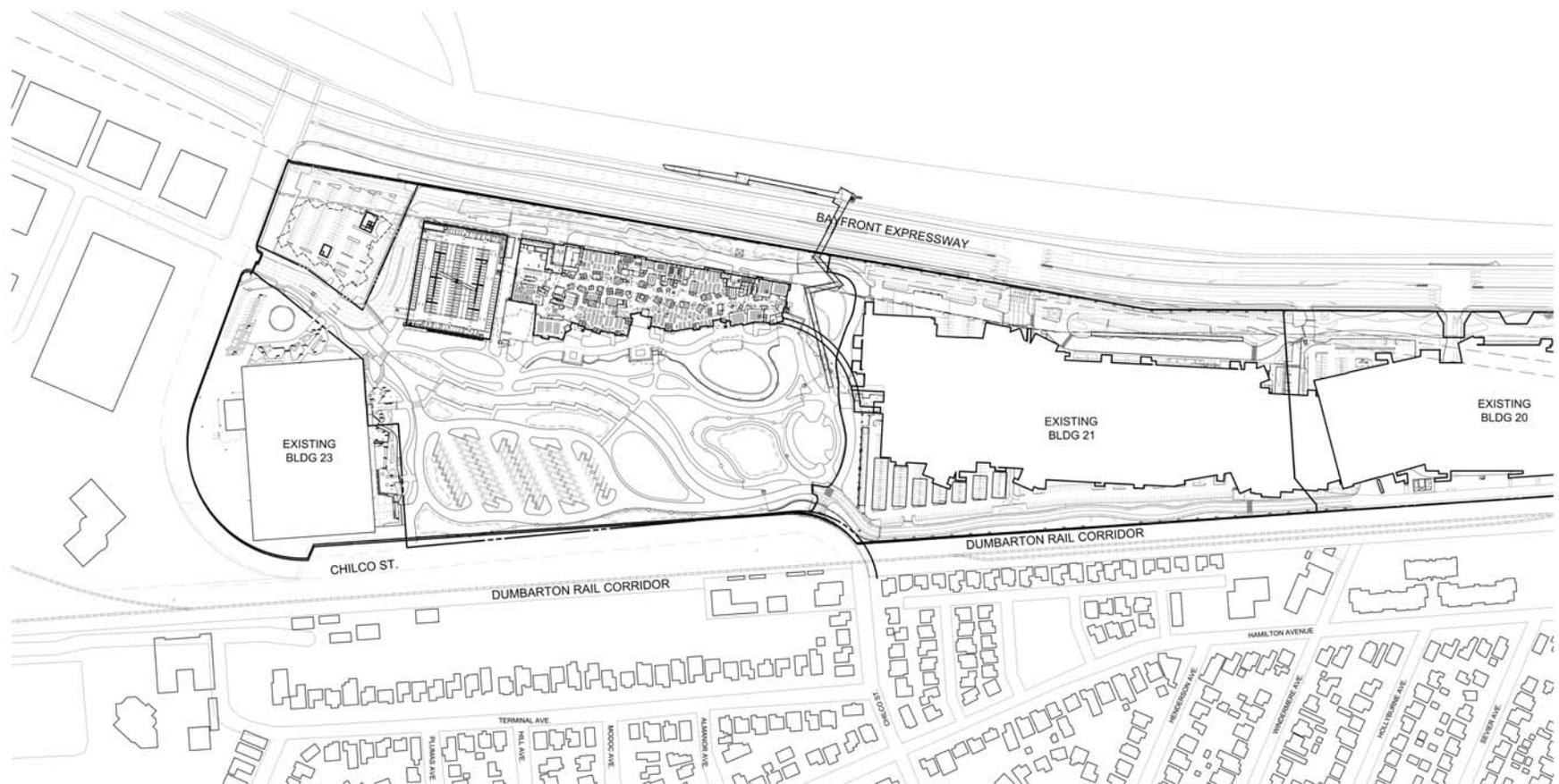
PARKING PHASING 2  
 INTERIM PHASE  
 HOTEL UNDER CONSTRUCTION

PROJECT NUMBER  
 2015-007  
 SCALE  
 1" = 150'  
 MAY 15, 2017  
 THIS SHEET IS IN N.T.S.  
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

A4-02

© GEHRY PARTNERS, LLP



KEY

	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 22	0
3 BUILDING 23	57
4 PARKING GARAGE	1,736
5 HOTEL	245
TOTAL	3,533



**GEHRY PARTNERS, LLP**  
 ARCHITECT  
 12511 BELLAIR STREET  
 LOS ANGELES, CALIFORNIA 90038  
 (310) 482-3000  
  
**FACEBOOK**  
 OWNER  
 1700 LEXINGTON AVENUE  
 NEW YORK, CALIFORNIA 94025

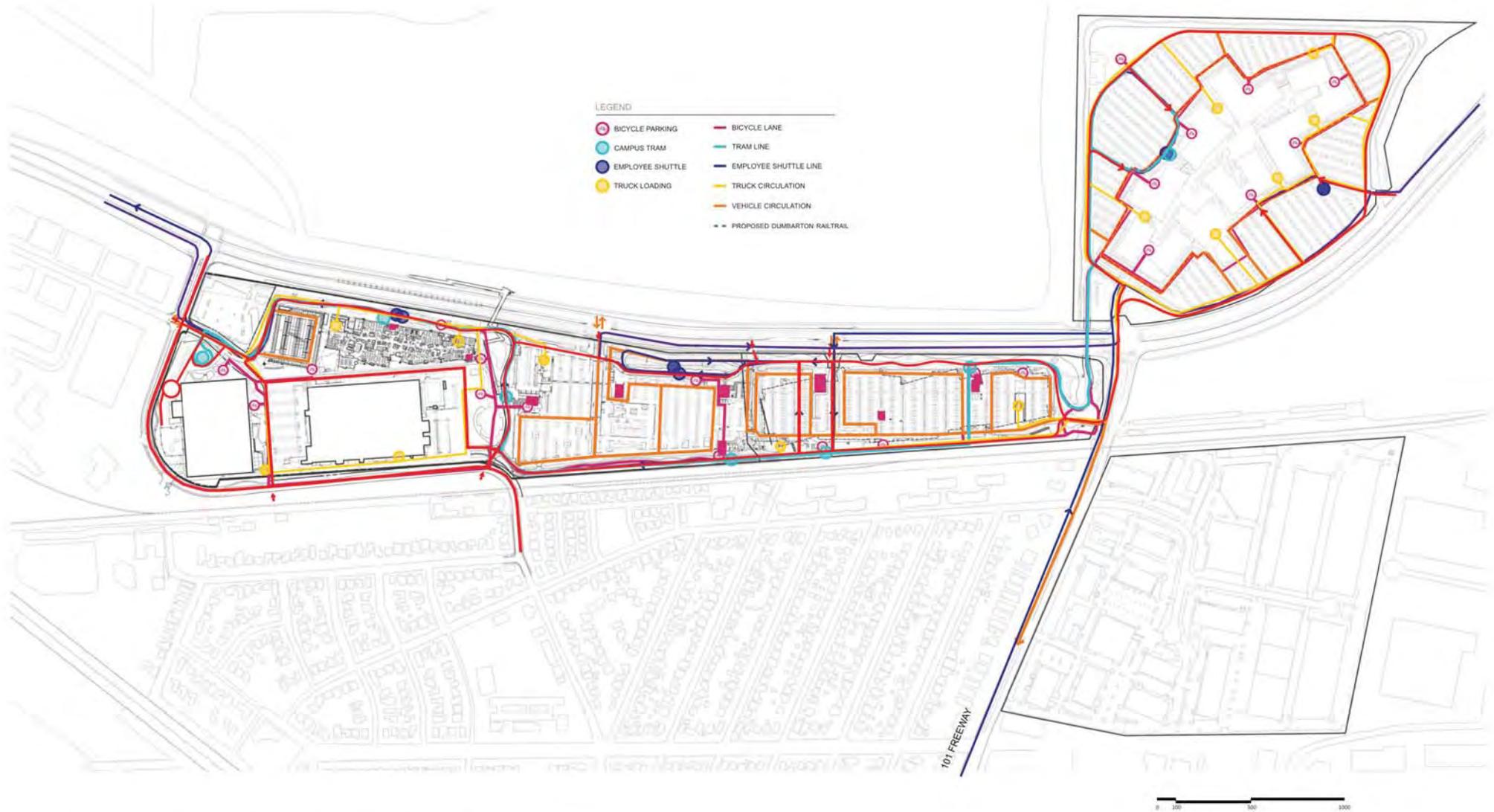
CONSULTANTS

**Facebook Campus Expansion**  
 Facebook Building 21, 22 & Hotel Site  
 300-309 Constitution Drive

TITLE  
**PARKING PHASING 3**  
**FINAL PHASE**

PROJECT NUMBER  
 2015-007  
 SCALE  
 1" = 150'  
  
 MAY 15, 2017  
THIS SHEET IS A PART OF THE ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER  
**A4-03**  
© GEHRY PARTNERS, LLP



## Facebook Campus Expansion

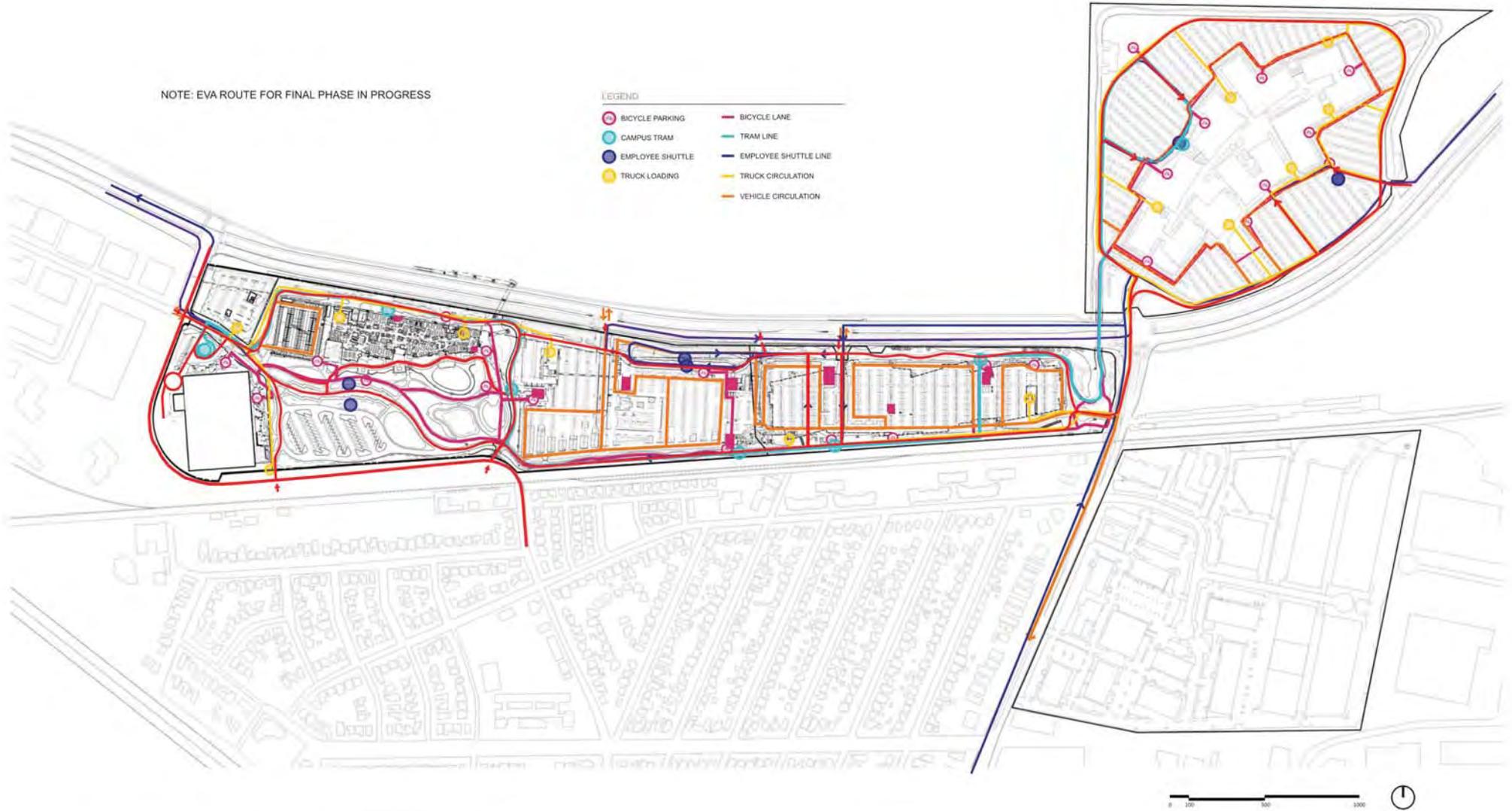
Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

T1-01 TRAFFIC DIAGRAMS - INTERIM OVERLAY

MAY 15, 2017

NOTE: EVA ROUTE FOR FINAL PHASE IN PROGRESS

- LEGEND
- BICYCLE PARKING
  - CAMPUS TRAM
  - EMPLOYEE SHUTTLE
  - TRUCK LOADING
  - BICYCLE LANE
  - TRAM LINE
  - EMPLOYEE SHUTTLE LINE
  - TRUCK CIRCULATION
  - VEHICLE CIRCULATION

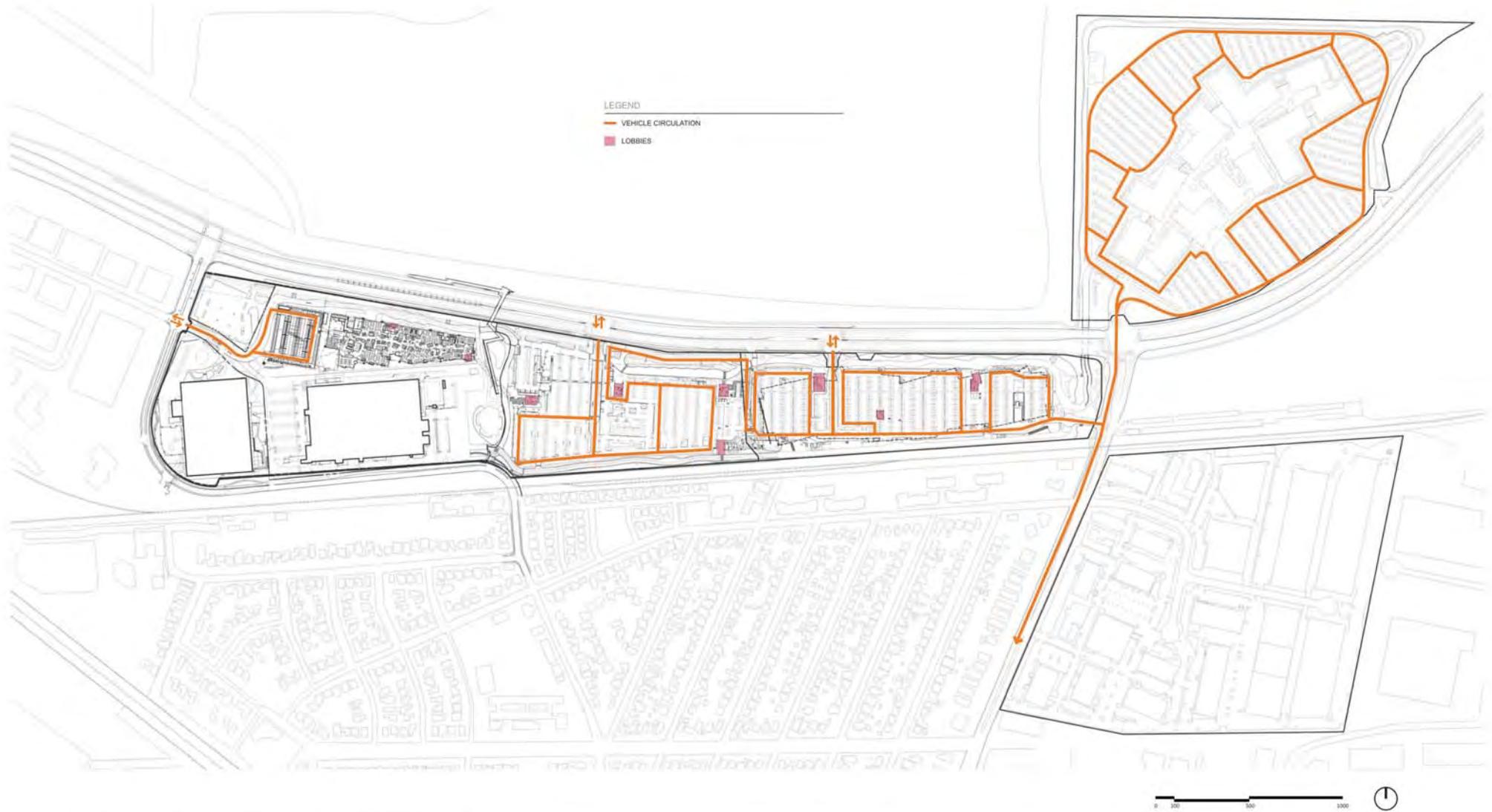


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
301-309 Constitution Drive, Menlo Park, California  
Gehry Partners, LLP

T1-02 TRAFFIC DIAGRAMS - FINAL OVERLAY

MAY 15, 2017

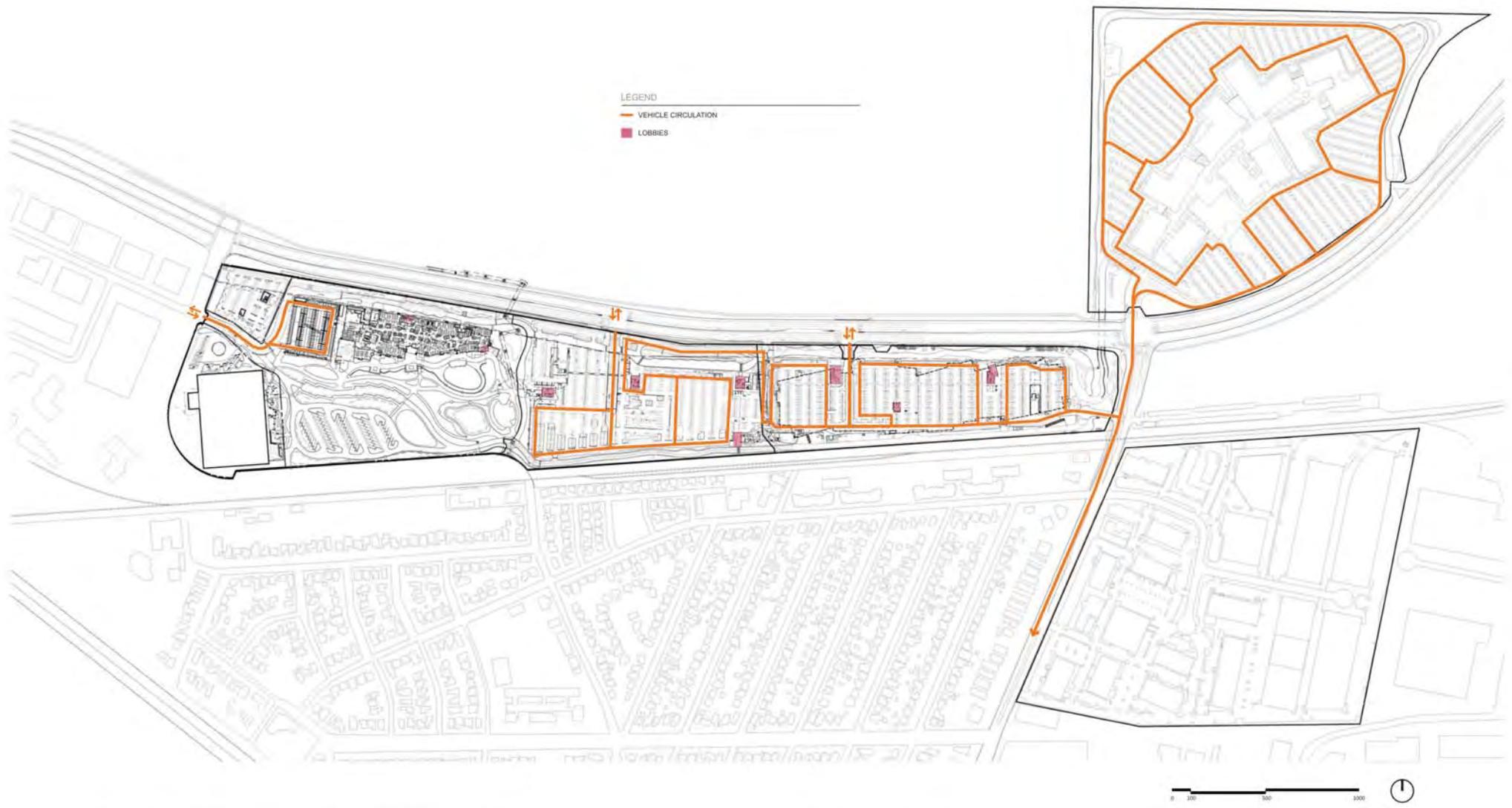


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-03 TRAFFIC DIAGRAMS - INTERIM INTERNAL VEHICLE ACCESS

MAY 15, 2017

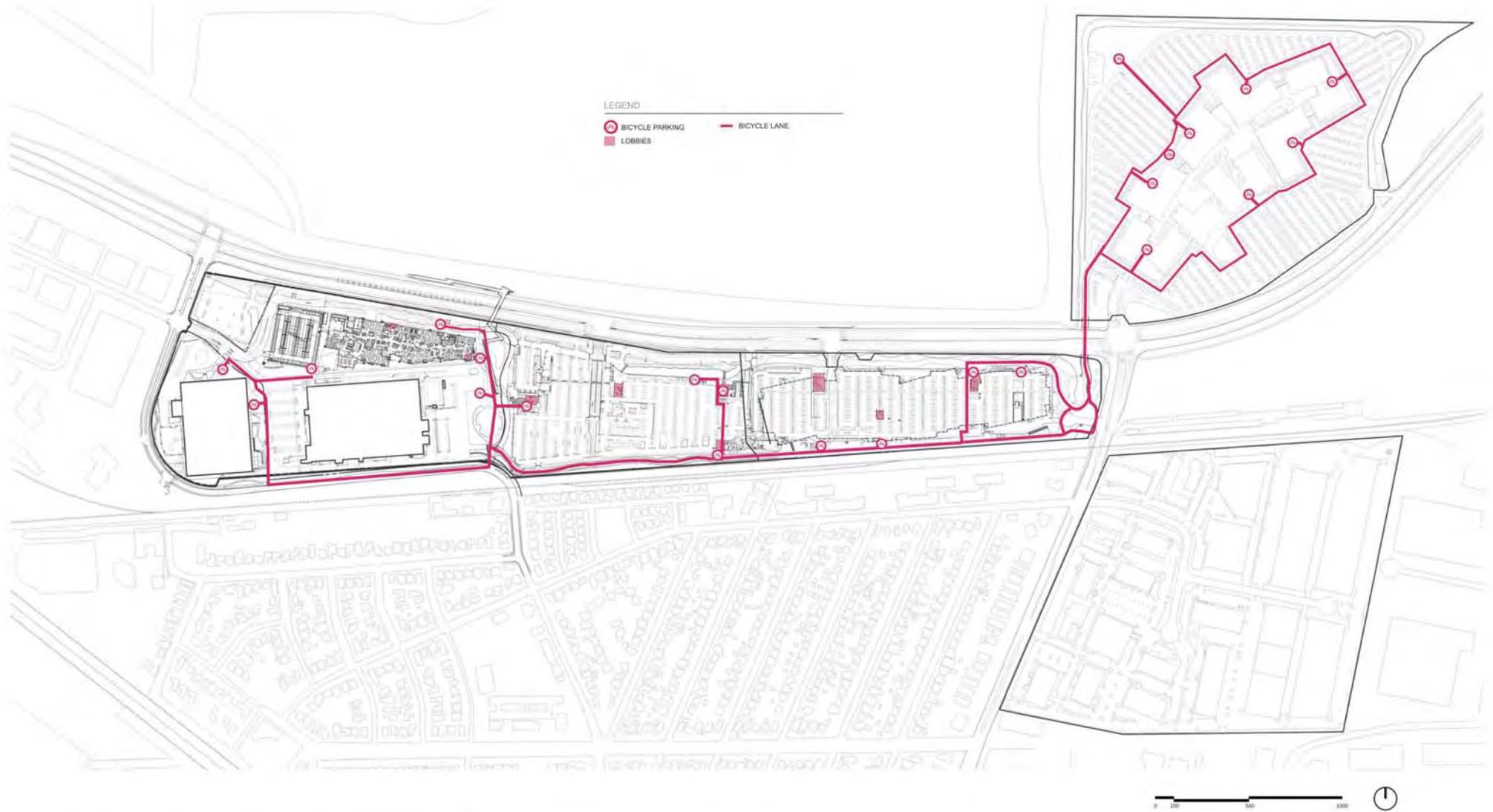


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-04 TRAFFIC DIAGRAMS - FINAL INTERNAL VEHICLE ACCESS

MAY 15, 2017

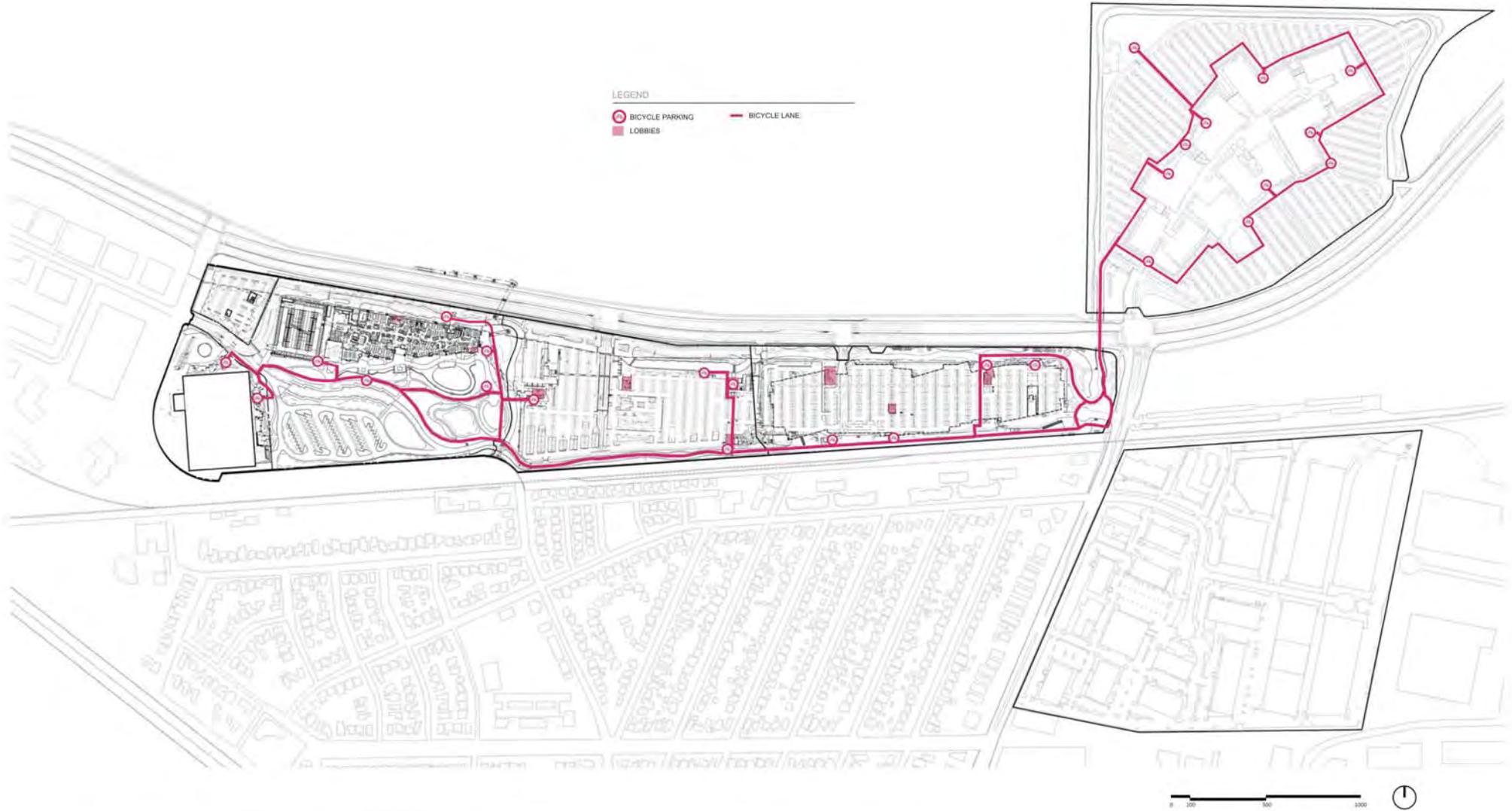


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-05 TRAFFIC DIAGRAMS - INTERIM INTERNAL BICYCLE CIRCULATION

MAY 15, 2017

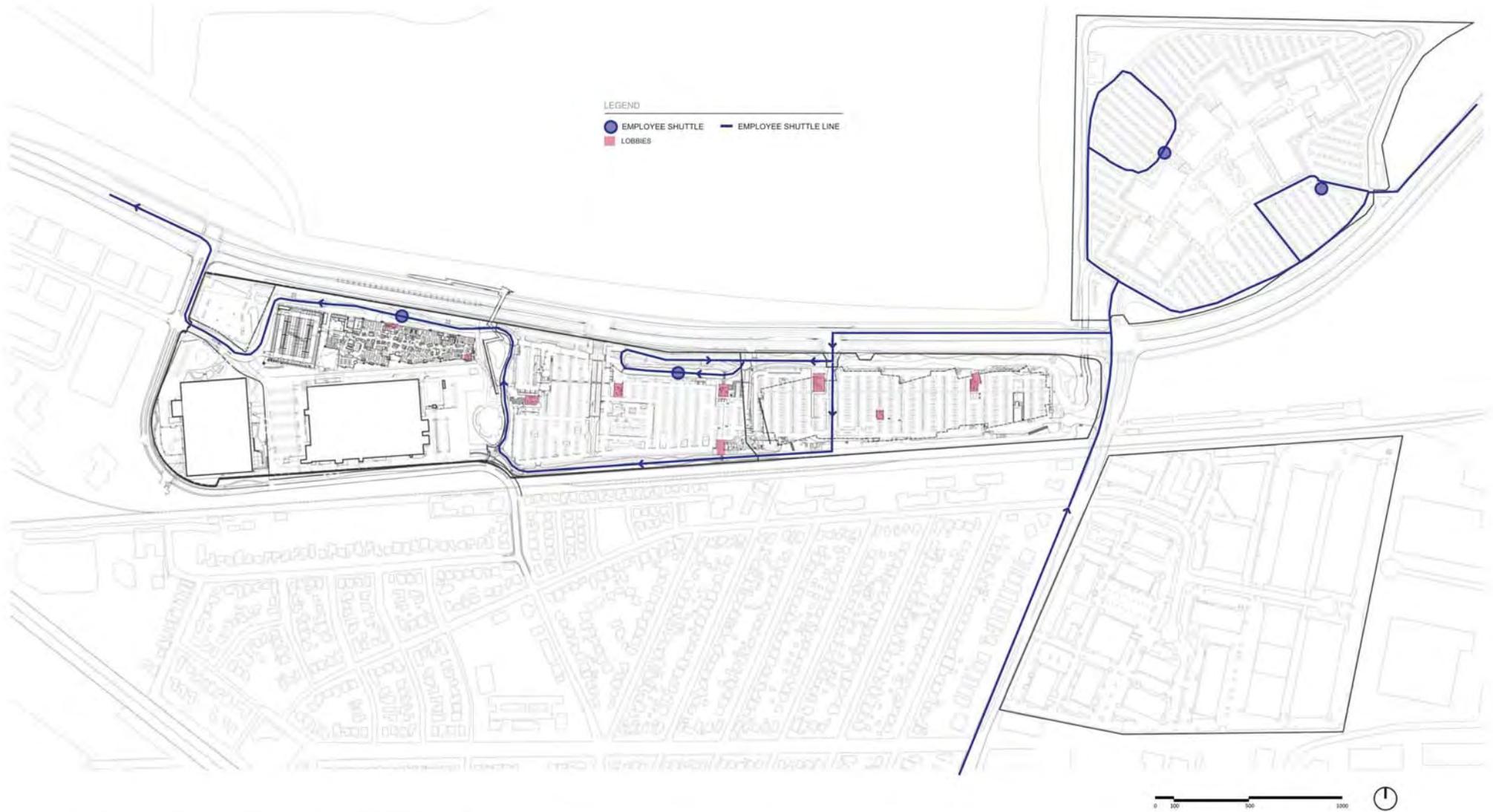


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-06 TRAFFIC DIAGRAMS - FINAL INTERNAL BICYCLE CIRCULATION

MAY 15, 2017

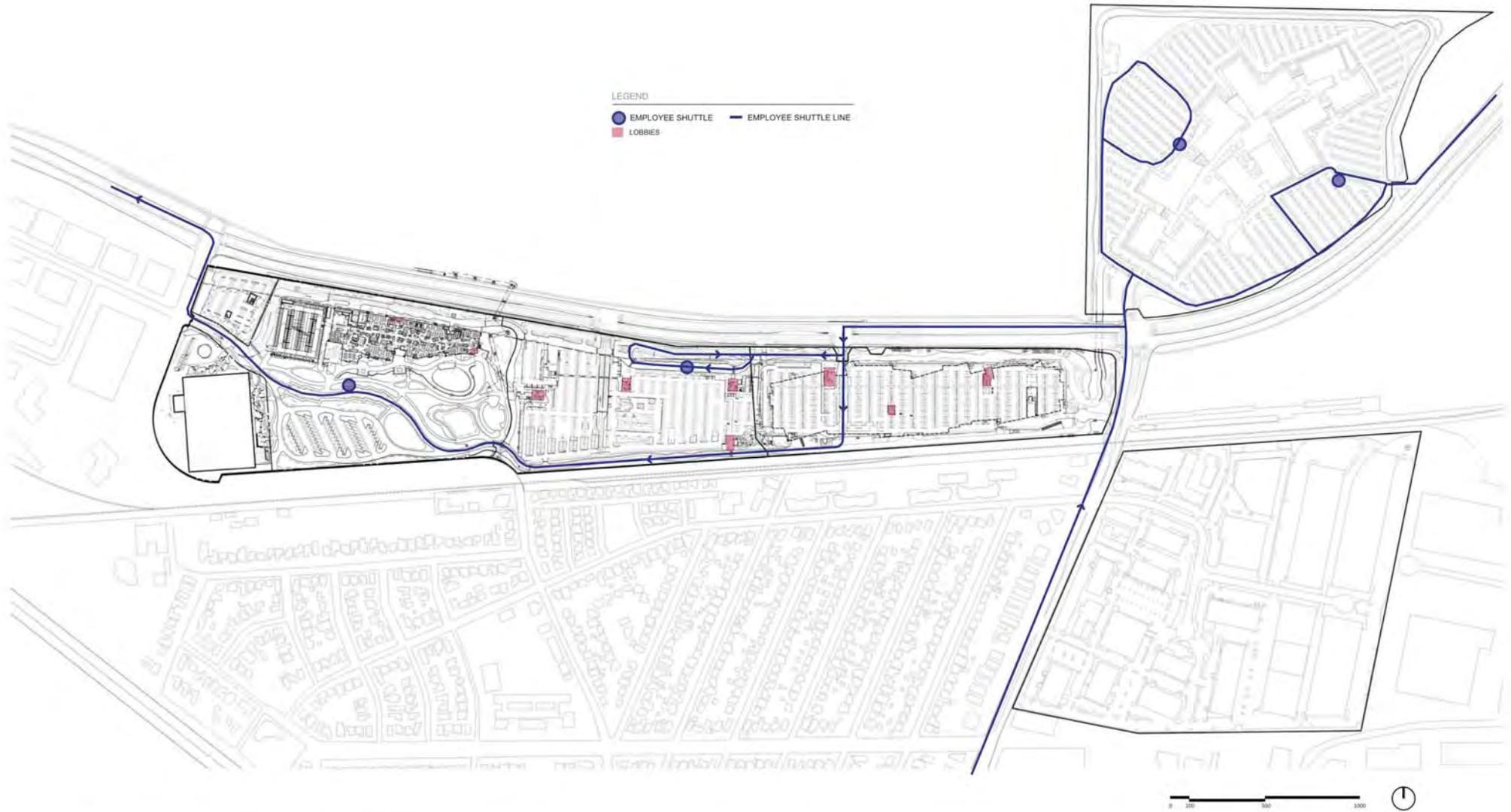


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-07 TRAFFIC DIAGRAMS - INTERIM EMPLOYEE SHUTTLE BUS NORTHBOUND

MAY 15, 2017

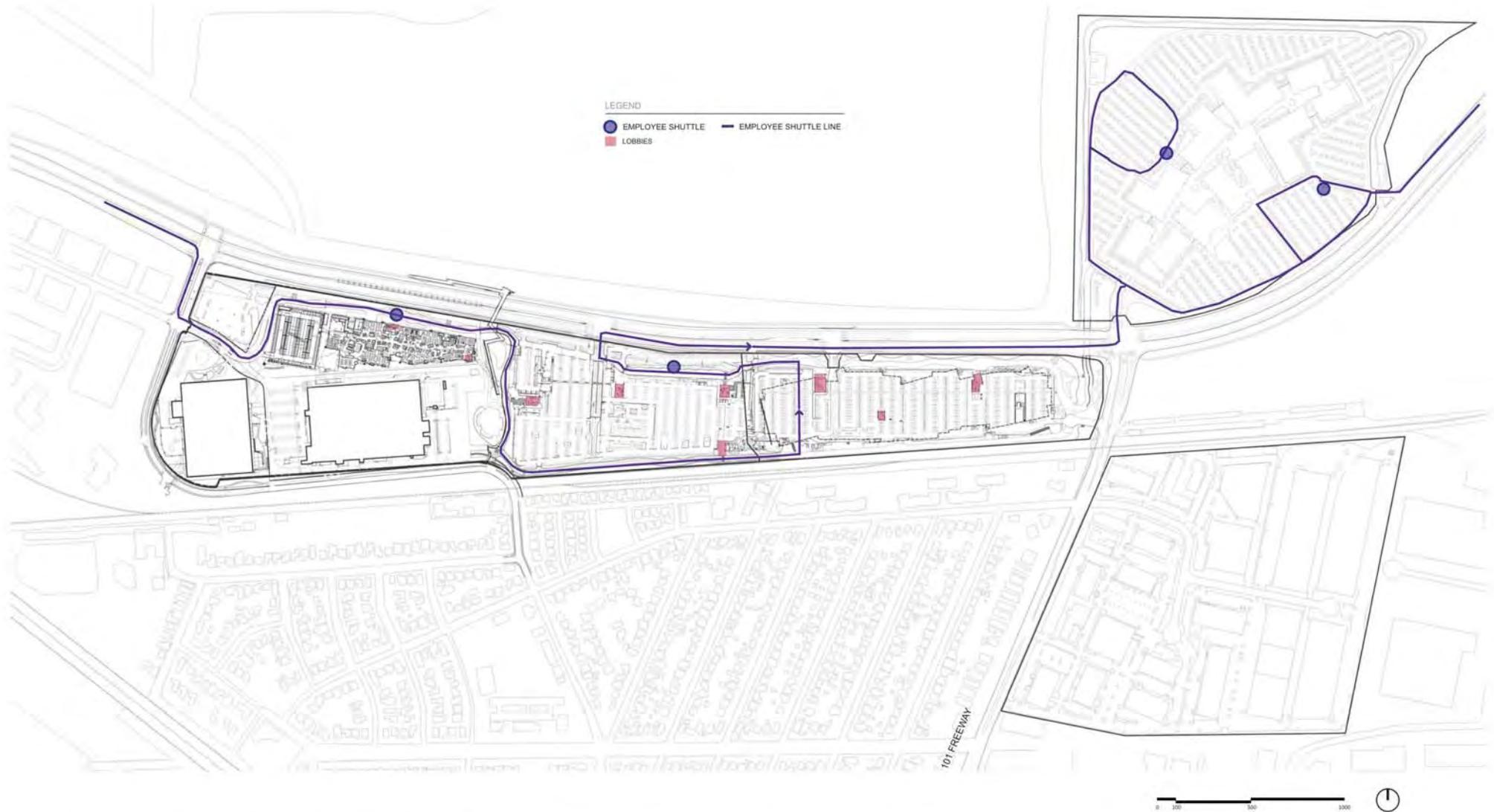


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-08 TRAFFIC DIAGRAMS - FINAL EMPLOYEE SHUTTLE BUS NORTHBOUND

MAY 15, 2017

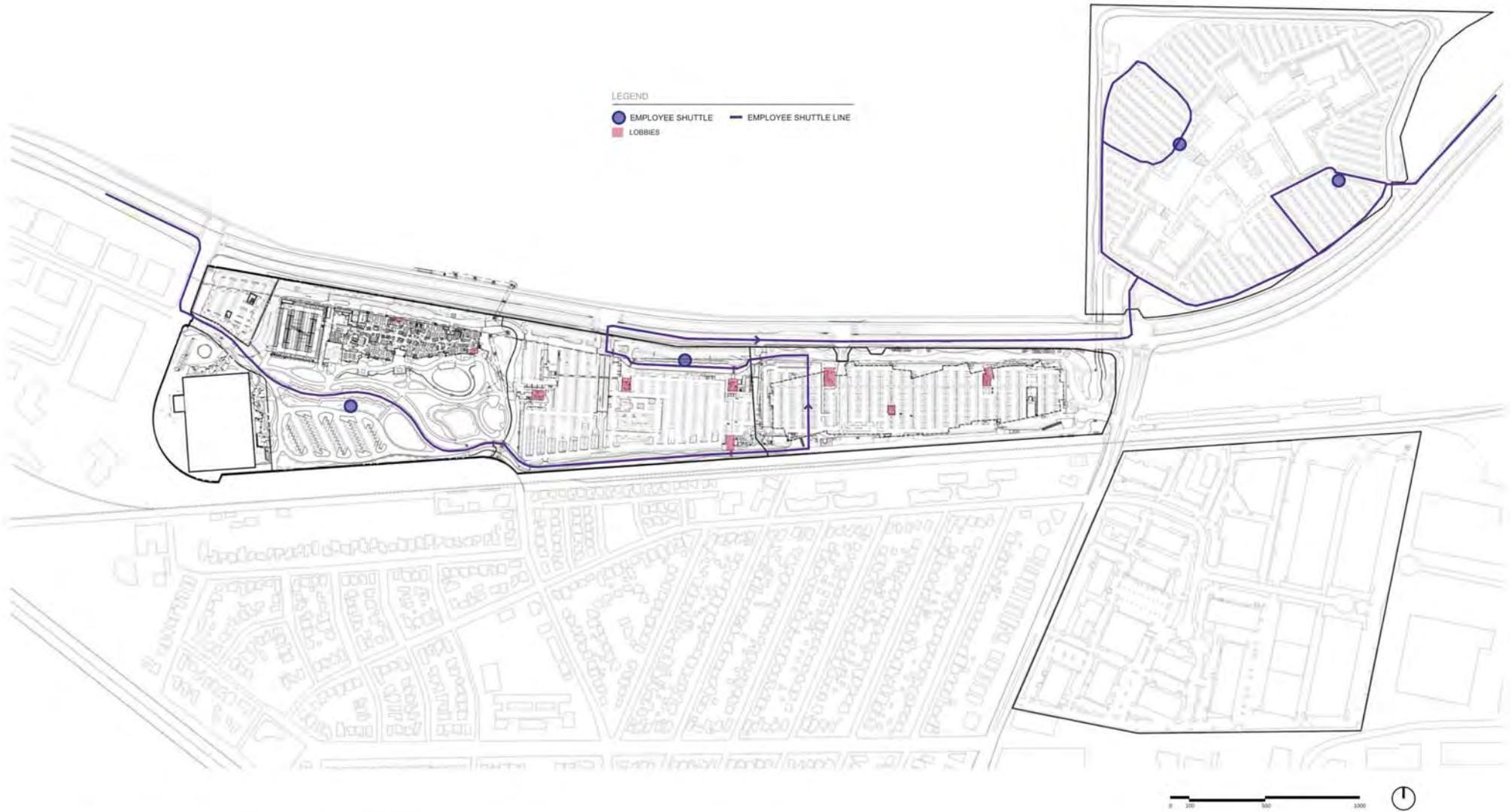


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-09 TRAFFIC DIAGRAMS - INTERIM EMPLOYEE SHUTTLE BUS SOUTHBOUND

MAY 15, 2017

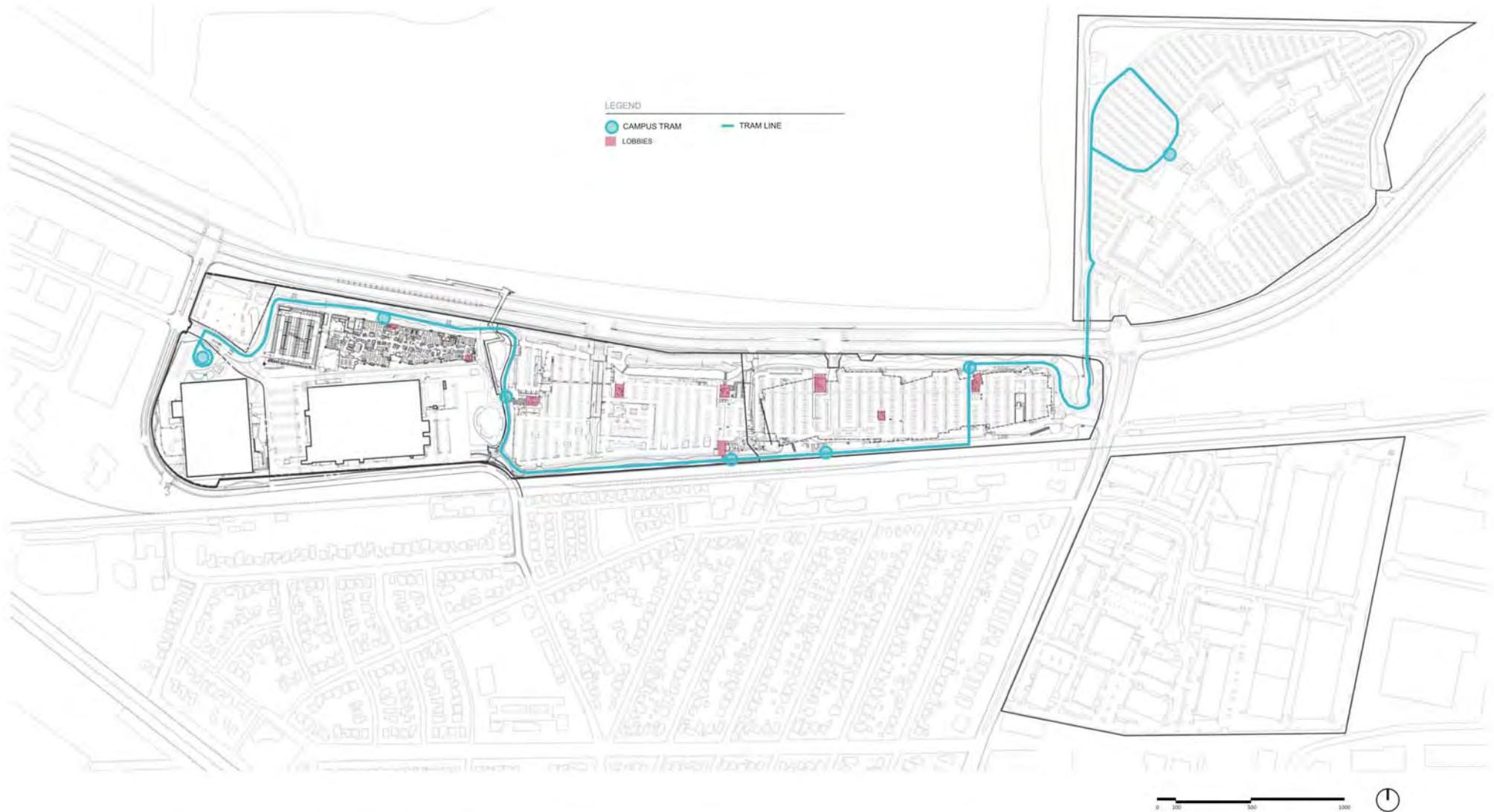


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-10 TRAFFIC DIAGRAMS - FINAL EMPLOYEE SHUTTLE BUS SOUTHBOUND

MAY 15, 2017

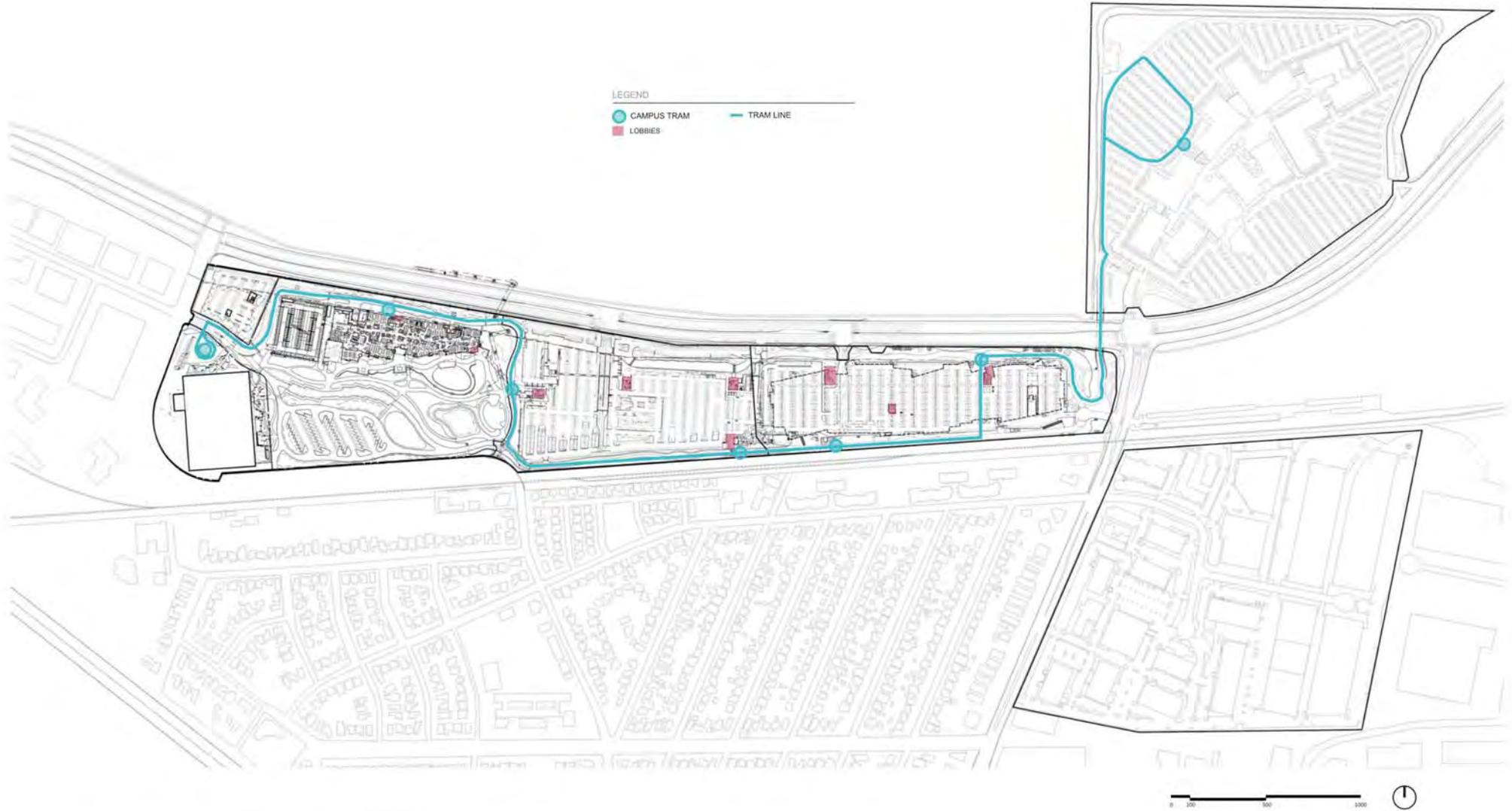


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-11 TRAFFIC DIAGRAMS - INTERIM EMPLOYEE INTRA CAMPUS TRAM

MAY 15, 2017

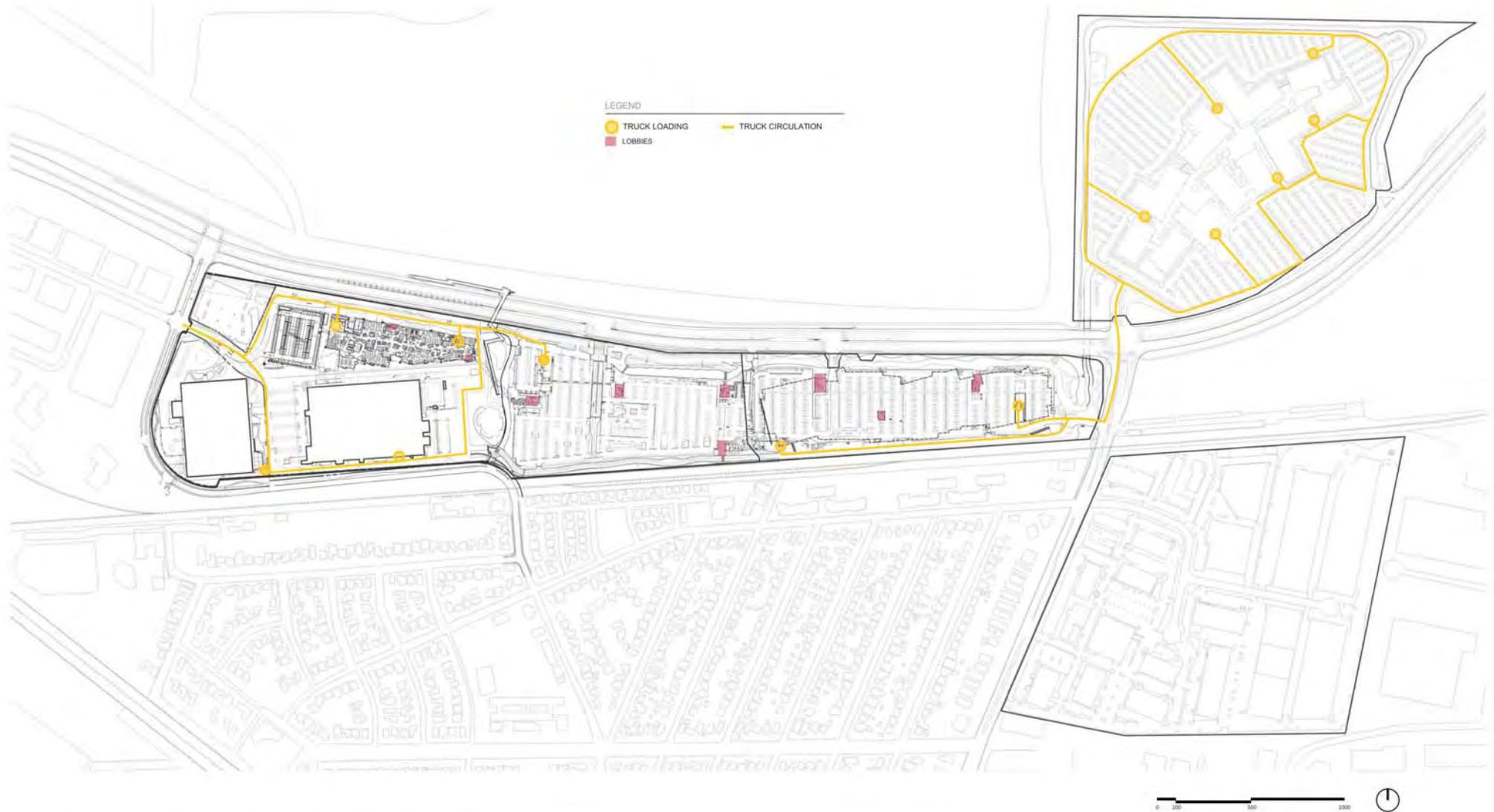


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-12 TRAFFIC DIAGRAMS - FINAL EMPLOYEE INTRA CAMPUS TRAM

MAY 15, 2017

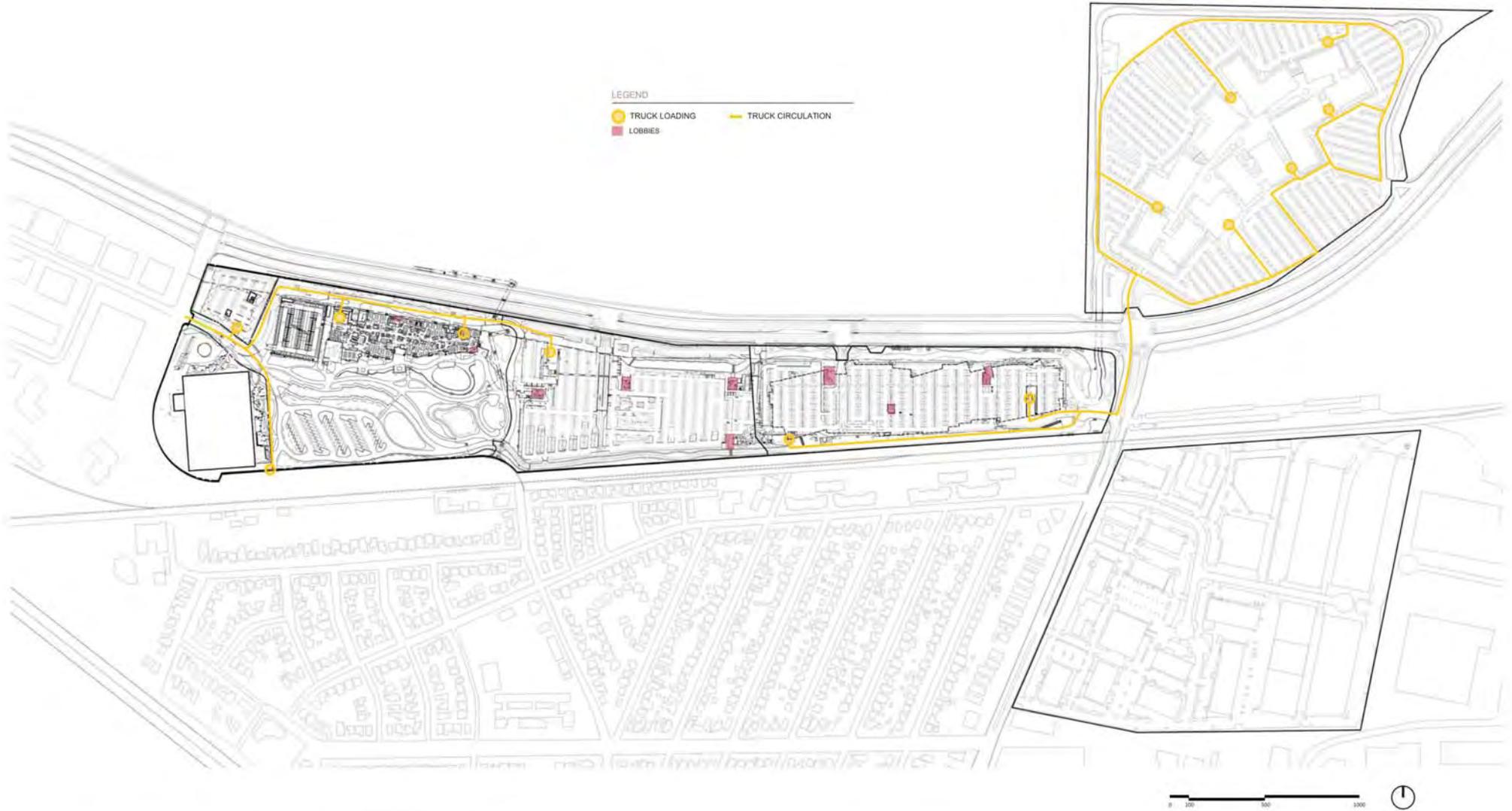


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-13 TRAFFIC DIAGRAMS - INTERIM TRUCK ACCESS

MAY 15, 2017

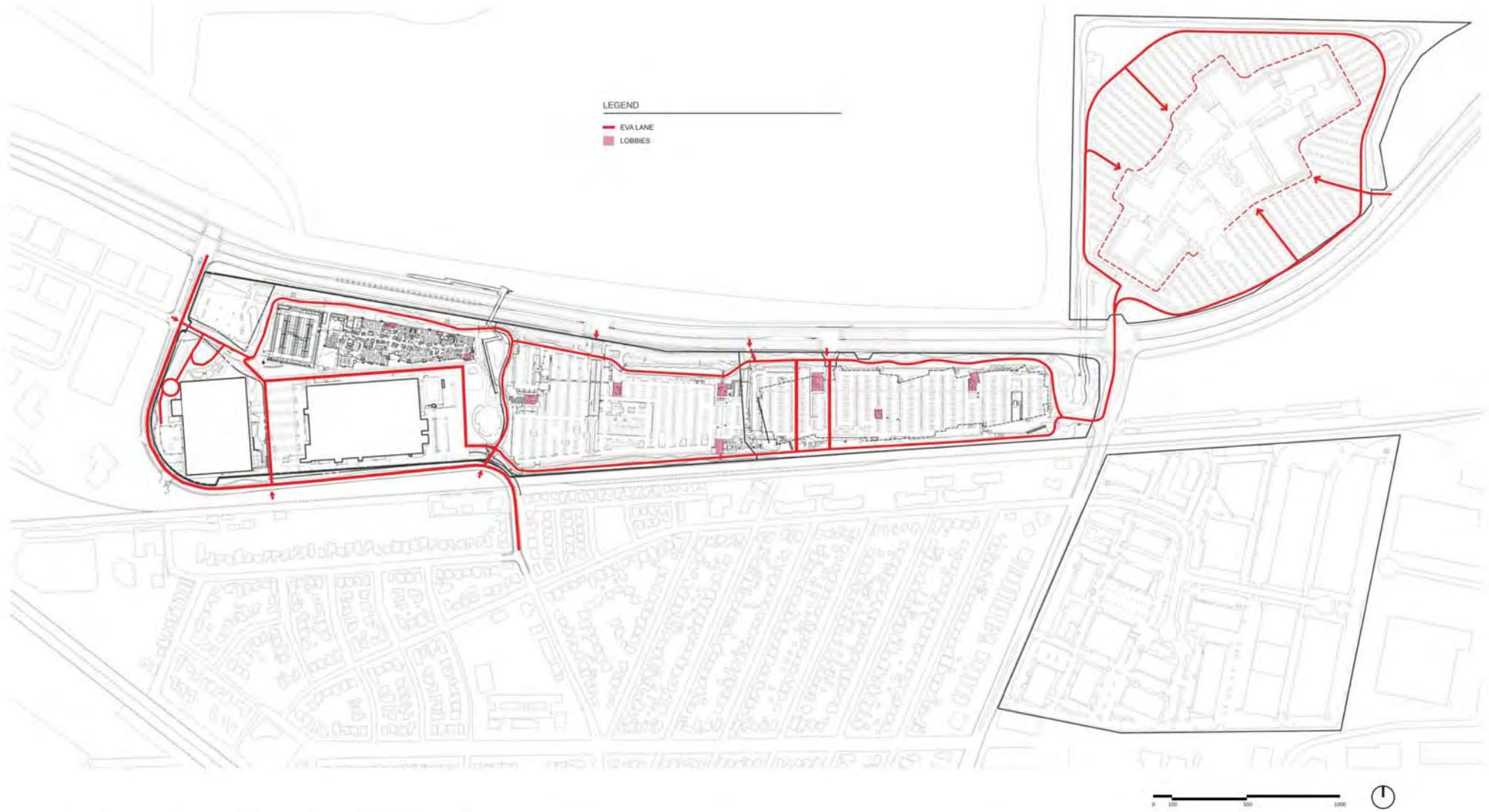


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-14 TRAFFIC DIAGRAMS - FINAL TRUCK ACCESS

MAY 15, 2017



## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

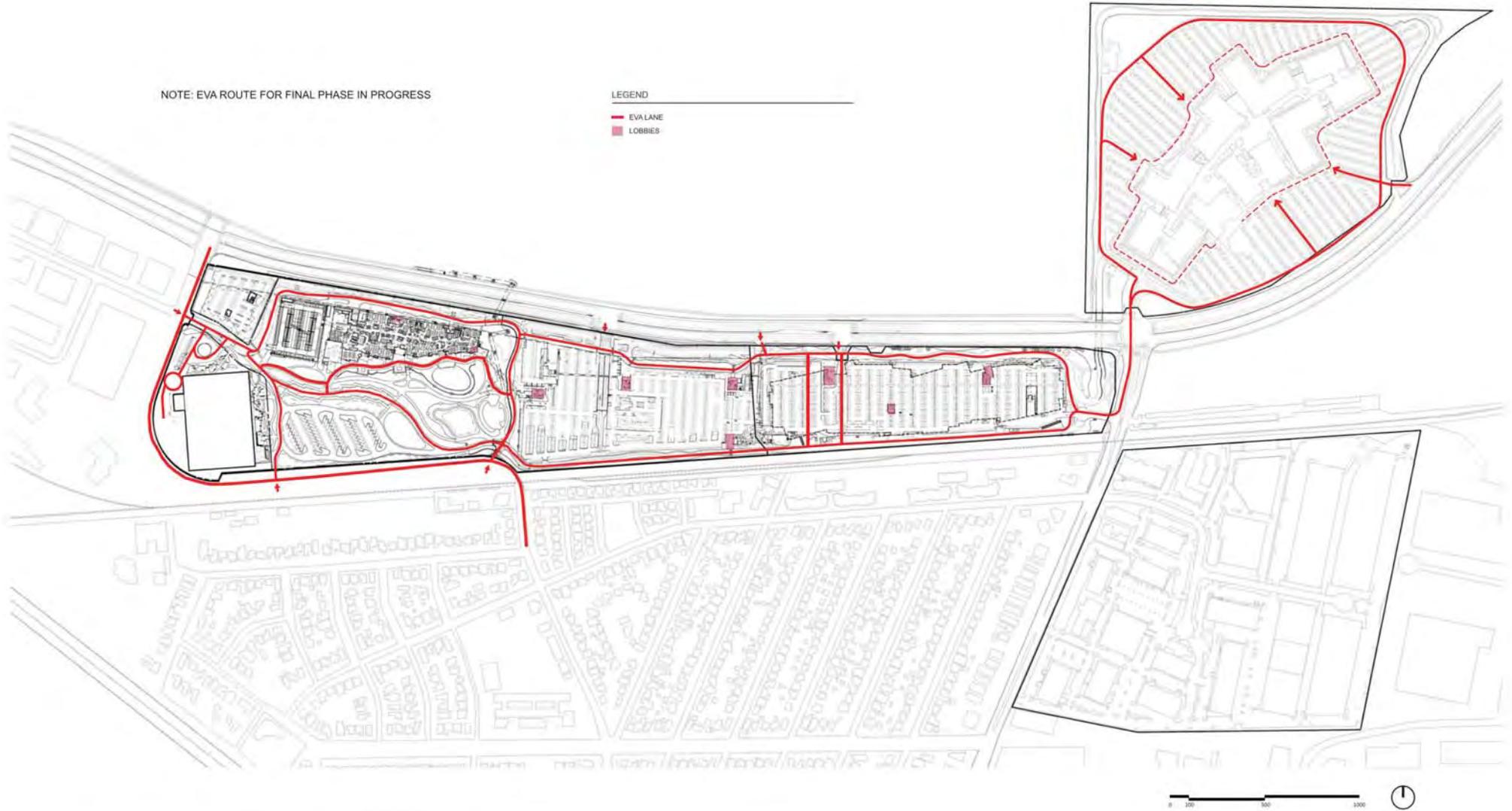
## T1-15 TRAFFIC DIAGRAMS - INTERIM EMERGENCY VEHICLE ACCESS

MAY 15, 2017

NOTE: EVA ROUTE FOR FINAL PHASE IN PROGRESS

LEGEND

- EVA LANE
- LOBBIES

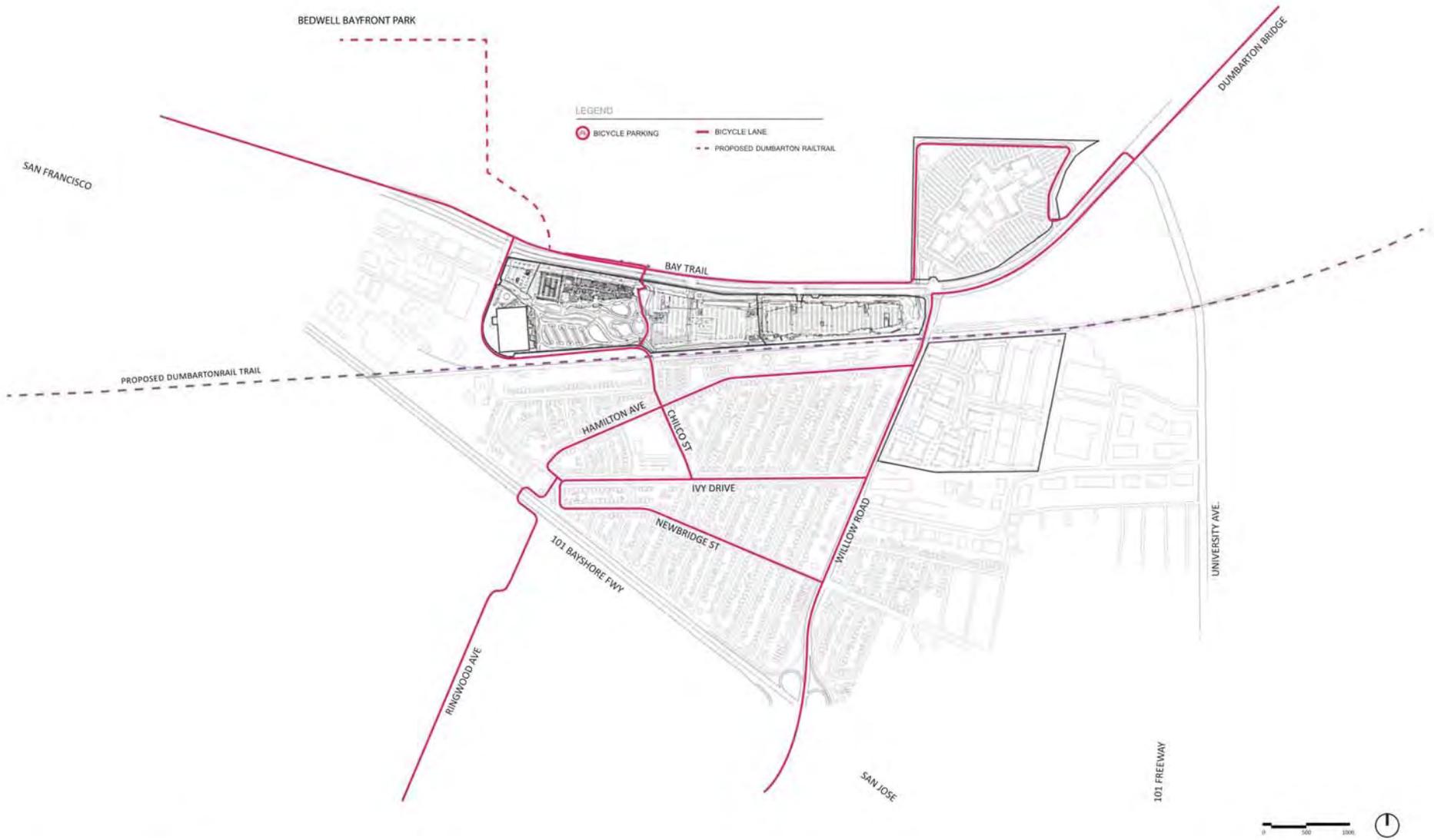


## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
301-309 Constitution Drive, Menlo Park, California  
Gehry Partners, LLP

## T1-16 TRAFFIC DIAGRAMS - FINAL EMERGENCY VEHICLE ACCESS

MAY 15, 2017



## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-17 TRAFFIC DIAGRAMS - FINAL REGIONAL BICYCLE ACCESS

MAY 15, 2017



## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

## T1-18 TRAFFIC DIAGRAMS - FINAL REGIONAL VEHICULAR ACCESS

MAY 15, 2017



**Facebook Campus Expansion**

Buildings 21, 22 & Hotel Site  
 301-309 Constitution Drive, Menlo Park, California  
 Gehry Partners, LLP

T1-19 TRAFFIC DIAGRAMS - FINAL REGIONAL PEDESTRIAN ACCESS

MAY 15, 2017



## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
301-309 Constitution Drive, Menlo Park, California  
CMG Landscape Architecture

INTERIM SITE PLAN

MAY 15, 2017



## Facebook Campus Expansion

Buildings 21, 22 & Hotel Site  
301-309 Constitution Drive, Menlo Park, California  
CMG Landscape Architecture

FINAL SITE PLAN

MAY 15, 2017

## **Attachment 4**

### **Hibiscus Property Description**



**EXHIBIT 'A'  
LEGAL DESCRIPTION  
LOT LINE ADJUSTMENT**

**ADJUSTED PARCEL 1**

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013, AS INSTRUMENT NO. 2013-006489, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT THE NORTHWEST CORNER OF SAID PARCEL 1, SAID CORNER BEING A POINT ON THE SOUTHERLY RIGHT OF WAY OF ROUTE 84 (BAYFRONT EXPRESSWAY), AS SAID ROUTE IS SHOWN ON THAT CERTAIN CALTRANS RIGHT OF WAY MAP FOR ROUTE 84 IN THE COUNTY OF SAN MATEO ON SHEETS R-105.2 THROUGH R-105.4;

THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL 1, SOUTH 64° 50' 10" EAST, 11.22 FEET;

THENCE CONTINUING ALONG SAID NORTHERLY LINE, SOUTH 81° 12' 00" EAST, 367.60 FEET;

THENCE SOUTH 22° 56' 17" WEST, 393.03 FEET;

THENCE NORTH 54° 23' 26" WEST, 198.82 FEET;

THENCE NORTH 65° 36' 59" WEST, 158.40 FEET TO A POINT ON WESTERLY LINE OF SAID PARCEL 1;

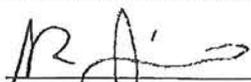
THENCE ALONG SAID WESTERLY LINE, NORTH 19° 16' 42" EAST, 238.47 FEET;

THENCE CONTINUING ALONG SAID WESTERLY LINE, NORTH 22° 32' 00" EAST, 18.08 FEET TO THE **POINT OF BEGINNING.**

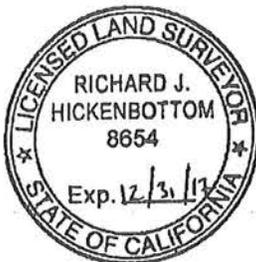
CONTAINING 2.60 ACRES OF LAND, MORE OR LESS.

AS SHOWN ON EXHIBIT "B", ATTACHED HERETO AND MADE A PART HEREOF.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.

  
RICHARD J. HICKENBOTTOM, LS 8654  
LICENSE EXPIRES: 12/31/17

2/16/17  
DATE





**EXHIBIT 'A'  
LEGAL DESCRIPTION  
LOT LINE ADJUSTMENT**

**ADJUSTED PARCEL 2**

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL 1 IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006489, AND ALL OF PARCEL A, AS SAID PARCEL A IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 13-01, RECORDED MAY 2, 2013 AS INSTRUMENT NO. 2013-066476, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT THE SOUTHEAST CORNER OF SAID PARCEL A, SAID CORNER BEING A POINT ON THE NORTHERLY LINE OF THE 100 FOOT WIDE SOUTHERN PACIFIC RAILROAD DUMBARTON CUT OFF RIGHT OF WAY;

THENCE ALONG THE SOUTHERLY LINE OF SAID PARCEL A AND SAID PARCEL 1, SOUTH 85° 08' 20" WEST, 3093.09 FEET;

THENCE ALONG THE SOUTHERLY AND WESTERLY LINES OF SAID PARCEL 1 THE FOLLOWING TWELVE (12) COURSES:

1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 50° 31' 12" WEST, HAVING A RADIUS OF 231.72 FEET, THROUGH A CENTRAL ANGLE OF 53° 54' 38" FOR AN ARC LENGTH OF 218.03 FEET,

2) SOUTH 85° 08' 20" WEST, 1370.12 FEET,

3) SOUTH 22° 32' 00" WEST, 42.84 FEET,

4) ALONG THE ARC OF A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 02° 53' 18" WEST, HAVING A RADIUS OF 335.00 FEET, THROUGH A CENTRAL ANGLE OF 112° 10' 00" FOR AN ARC LENGTH OF 655.82 FEET,

5) NORTH 19° 16' 42" EAST, 388.09 FEET,

6) ALONG THE ARC OF A CURVE THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 93° 36' 25" FOR AN ARC LENGTH OF 32.67 FEET,

7) SOUTH 67° 06' 53" EAST, 5.00 FEET,

8) NORTH 22° 53' 07" EAST, 30.00 FEET,

9) SOUTH 67° 06' 53" EAST, 5.63 FEET,

10) NORTH 22° 53' 07" EAST, 30.00 FEET,

11) NORTH 67° 06' 53" WEST, 5.00 FEET, AND

12) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 86° 23' 35" FOR AN ARC LENGTH OF 30.16 FEET;

THENCE SOUTH 65° 36' 59" EAST, 158.40 FEET;

THENCE SOUTH 54° 23' 26" EAST, 198.82 FEET;

THENCE NORTH 22° 56' 17" EAST, 393.03 FEET TO A POINT ON THE NORTHERLY LINE OF SAID PARCEL 1;

W:\2015\A15571-10\Documents\Legal Descriptions\A15571-10-LLA-ADJ PCL 2\_RECORD.docx

THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL 1, SOUTH 81° 12' 00" EAST, 2132.40 FEET;

THENCE ALONG THE NORTHERLY LINES OF SAID PARCEL 1 AND SAID PARCEL A, NORTH 89° 21' 50" EAST, 823.77 FEET TO THE NORTHWEST CORNER OF PARCEL B, AS SAID PARCEL IS DESCRIBED IN SAID LOT LINE ADJUSTMENT NO. 13-01 (2013-066476);

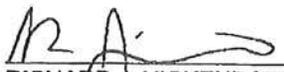
THENCE ALONG THE NORTHERLY AND EASTERLY LINES OF SAID PARCEL A THE FOLLOWING FIFTEEN (15) COURSES:

- 1) SOUTH 31° 06' 06" EAST, 39.45 FEET,
- 2) SOUTH 0° 38' 10" EAST, 40.00 FEET,
- 3) NORTH 89° 21' 50" EAST, 60.00 FEET,
- 4) NORTH 0° 38' 10" WEST, 40.00 FEET
- 5) NORTH 29° 49' 46" EAST, 39.45 FEET,
- 6) NORTH 89° 21' 50" EAST, 79.91 FEET,
- 7) SOUTH 31° 06' 06" EAST, 39.45 FEET,
- 8) NORTH 89° 21' 50" EAST, 60.00 FEET,
- 9) NORTH 29° 49' 46" EAST, 39.45 FEET,
- 10) NORTH 89° 21' 50" EAST, 1012.83 FEET,
- 11) SOUTH 82° 24' 08" EAST, 162.24 FEET,
- 12) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 45.00 FEET, THROUGH A CENTRAL ANGLE OF 81° 18' 03" FOR AN ARC LENGTH OF 63.85 FEET,
- 13) SOUTH 01° 06' 05" EAST, 171.90 FEET,
- 14) SOUTH 14° 09' 17" WEST, 107.79 FEET, AND
- 15) SOUTH 22° 13' 35" WEST, 112.49 FEET TO THE POINT OF BEGINNING.

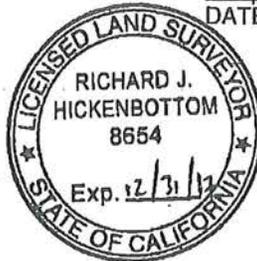
CONTAINING 77.71 ACRES OF LAND, MORE OR LESS.

AS SHOWN ON EXHIBIT "B", ATTACHED HERETO AND MADE A PART HEREOF.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.

  
RICHARD J. HICKENBOTTOM, LS 8654  
LICENSE EXPIRES: 12/31/17

2/16/17  
DATE





0 300' 600' 1200'  
Scale 1" = 600 ft

ADJUSTED PARCEL 1  
AREA = 2.60± AC.

PARCEL 1  
LLA 12-01  
2013-006489

PROPOSED  
PROPERTY LINE

ROUTE 84 (BAYFRONT EXPRESSWAY)

SEE SHEET 2  
SEE SHEET 3

ADJUSTED PARCEL 2  
AREA = 77.71± AC.

SEE SHEET 3  
SEE SHEET 4

PARCEL B  
LLA 13-01  
2013-066476

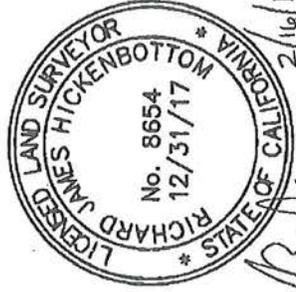
PARCEL A  
LLA 13-01  
2013-066476

PROPERTY LINE  
BEING REMOVED

S.P.R.R. DUMBARTON CUT OFF (100' WIDE)

CHILCO STREET

ROUTE 114 (WILLOW ROAD)



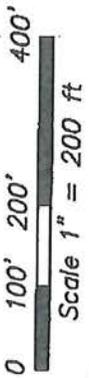
HIBISCUS  
LEGAL  
PROPERTIES, LLC

**KIER & WRIGHT**  
CIVIL ENGINEERS & SURVEYORS, INC.  
2850 Collier Canyon Road Phone (925) 245-8788  
Livermore, California 94551 Fax (925) 245-8796

**EXHIBIT "B"**  
**PLAT TO ACCOMPANY LEGAL DESCRIPTION**

MENLO PARK, CALIFORNIA

DATE	SEPTEMBER, 2016
SCALE	1" = 600'
BY	RJH
JOB NO.	A15571-10
SHEET	1 OF 4



ROUTE 84 (BAYFRONT EXPRESSWAY)  
S81°12'00"E 2132.40'

POINT OF BEGINNING  
ADJUSTED PARCEL 1

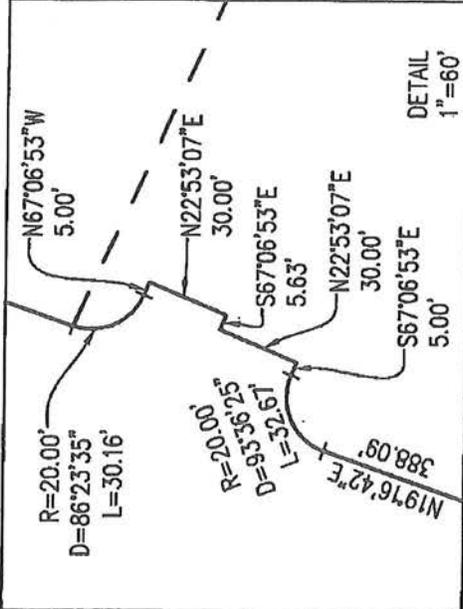
CONSTITUTION  
DRIVE

ADJUSTED PARCEL 1  
AREA = 2.60± AC.  
N65°36'59"W 158.40'  
N22°32'00"E 18.08'  
S81°12'00"E 367.60'  
N19°16'42"E 238.47'  
S22°56'17"W 393.03'  
N54°23'26"W 198.82'

PROPOSED  
PROPERTY LINE

PARCEL 1  
LLA 12-01  
2013-006489

ADJUSTED PARCEL 2  
AREA = 77.71± AC.



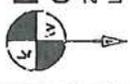
CHILCO STREET

S.P.R.R. DUMBARTON CUT OFF (100' WIDE)

S22°32'00"W 42.84'

S85°08'20"W 1370.12'

SEE SHEET 3



**KIER & WRIGHT**  
CIVIL ENGINEERS & SURVEYORS, INC.  
2850 Collier Canyon Road  
Livermore, California 94551  
Phone (925) 245-8788  
Fax (925) 245-8796

**EXHIBIT "B"**  
**PLAT TO ACCOMPANY LEGAL DESCRIPTION**

MENLO PARK, CALIFORNIA

DATE	SEPTEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-10
SHEET	2 OF 4



ROUTE 84 (BAYFRONT EXPRESSWAY)

S81°12'00"E 2132.40'

N89°21'50"E 823.77'

384.14'

439.63'

PARCEL 1  
LLA 12-01  
2018-006489

ADJUSTED PARCEL 2  
AREA = 77.71± AC.

PARCEL A  
LLA 13-01  
2018-066476

PROPERTY LINE  
BEING REMOVED  
S4°51'40"E 482.86'

S40°23'39"E

97.46'

S4°51'40"E

20.94'

1767.03'

1326.06'

S85°08'20"W 3093.09'

S.P.R.R. DUMBARTON CUT OFF (100' WIDE)

R=231.72'  
D=53°54'38"  
L=218.03'

S85°08'20"W 1370.12'

S50°31'12"(WR)

SEE SHEET 2

SEE SHEET 4

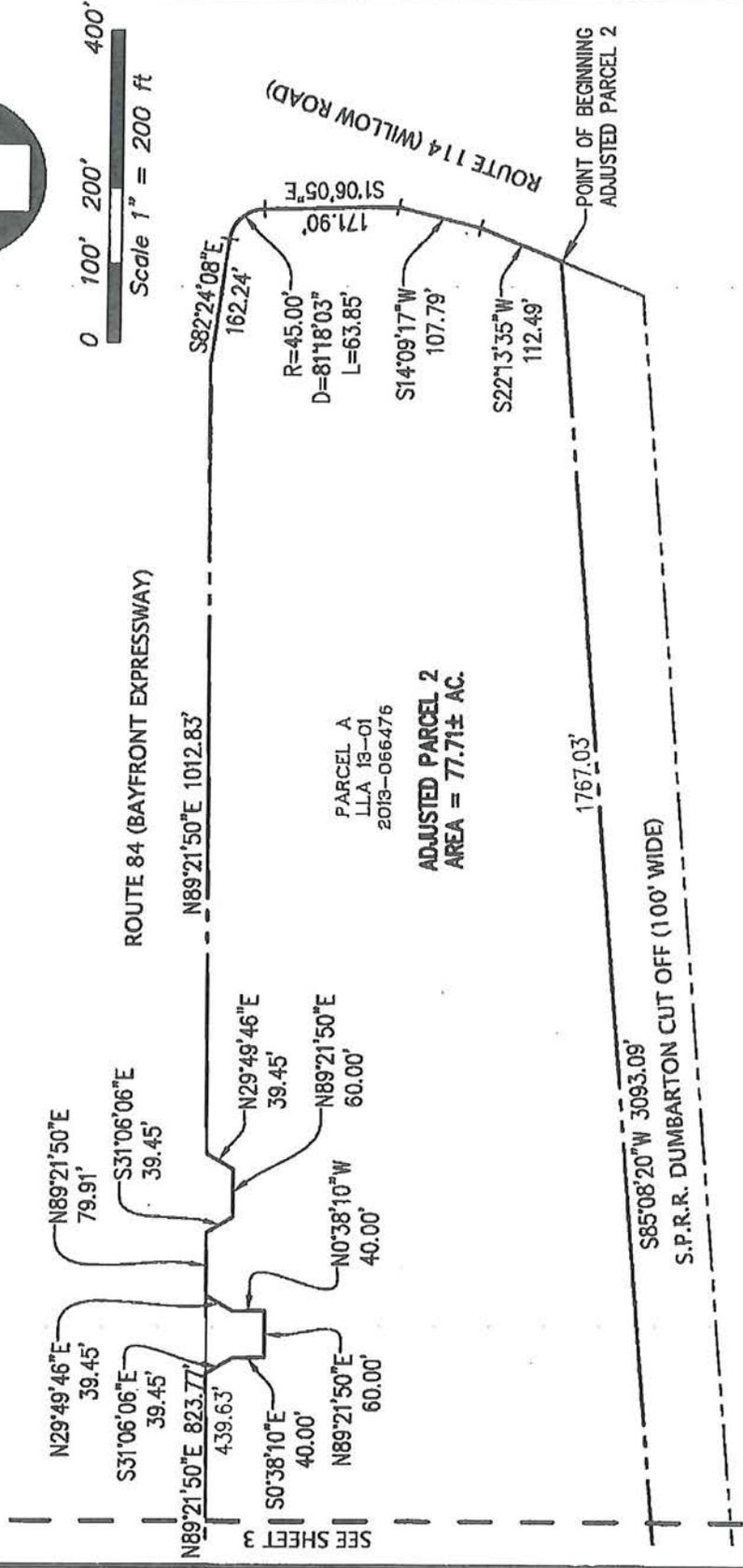
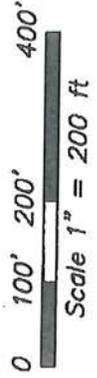


**KIER & WRIGHT**  
CIVIL ENGINEERS & SURVEYORS, INC.  
2850 Collier Canyon Road Phone (925) 245-8788  
Livermore, California 94551 Fax (925) 245-8796

DATE	SEPTEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-10
SHEET	3 OF 4

### EXHIBIT "B" PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK, CALIFORNIA



DATE	SEPTEMBER, 2016
SCALE	1" = 200'
BY	R/JH
JOB NO.	A15571-10
SHEET	4 OF 4

**EXHIBIT "B"**

**PLAT TO ACCOMPANY LEGAL DESCRIPTION**

MENLO PARK, CALIFORNIA

**KIER & WRIGHT**  
**CIVIL ENGINEERS & SURVEYORS, INC.**  
 2850 Collier Canyon Road  
 Livermore, California 94551  
 Phone (925) 245-8788  
 Fax (925) 245-8796

z:\2015\A15571-10\Survey\A15571-10-LLA.dwg 2-16-17 01:01:32 PH mburro1

## **Attachment 5**

### **Draft Bridge Maintenance Agreement**

WHEN RECORDED MAIL TO  
City of Menlo Park  
701 Laurel Street  
Menlo Park, CA 94025  
Attn: City Clerk

Documentary Transfer Tax: \$-0- No apparent value  
"No fee required" (Government Code Section 6103 &  
27383)  
Recorded for the benefit of the City of Menlo Park.

The undersigned declares this instrument to be exempt  
from Recording Fees (Govt. Code § 27383) and  
Documentary Transfer Tax (Rev. & Tax. Code §11922).

(Space above this line reserved for Recorder's use only)

**MAINTENANCE AGREEMENT**

This Maintenance Agreement ("Agreement") is made and entered into effective as of \_\_\_\_\_, 2018 ("Effective Date"), by and between Hibiscus Properties, LLC ("Facebook"), and the CITY OF MENLO PARK, a California municipal corporation ("City,") each referred to as a "Party" and collectively referred to as the "Parties" with reference to the following facts:

**RECITALS**

- A. Facebook is the project sponsor and lead developer of the Facebook Campus Expansion Project, a redevelopment of an approximate 58-acre site located at 301-309 Constitution Drive within the City of Menlo Park, County of San Mateo, State of California, which is now particularly described in Exhibit A, attached hereto and incorporated herein by this reference, (the "Property").
- B. The Facebook Expansion Project includes two office buildings, a parking garage, and a hotel. The project is subject to an "Amended and Restated Conditional Development Permit," recorded December 16, 2016, as instrument 2016-133796, in Official Records of the County of San Mateo, which sets forth certain conditions that apply to the project (the "Conditions of Approval"). Conditions 9.44 and 11.15 of the Conditions of Approval require Facebook to enter into a long-term maintenance agreement for certain improvements to be constructed within Caltrans' right-of-way, as well as the Bicycle and Pedestrian Bridge. Facebook is also obligated to maintain certain on-site landscaping improvements installed as part of the project.
- C. Facebook is also obligated to maintain certain improvements from MPK 20, also known as the West Campus. The West Campus entitlements were approved by the City Council in March and April of 2013. Construction was completed in 2016. The project is governed by the "Second Amended and Restated Conditional Development Permit," recorded December 19, 2017, as instrument 2017-114053, in Official Records of the County of San Mateo, which sets forth certain conditions that apply to the project (the "Conditions of Approval").
- D. The projects described above in paragraphs B and C are hereinafter referred to as the "Project."
- E. The City and Facebook desire to enter into this Agreement for the purpose of assuring the maintenance, periodic inspection, repair, and, if and when necessary, replacement of certain improvements by Facebook pursuant to the Conditions of Approval and this agreement.

**AGREEMENT**

NOW, THEREFORE, in consideration of the terms and conditions of this Agreement, the Parties agree as follows:

1. **Obligation to Maintain Certain Improvements.** Facebook, at its sole expense, shall be responsible for the maintenance, periodic inspection, repair, and, if and when necessary, replacement of the improvements which have been, or are to be, constructed as part of the Project (the "Improvements") as described below and as depicted on Exhibits B-K, attached hereto.

Number	Reference	Location	Improvements to be maintained*	Jurisdiction
1	CDP 8.2	Access Improvements (new access from Bayfront Expressway)	Utility improvements, curb cuts, driveway, signal, frontage improvements (See Exhibit B)	Caltrans
2	CDP 11.3, 11.15	Bicycle Pedestrian bridge over Bayfront Expressway	New bridge (See Exhibit B)	Caltrans
4	TRA-1.1f	Constitution Drive and Chilco Street intersection	New traffic signal, median modifications, signing and striping, landscaping (See Exhibit C)	City of Menlo Park
5	TRA-1.1m	Bayfront Expressway and Building 21 entrance	New Building 21 signalized entrance from Bayfront Expressway (See Exhibit B)	Caltrans
6	TRA-4.1	Constitution Drive	New sidewalk, pedestrian crosswalks and curb ramps (See Exhibit D)	City of Menlo Park
7	TRA-5.1a	Hamilton Avenue	Bicycle boulevard treatments between Chilco and the ped/bike overcrossing of US 101 (See Exhibit E)	City of Menlo Park
8	TRA-5.1b	Northbound access to the project site for bicyclists	Improvements for northbound bicyclists to cross Willow and access the project site (See Exhibit F)	Caltrans
9	DA 7.1.6	Chilco Streetscape Improvements phases 1-4	Bicycle barrier, bioswale, bike lane, median, landscaping, stormwater maintenance facilities (See Exhibit G)	City of Menlo Park
10	DA 7.1.7	Chilco Streetscape Improvements phases 5-6	Bicycle barrier, bioswale, bike lane, median, landscaping, stormwater maintenance facilities (See Exhibit G)	City of Menlo Park
11	MPK 20	Bayfront Expressway along MPK 20 frontage	Facebook is responsible to maintain drainage system and landscaping along Bayfront Expressway (Exhibit H)	Caltrans
12	MPK 20	Willow Road along MPK 20 frontage	Facebook is responsible to maintain drainage system and landscaping along Willow Road (Exhibit I and J)	Caltrans
13	MPK 20	Bayfront Expressway, post mile 27.3 to 27.7,	Traffic signal operation and maintenance cost. Facebook is	Caltrans

		new traffic signal, new left turn to Facebook West Campus	responsible for paying 100% of traffic signal operation and maintenance costs (Exhibit K)	
14	MPK 20	Emergency vehicle access on Willow Road, between Bayfront Expressway and railroad tracks	Facebook is responsible to maintain landscaping in median island. (Exhibit I)	Caltrans

\* Intended as a summary but not comprehensive list of improvements to be maintained by Facebook

All repairs and replacement shall comply with Caltrans and City standards, at a minimum. Facebook shall further perform the following with respect to the Improvements:

- a. Regularly maintain, repair and replace the Improvements in a good condition and in a way that presents a healthy, neat and orderly appearance;
- b. Maintain all plazas, open space, sidewalks, paths, separated bicycle lanes, and other paved areas in clean and weed-free condition, and keep such areas reasonably clear of dirt, mud, trash, debris or other matter which is unsafe or unsightly;
- c. Perform such other acts that are reasonably necessary or desirable to preserve and protect the Improvements and the appearance, safety and operation thereof in accordance with this Agreement and the foregoing standard.
- d. Facebook shall be responsible for performing all of the City’s maintenance and other responsibilities required pursuant to that certain Freeway Maintenance Agreement, “For Pedestrian Overcrossing and Various Improvements in the City of Menlo Park,” Bayfront Expressway POC (Br.No. 35-0358) and Various Improvements SM-84-PM 26.4/27.6.
- e. For any other improvements within Caltrans jurisdiction listed above, for which the City enters into a Maintenance Agreement, Facebook shall be responsible for performing all of the City’s maintenance and other responsibilities required pursuant to such Maintenance Agreement.
- f. Prior to completion of construction of the POC, Facebook shall prepare and submit a maintenance plan and schedule to the Director of Public Works for review and approval, and shall comply with such approved Maintenance Plan.

2. **Reservation of Management Authority.** Facebook reserves the right to delegate its obligations under this Agreement to an agent or manager or to designate another party to perform the rights and obligations of Facebook at its sole discretion (such party, a “Third-Party Manager”). Facebook’s appointment of a Third-Party Manager shall not relieve Facebook of its obligations under this Agreement.
3. **Compliance with Applicable Laws.** All maintenance work shall conform to all applicable federal and state Occupation Safety and Health Act standards and regulations for the performance of maintenance. Any and all chemicals, unhealthful substances, and pesticides used in and during maintenance shall be applied in accordance with all governmental requirements.
4. **Changes and Modifications to Improvements.** If Facebook determines that changes or modifications to the Improvements are reasonably necessary to ensure that the Improvements are adequately maintained, Facebook shall notify the City in writing of such determination and of the changes/modifications that Facebook believes to be necessary, and submit (if necessary) a building and/or encroachment permit application, complete with plans and required fees, to the City for approval. Any and all changes shall be consistent with the Conditions of Approval. Any changes in Caltrans right of way shall be subject to Caltrans approvals and Caltrans terms and conditions for work within its right of way.
5. **Encroachment Permit; Right of Entry.** Facebook and the City acknowledge that, to the extent that the Improvements are located within the City rights-of-way, Facebook will be required to obtain encroachment permit (or permits) from the City to allow Facebook to perform its

obligations under this Agreement. The City agrees to expedite review and approval of any application for an encroachment permit pursuant to this Section 5. Facebook is responsible for the payment of any encroachment permit fees, unless otherwise waived by the Public Works Director. Prior to any work in the public right of way, Facebook shall submit typical traffic control plans for approval for work performed in the City right of way. Facebook shall submit supplementary traffic control plans for unusual circumstances that are out of the ordinary for right of way maintenance. The encroachment permit shall set forth the terms and provisions upon which Facebook has a right to enter into such rights-of-way in order to perform maintenance, inspection, and repair and, if and when necessary, replacement services (collectively, "Maintenance Services"), consistent with the following:

- a. **Permitted Uses.** Facebook may enter upon such Improvements as are located within the City rights-of-way, and may temporarily block reasonably necessary portions of the adjoining street surfaces, solely for the purpose of performing Maintenance Services, and incidental purposes thereto, such as operating equipment and storing materials during the period maintenance services are being performed. Facebook shall not use such areas for any other purpose.
  - b. **Insurance.** Facebook shall obtain and deliver to the City, at no cost to the City, certificates of comprehensive public liability and property damage insurance naming City, its officers, agents, and employees as additional insureds in a combined single limit of \$1,000,000.00 for the death and injury of any persons in any one occurrence; and for property damage in any one occurrence. A certificate of said insurance policy shall be provided to City prior to commencement of any work within the City rights-of-way pursuant to this Agreement.
  - c. **Permits and Approvals.** To the extent that performance of the Maintenance Services requires other governmental permits or consents from agencies other than the City, Facebook shall, at its sole cost and expense, diligently pursue obtaining such approvals or consents at its own expense.
6. **Term.** This Agreement shall commence immediately upon the Effective Date and shall continue in perpetuity until and unless terminated by the City. In the event that the City determines that the maintenance obligations under this Agreement are no longer required, then the City, at the request of Facebook, shall execute a release of this Agreement, which Facebook shall record in the County Recorder's Office at its sole expense.
  7. **Indemnification.** Facebook agrees, while engaged in the Maintenance Services provided for in this Agreement, to place and maintain suitable safeguards sufficient to prevent injury to any persons and to indemnify, defend and hold harmless the City, its officers, representatives, and employees from and against any and all claims for loss, injury or damage (the "Claims") resulting from the prosecution of said work except to the extent any Claim arises out of the negligence or willful misconduct of the City.
  8. **Default.** The failure to maintain the Improvements will constitute an event of default. Upon such event of default, the City shall provide written notice to Facebook. Upon receipt of the written notice, Facebook shall have thirty (30) days to remedy such event of default (or such longer period of time as may reasonably be required, provided that Facebook shall commence to remedy such default within thirty (30) days period and thereafter diligently prosecute such remedy to completion). If Facebook fails to remedy the event of default within the prescribed time period, the City shall have the right to do all work necessary to remedy the event of default and charge Facebook actual costs incurred by the City for such work. Any disputes about the occurrence of an event of default shall be resolved pursuant to the dispute resolution procedure set forth in Section 9.
  9. **Hazards.** Hazards as determined by the City shall either be repaired or temporarily repaired by Facebook as quickly as reasonably possible, but in no event longer than ten (10) days of written notification of the hazard. If hazard is not repaired or temporarily repaired within ten (10) days,



Attn: City Attorney

14. **MISCELLANEOUS.**

- a. **Entire Agreement, Amendments.** This Agreement contains the entire understanding and agreement of the parties. This Agreement may be altered, amended or modified only by an instrument in writing executed by the Parties.
- b. **Recitals.** The Recitals are hereby incorporated by reference into the terms of this Agreement.
- c. **Governing Law.** This Agreement shall be construed and governed in accordance with the laws of the State of California. Venue shall be the County of San Mateo.
- d. **Counterparts.** This Agreement may be executed in any number of counterparts which together shall constitute the Agreement of the Parties.
- e. **Exhibits.** Any and all exhibits and schedules attached or to be attached hereto are hereby incorporated and made a part of the Agreement by reference.
- f. **Severability.** If any term, provision, covenant or condition of this Agreement is held to be invalid, void or unenforceable, the remainder of its provisions shall remain in full force and effect.
- g. **Authority.** Each person executing this Agreement on behalf of a Party represents and warrants that such person is duly and validly authorized to do so on behalf of the entity it purports to bind and if such party is a partnership, corporation or trustee, that such partnership, corporation or trustee has full right and authority to enter into this Agreement and perform all of its obligations hereunder.
- h. **No Agency Relationship.** Neither Facebook nor any of Facebook's agents, contractors or subcontractors are or shall be considered to be agents of City in connection with the performance of any of Facebook's obligations under this Agreement.
- i. **Attorneys' Fees and Costs.** Either party may bring a lawsuit to enforce or require performance of the terms of this Agreement, and the prevailing party in such suit or proceeding shall be entitled to recover from the other party's reasonable costs and expenses, including attorneys' fees.
- j. **Covenants Running with Lands.** The obligations of Facebook herein shall be binding on Facebook's successors and assigns and shall constitute covenants running with the land and binding on future owners of the land.

Attachments: Exhibit A      Legal Description of the Property  
 Exhibit B-K      Maintenance Areas

(Signature blocks on following page.)

IN WITNESS HEREOF, the parties have executed this Agreement as of the date first stated above.

**FACEBOOK:**

HIBISCUS PROPERTIES LLC  
a Delaware limited liability company

\_\_\_\_\_  
Name, Title

\_\_\_\_\_  
Date

**APPROVED AS TO FORM:**

\_\_\_\_\_  
William L. McClure, City Attorney

\_\_\_\_\_  
Date

**CITY OF MENLO PARK:**

\_\_\_\_\_  
Alex D. McIntyre, City Manager

\_\_\_\_\_  
Date

**ATTEST:**

\_\_\_\_\_  
Judi A. Herren, City Clerk

\_\_\_\_\_  
Date

**Exhibit A**

Legal Description

**Exhibits B-K**

Maintenance Areas

## **Attachment 6**

**PG&E Easements  
(LD 2305-03-1503,  
2305-03-2103 and  
2350-03-2243)**

13890AB

RECORDED AT REQUEST OF

PACIFIC GAS AND ELECTRIC CO.

JAN 24 11 28 AM 1968

OFFICIAL RECORDS  
SAN MATEO COUNTY  
Marian Blumh  
Recorder

EXHIBIT "B"

*Stands 60KV T/L  
(Stands Lines Reconstruction)*

RET. TO  
PACIFIC GAS AND ELECTRIC CO.  
20065 STEVENS CRK. BLVD., RM. 105  
CUPERTINO, CALIFORNIA 95014

*A 40  
A 40*

Exchange Value \$2,374.00

RAYCHEM CORPORATION, a California corporation,  
hereinafter called first party, in consideration of value adequate therefor paid by PACIFIC GAS AND ELECTRIC COMPANY, a California corporation, hereinafter called second party, the receipt whereof is hereby acknowledged, hereby grants to second party the right to erect, construct, reconstruct, replace, remove, maintain and use a line of towers with such wires and cables as second party shall from time to time suspend therefrom for the transmission of electric energy, and for communication purposes, and all necessary and proper foundations, footings, crossarms and other appliances and fixtures for use in connection with said towers, wires and cables, together with a right of way, on, along and in all of the hereinafter described strip of those certain lands which are situate in the City of Menlo Park County of San Mateo, State of California, and are described as follows:

The three parcels of land, (a) described and designated PARCEL I in the deed from L. C. Smith and others to Raychem Corporation dated February 1, 1967 and recorded in the office of the County Recorder of said County of San Mateo in Book 5273 of Official Records at page 163, (b) described in the deed from A. G. Thies and others to Raychem Corporation dated January 27, 1965 and recorded in the office of said County Recorder in Book 4908 of Official Records at page 318, and (c) described in the deed from David D. Bohannon and wife to Raychem Corporation dated January 27, 1965 and recorded in the office of said County Recorder in Book 4910 of Official Records at page 325.

The aforesaid strip is described as follows:

1. A strip of land of the uniform width of 95 feet extending from the easterly boundary line of the parcel of land described and designated PARCEL I in the deed from L. C. Smith and others to Raychem Corporation dated February 1, 1967 and recorded in the office of the County Recorder of said County of San Mateo in Book 5273 of Official Records at page 163, northwesterly to the northwesterly boundary line of the parcel of land described in the deed from David D. Bohannon and wife to Raychem Corporation dated January 27, 1965 and recorded in the office of said County Recorder in Book 4910 of Official Records at page 325 and lying 37.5 feet on the northerly side and 57.5 feet on the southerly side of the line which begins at a point in the easterly boundary line of said parcel of land designated PARCEL I and runs thence north 84° 15' west 85 feet, more or less; thence north 80° 06-1/2' west 730.6 feet, to a point herein for convenience called Point "A"; thence continuing north 80° 06-1/2'

13890AB

west 113.5 feet, to a point that is distant 37.5 feet southerly from (measured at a right angle to) the northerly boundary line of the parcel of land described in the deed from A. G. Thies and others to Raychem Corporation dated January 27, 1965 and recorded in the office of said County Recorder in Book 4908 of Official Records at page 318; thence parallel with the northerly boundary line of the parcel of land described in said deed recorded in Book 4908 of Official Records at page 318 and continuing 37.5 feet southerly from (measured at a right angle to) the northerly boundary line of the parcel of land described in said deed recorded in Book 4910 of Official Records at page 325 the following two courses, namely: north  $81^{\circ} 18'$  west 1317.0 feet, to a point herein for convenience called Point "B"; thence north  $64^{\circ} 56'$  west 12 feet, more or less, to a point in the northwesterly boundary line of the parcel of land described in said deed recorded in Book 4910 of Official Records at page 325; said Point "A" bears south  $4^{\circ} 57\text{-}1/2'$  east 40.8 feet distant, from the  $7/8$  inch iron pipe (tagged R.C.E. 5476) marking the northeasterly corner of the parcel of land described in said deed recorded in Book 4908 of Official Records at page 318.

First party further grants to second party the right to erect, construct, reconstruct, replace, remove, maintain and use a line of steel poles with such wires as second party shall from time to time suspend therefrom for the transmission of electric energy, and for communication purposes, and all necessary and proper foundations, footings, crossarms, guys, anchors and other appliances and fixtures for use in connection with said poles and wires, together with a right of way, on, along and in all of the strip of land described as follows:

2. A strip of land of the uniform width of 50 feet extending from the southerly boundary line of the strip of land hereinbefore described and designated 1 southwesterly and southeasterly to the northwesterly and westerly boundary line of the strip of land described in the deed from David D. Bohannon and wife to Pacific Gas and Electric Company dated August 5, 1952 and recorded in the office of said County Recorder in Book 2290 of Official Records at page 519 and lying equally on each side of the line which begins at a point in the southerly boundary line of said strip of land designated 1 from which said Point "B" bears north  $11^{\circ} 41\text{-}1/2'$  east 57.6 feet distant and runs thence south  $22^{\circ} 27\text{-}1/2'$  west 771.0 feet; thence south  $16^{\circ} 51\text{-}1/2'$  east 398.1 feet, more or less, to a point in the northwesterly boundary line of the strip of land described in said deed dated August 5, 1952.

First party also grants to second party, the right to erect, maintain, replace, remove and use a line of poles with all necessary and proper crossarms, braces, anchors, guys and other appliances and fixtures for use in connection therewith, and to suspend therefrom, maintain and use such wires as second party shall from time to time deem necessary for the transmission and distribution of electric energy within the boundary lines of said strip of land designated 1.

First party, for the consideration aforesaid, further grants to second party, the right of ingress to and egress from said strip over and across said lands by means of roads and lanes thereon, if such there be, otherwise by such route or routes as shall occasion the least practicable damage and inconvenience to first party, provided, that such right of ingress and egress shall not extend to any portion of said lands which is isolated from said strip by any public road or highway, now crossing or hereafter crossing said lands.

First party shall have the right to use said strip for purposes not inconsistent with second party's full enjoyment of the rights hereby granted, provided that first party shall not erect or construct any building or other structure, or drill or operate any well, within said strip.

Second party shall have the further right to install, maintain and use gates in all fences which now cross or shall hereafter cross said strip.

Second party shall also have the right from time to time to trim and to cut down and clear away any and all trees and brush now or hereafter on said strip and shall have the further right from time to time to trim and to cut down and clear away any trees on either side of said strip which now or hereafter in the opinion of second party may be a hazard to said towers, wires or cables, by reason of the danger of falling thereon, provided, however, that all trees which second party is hereby authorized to cut and remove, if valuable for timber or wood, shall continue to be the property of first party, but all tops, lops, brush and refuse wood shall be burned or removed by second party.

Second party shall also have the right to mark the location of said strip by suitable markers set in the ground or on said towers, but said markers when set in the ground shall be placed in fences or other locations which will not interfere with any reasonable use first party shall make of said strip.

Second party shall repair any damage it shall do to first party's private roads or lanes on said lands, and shall indemnify first party against any loss and damage which shall be caused by the exercise of said ingress and egress, or by any wrongful or negligent act or omission of second party or of its agents or employees in the course of their employment.

The provisions hereof shall inure to the benefit of and bind the successors and assigns of the respective parties hereto.

IN WITNESS WHEREOF first party has executed these presents this Fifteenth.....day of  
December....., 19 67.

RAYCHEM CORPORATION

By *R.P. Wechsner*  
Its Vice President

And By *[Signature]*  
Its Assistant Secretary

Executed in the presence of

*Marsha Jenkins*

Witness



FOR NOTARY'S USE ONLY

San Jose  
E.S. 2683  
Dwgs. 28221  
& 28222  
& 026289  
T. 5S., R. 3W.,  
M.D.B. & M.  
Section 23  
N $\frac{1}{2}$  of SW $\frac{1}{4}$   
NW $\frac{1}{4}$  of SE $\frac{1}{4}$   
67-205 SS

Prepared *[Signature]*

Checked *[Signature]*

OCT. 31 1967

STATE OF CALIFORNIA,  
San Mateo

} ss.

County of \_\_\_\_\_  
On this 15th day of December in the year one thousand nine hundred and 67

before me, Helen M. Gordon, a Notary Public,  
State of California, duly commissioned and sworn, personally appeared

R. W. Muchmore and A. O. Armstrong

known to me to be the Vice President and Assistant Secretary respectively  
of the corporation described in and that executed the within instrument, and also known to me to be  
the person who executed the within instrument on behalf of the corporation therein named, and  
acknowledged to me that such corporation executed the same.

IN WITNESS WHEREOF I have hereunto set my hand and affixed my official seal in the  
County of San Mateo the day and year in this certificate  
first above written.

*Helen M. Gordon*

Notary Public, State of California.

LD 2305-03-2103  
2000006 (22-00-011) 2-00-2  
Cellular One, C1-859,  
Bair-Cooley Landing 60kV, Twr 2/17

DOC # 2001-026990

03/05/2001 09:56A AG Fee:16.00

Page 1 of 4

Recorded in Official Records

County of San Mateo

Warren Slocum

Assessor-County Clerk-Recorder

Recorded By BARBARA COLLIGNON



AFTER RECORDING, RETURN TO:

**PACIFIC GAS AND ELECTRIC COMPANY**  
N10A, 245 Market Street  
P.O. Box 770000  
San Francisco, California 94177

CONSIDERATION: \$15,000

Location: City/Unit Menlo Park  
Recording Fee \$ 1614.00  
Document Transfer Tax \$ 5.50

- Computed on Full Value of Property Conveyed, or  
 Computed on Full Value Less Liens & Encumbrances  
Remaining at Time of Sale

Barbara Collignon  
Signature of declarant or agent determining tax

4p

AGREEMENT  
MODIFYING AN EASEMENT

TYCO/RAYCHEM ELECTRONICS

hereinafter called Grantor, makes this agreement with PACIFIC GAS AND ELECTRIC COMPANY, a California corporation, hereinafter called PG&E, affecting the lands of Grantor, situate in the City of Menlo Park, County of San Mateo, State of California, and described as follows:

(APN 055-244-01)

The parcel of land described in the deed from David D. Bohannon and Ophelia E. Bohannon, his wife, to Raychem Corporation, dated January 27, 1965, and recorded in Book 4910 of Official Records at page 325, San Mateo County Records; EXCEPTING THEREFROM the parcel of land described and designated PARCEL 46737-3 in the final order of condemnation, wherein Raychem Corporation, et al, are defendants, dated July 27, 1983 and recorded as Document Number 83078012, Official Records of the County of San Mateo.

PG&E is the owner of the following right of way and easement (hereinafter called the Easement) across Grantor's said lands:

The right of way and easement conveyed by Raychem Corporation to PG&E by deed dated December 15, 1967 and recorded in Volume 5422 of Official Records at page 99, San Mateo County Records, for the construction and reconstruction of electric transmission facilities.



PG&E desires, and Grantor is willing, to modify the Easement to include additional rights for telecommunications facilities:

NOW, THEREFORE, in consideration of the foregoing and for good and valuable consideration paid to Grantor by PG&E, the receipt of which is hereby acknowledged, the Grantor hereby grants to PG&E the additional right to install, construct, maintain, repair, replace, use and operate for the purposes of transmitting information in any form, telecommunications equipment, including, but not limited to, wires, fiberoptic cables, powered transceiver stations, and antennae, and related appliances, appurtenances, and fixtures, affixed to or suspended from PG&E's towers or placed upon or in the ground adjacent to PG&E's towers or poles within the strips of land described in said Easement, together with the right to install, receive and maintain utility services therefor.

Grantor further grants to PG&E the right to assign, lease, permit, or license to telecommunications service companies any or all of the additional rights granted herein together with the right of ingress and egress granted in said Easement. PG&E's right to assign, lease, permit, or license to telecommunications service companies any or all of the additional rights granted herein together with the right of ingress and egress shall be subject to Grantor's prior written consent, not unreasonably withheld; further, Grantor shall have the right to approve, not unreasonably withheld, the instrument of assignment, lease, permit or license proposed by Grantee. Grantee's Assignee's right of ingress and egress shall be subject to 48 hours prior notice to Grantor's Customer Service Department.

All work undertaken by PG&E and/or its assignee(s) shall be performed in accordance with plans submitted to and approved by Grantor, which approval shall not be unreasonably withheld.

Except as expressly set forth herein, this agreement shall not in any way alter, modify, or terminate any provision of said Easement. Except as expressly provided herein, PG&E and assignee(s) shall have no additional rights in Grantor's property.

This Agreement shall inure to the benefit of and bind the successors and assigns of the respective parties hereto.

IN WITNESS WHEREOF the parties hereto have executed this Agreement this \_\_\_\_\_ day of June 23, 2000.

TYCO/RAYCHEM ELECTRONICS

PACIFIC GAS AND ELECTRIC COMPANY

By SK Nesline  
SK Nesline  
Director, Site Services

By Jared W. Brown  
Jared Brown  
Supervisor - Land Rights Services

By \_\_\_\_\_



# CALIFORNIA ALL-PURPOSE ACKNOWLEDGMENT

State of California  
County of San Mateo } ss.

On 06/23/2000, before me, Linda M. Costa, Notary Public,  
Date Name and Title of Officer (e.g., Jane Doe, Notary Public)  
personally appeared Spence Leslie,  
Name(s) of Signer(s)

- personally known to me
- proved to me on the basis of satisfactory evidence



to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

Linda M. Costa  
Signature of Notary Public

Place Notary Seal Above

## OPTIONAL

Though the information below is not required by law, it may prove valuable to persons relying on the document and could prevent fraudulent removal and reattachment of this form to another document.

### Description of Attached Document

Title or Type of Document: Agreement Modifying an Easement

Document Date: 6/23/2000 Number of Pages: 3

Signer(s) Other Than Named Above: —

### Capacity(ies) Claimed by Signer

Signer's Name: Spence Leslie

- Individual
- Corporate Officer — Title(s): \_\_\_\_\_
- Partner —  Limited  General
- Attorney in Fact
- Trustee
- Guardian or Conservator
- Other: \_\_\_\_\_

Signer Is Representing: \_\_\_\_\_

RIGHT THUMBPRINT OF SIGNER

Top of thumb here



CALIFORNIA ALL-PURPOSE ACKNOWLEDGEMENT

State of California  
County of San Francisco

On 7/26/00, before me, Jeanette Johnson, Notary Public, personally appeared Jared W. Brown, personally known to me to be the person whose name is subscribed to the within instrument and acknowledged to me that he executed the same in his authorized capacity, and that by his signature on the instrument the person, or the entity upon behalf of which the person acted, executed the instrument.

WITNESS my hand and official seal



Jared Brown, Supervisor Land Rights Services  
Representing: Pacific Gas & Electric Company

Golden Gate Region  
Peninsula Division  
Auth.: 8031565  
T5S, R3W, MDB&M  
Sec 23, N2 of SW4  
Ref Dwg.: 28221  
Aff: 2305-03-1503  
Prepared: JBO  
Revised: JBO

**RECORDING REQUESTED BY AND RETURN TO:**

**PACIFIC GAS AND ELECTRIC COMPANY**  
**San Jose Land Services Office**  
**111 Almaden Boulevard, Room 814**  
**San Jose, CA 95113**

Location: City/Uninc MENLO PARK

Recording Fee \$ 42-

Document Transfer Tax \$ NONE

This is a conveyance where the consideration and Value is less than \$100.00 (R&T 11911).

Computed on Full Value of Property Conveyed, or

Computed on Full Value Less Liens & Encumbrances Remaining at Time of Sale

[Signature]  
Signature of declarant or agent determining tax

**2017-037078**

2:05 pm 04/28/17 ES Fee: 42.00

Count of Pages 10

Recorded in Official Records

County of San Mateo

Mark Church

Assessor-County Clerk-Recorder



(SPACE ABOVE FOR RECORDER'S USE ONLY)

LD# 2305-03-2243

**EASEMENT DEED**

109

HIBISCUS PROPERTIES, LLC, a Delaware limited liability company,

hereinafter called Grantor, hereby grants to PACIFIC GAS AND ELECTRIC COMPANY, a California corporation, hereinafter called Grantee, the right from time to time to construct, reconstruct, install, inspect, maintain, replace, remove, and use facilities of the type hereinafter specified, together with a right of way therefor, within the easement areas as hereinafter set forth, and also ingress thereto and egress therefrom, over and across the lands of Grantor situate in the City of Menlo Park, County of San Mateo, State of California, described as follows:

(APN 055-260-250)

The parcel of land described in the deed from Tyco Electronics Corporation, a Pennsylvania corporation to Hibiscus Properties, LLC dated September 4, 2014 and recorded as Document No. 2014-079881, San Mateo County Records.

Said facilities and easement area are described as follows:

Such underground conduits, pipes, manholes, service boxes, wires, cables, and electrical conductors; aboveground marker posts, risers, and service pedestals; underground and aboveground switches, fuses, terminals, and transformers with associated concrete pads; and fixtures and appurtenances necessary to any and all thereof, as Grantee deems necessary for the distribution of electric energy and communication purposes located within the strips of land of various widths, the center lines are delineated by the heavy lines as described in Exhibit "A" and shown upon Exhibit "B", attached hereto and made a part hereof.

PG&E  
COPY

Grantor further grants to Grantee the right, from time to time, to trim or to cut down any and all trees and brush now or hereafter within said easement areas, and shall have the further right, from time to time, to trim and cut down trees and brush along each side of said easement areas which now or hereafter in the opinion of Grantee may interfere with or be a hazard to the facilities installed hereunder, or as Grantee deems necessary to comply with applicable state or federal regulations.

Grantor shall not erect or construct any building or other structure or drill or operate any well within said easement areas.

Grantor further grants to Grantee the right to assign to another public utility as defined in Section 216 of the California Public Utilities Code the right to install, inspect, maintain, replace, remove and use underground communications facilities within said easement areas (including ingress thereto and egress therefrom).

Grantor acknowledges that they have read the "Grant of Easement Disclosure Statement", Exhibit "C", attached hereto and made a part hereof.

The provisions hereof shall inure to the benefit of and bind the successors and assigns of the respective parties hereto.

Dated: NOV. 1, 2016.

HIBISCUS PROPERTIES, LLC, a Delaware limited liability company

*SM*  
  
By John Tenanes, Member

By \_\_\_\_\_, Member



A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

State of California  
County of San Mateo )

On November 1, 2016, before me, Kristen Ellering, Notary Public,  
Here insert name and title of the officer  
personally appeared John Tenanes

who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Kristen Ellering  
Signature of Notary Public



(Seal)

**CAPACITY CLAIMED BY SIGNER**

- Individual(s) signing for oneself/themselves
- Corporate Officer(s) of the above named corporation(s)
- Trustee(s) of the above named Trust(s)
- Partner(s) of the above named Partnership(s)
- Attorney(s)-in-Fact of the above named Principal(s)
- Other \_\_\_\_\_

**EXHIBIT "A"**  
**LEGAL DESCRIPTION**  
**PG&E RULE 20 EASEMENT PARCEL 1**

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013, AS INSTRUMENT NO. 2013-006489, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

A 10.00 FOOT WIDE STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:

**COMMENCING** AT A POINT ON THE SOUTHERLY RIGHT OF WAY FOR HIGHWAY ROUTE 84, SAID POINT ALSO BEING THE NORTHEAST CORNER OF SAID PARCEL 1;

THENCE ALONG THE EAST LINE OF SAID PARCEL 1, SOUTH 4° 51' 40" EAST, 36.23 FEET TO THE **POINT OF BEGINNING**;

THENCE NORTH 87° 01' 32" WEST, 514.54 FEET;

THENCE NORTH 80° 00' 54" WEST, 160.89 FEET TO A POINT LABELED "A" FOR CONVENIENCE;

THENCE NORTH 49° 10' 35" WEST, 15.70 FEET;

THENCE NORTH 88° 11' 18" WEST, 119.93 FEET;

THENCE NORTH 81° 12' 55" WEST, 483.71 FEET;

THENCE NORTH 83° 57' 51" WEST, 65.68 FEET;

THENCE NORTH 80° 05' 01" WEST, 53.61 FEET TO A POINT LABELED "B" FOR CONVENIENCE;

THENCE NORTH 51° 28' 26" WEST, 30.62 FEET TO A POINT LABELED "C" FOR CONVENIENCE, ALSO BEING THE TERMINUS OF THIS 10.00 FOOT WIDE STRIP.

**TOGETHER WITH** A 10.00 WIDE STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:

**BEGINNING** AT THE ABOVE REFERENCED POINT "A";

THENCE NORTH 80° 57' 32" WEST, 20.72 FEET;

THENCE NORTH 88° 02' 12" WEST, 32.92 FEET;

THENCE NORTH 57° 59' 55" WEST, 14.29 FEET TO THE TERMINUS OF THIS 10.00 WIDE STRIP.

**TOGETHER WITH** A 10.00 FOOT STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:

**BEGINNING** AT THE ABOVE REFERENCED POINT "B";

THENCE NORTH 60° 18' 36" WEST, 16.43 FEET TO A POINT LABELED "D" FOR CONVENIENCE, ALSO BEING THE TERMINUS OF THIS 10.00 FOOT WIDE STRIP.

**TOGETHER WITH** AN 11.50 FOOT WIDE STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:

**BEGINNING AT THE ABOVE REFERENCED POINT "C";**

THENCE NORTH 81° 12' 48" WEST, 20.00 FEET TO A POINT LABELED "E" FOR CONVENIENCE, SAID POINT ALSO BEING THE TERMINUS OF THIS 11.50 WIDE STRIP.

**TOGETHER WITH AN 11.50 FOOT STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:**

**BEGINNING AT THE ABOVE REFERENCED POINT "D";**

THENCE NORTH 81° 12' 48" WEST, 20.00 FEET TO A POINT LABELED "F" FOR CONVENIENCE;

THENCE NORTH 81° 12' 48" WEST, 11.31 FEET TO THE TERMINUS OF THIS 11.50 WIDE STRIP.

**TOGETHER WITH A 10.00 FOOT STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:**

**BEGINNING AT THE ABOVE REFERENCED POINT "E";**

THENCE SOUTH 09° 12' 08" WEST, 78.80 FEET;

THENCE SOUTH 81° 20' 34" EAST, 32.00 FEET TO THE TERMINUS OF THIS 10.00 WIDE STRIP.

**TOGETHER WITH A 10.00 FOOT STRIP OF LAND, THE CENTERLINE OF WHICH BEING DESCRIBED AS FOLLOWS:**

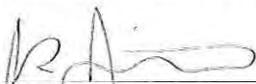
**BEGINNING AT THE ABOVE REFERENCED POINT "F";**

THENCE SOUTH 07° 43' 13" WEST, 69.47 FEET TO THE TERMINUS OF THIS 10.00 WIDE STRIP.

CONTAINING 16,989 SQUARE FEET OF LAND, MORE OR LESS.

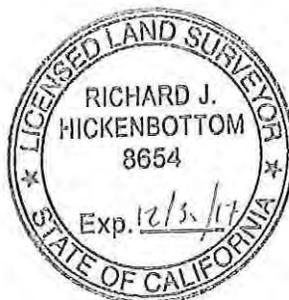
THE BEARING, NORTH 15° 53' 58" EAST, BETWEEN TWO FOUND MONUMENTS ON WILLOW ROAD, AS SHOWN ON THAT CERTAIN PARCEL MAP FILED FOR RECORD ON DECEMBER 31, 1998 IN BOOK 71 OF PARCEL MAPS AT PAGES 15 AND 16, SAN MATEO COUNTY RECORDS, WAS TAKEN FOR THE BASIS OF ALL BEARINGS FOR THIS DESCRIPTION.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.



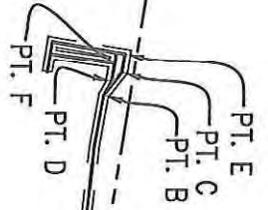
RICHARD J. HICKENBOTTOM, LS 8654  
LICENSE EXPIRES: 12/31/17

3/6/10  
DATE



SEC. 24, T5S, R3W, M.D.B. & M.

ROUTE 84

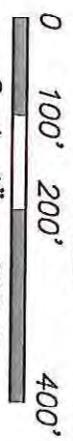


SEE SHEET 3  
SEE SHEET 2

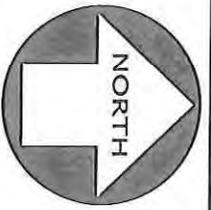


PG&E RULE 20 EASEMENT  
PARCEL 1  
AREA = 17,806± S.F.

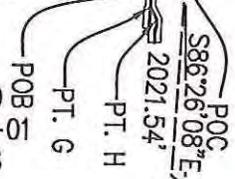
LANDS OF HIBISCUS PROPERTIES LLC  
APN=055-260-250  
PARCEL 1  
LLA 12-01  
DOC# 2013-006489



Scale 1" = 200 ft



LANDS OF GIANT PROPERTIES LLC  
APN=055-260-290  
PARCEL A - LLA 13-01  
DOC# 2013-066476



WILLOW ROAD

N15°53'58"E 652.33' BASIS OF BEARINGS

FND MON (71-PM-15)



CHILCO STREET

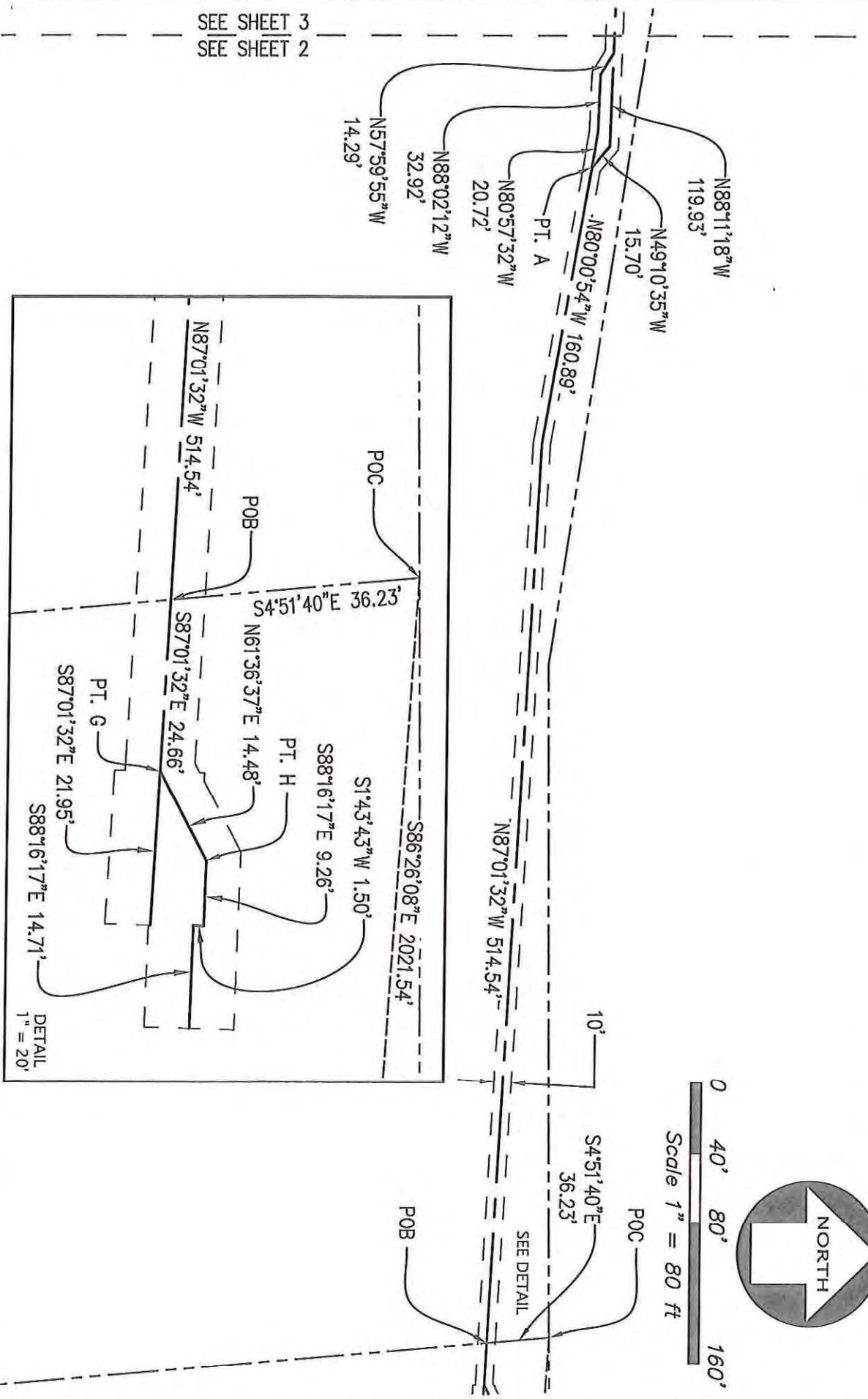
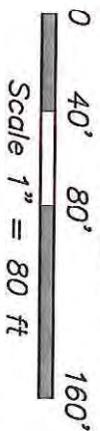
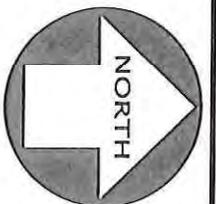
S.P.R.R. DUMBARTON CUT OFF (100' WIDE)



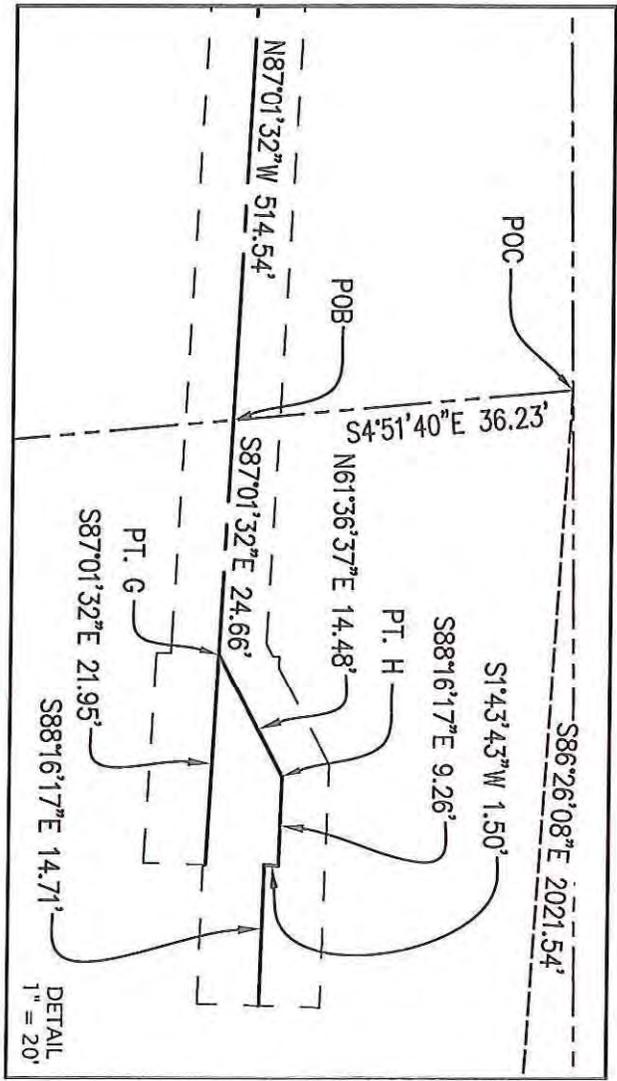
**KIER & WRIGHT**  
CIVIL ENGINEERS & SURVEYORS, INC.  
2850 Collier Canyon Road  
Livermore, California 94551  
Phone (925) 245-8788  
Fax (925) 245-8796

**EXHIBIT "B"**  
**PG&E RULE 20 EASEMENT**  
**PLAT TO ACCOMPANY LEGAL DESCRIPTION**  
MENLO PARK,  
CALIFORNIA

DATE	SEPT. 2016
SCALE	1" = 200'
BY	R/H
JOB NO.	A15571-24
SHEET	1 OF 3



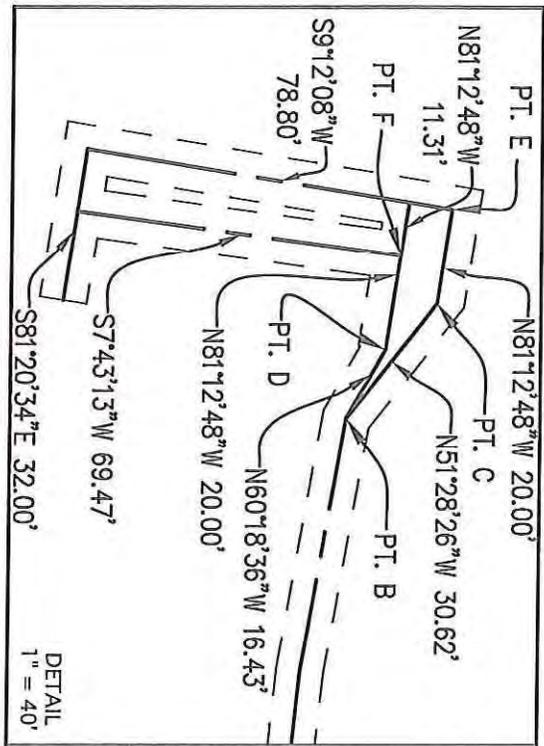
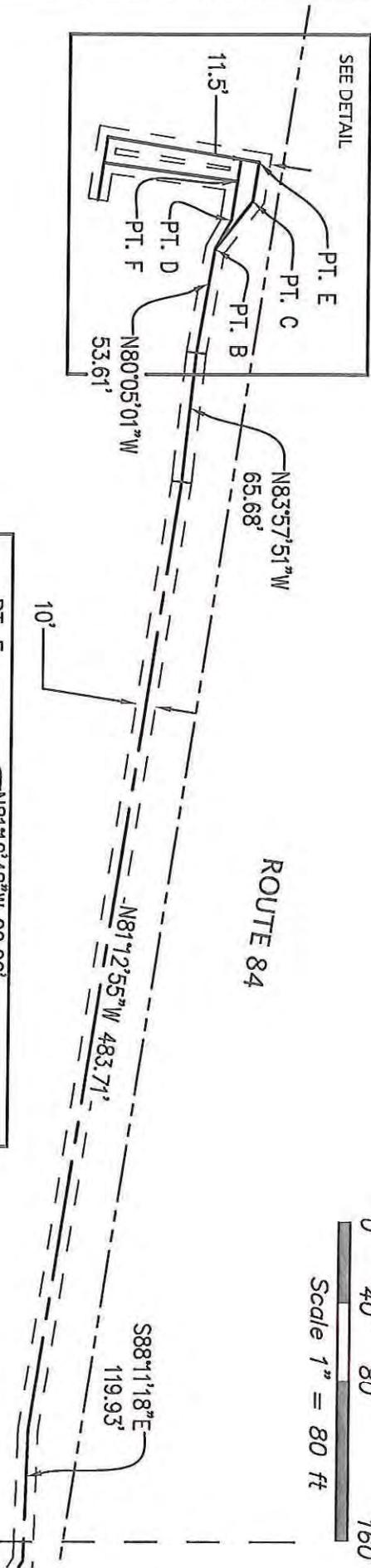
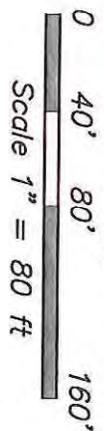
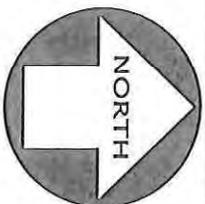
SEE SHEET 3  
SEE SHEET 2



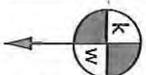
**KIER & WRIGHT**  
**CIVIL ENGINEERS & SURVEYORS, INC.**  
 2850 Collier Canyon Road  
 Livermore, California 94551  
 Phone (925) 245-8788  
 Fax (925) 245-8796

**EXHIBIT "B"**  
**PG&E RULE 20 EASEMENT**  
**PLAT TO ACCOMPANY LEGAL DESCRIPTION**  
**MENLO PARK,**  
**CALIFORNIA**

DATE	SEPT. 2016
SCALE	1" = 80'
BY	RJH
JOB NO.	A15571-24
SHEET	2 OF 3



SEE SHEET 3  
SEE SHEET 2



**KIER & WRIGHT**  
**CIVIL ENGINEERS & SURVEYORS, INC.**  
 2850 Collier Canyon Road  
 Livermore, California 94551  
 Phone (925) 245-8788  
 Fax (925) 245-8796

**EXHIBIT "B"**  
**PG&E RULE 20 EASEMENT**  
**PLAT TO ACCOMPANY LEGAL DESCRIPTION**  
**MENLO PARK,**  
**CALIFORNIA**

DATE	SEPT. 2016
SCALE	1" = 80'
BY	RJH
JOB NO.	A15571-24
SHEET	3 OF 3



Pacific Gas and Electric Company



## EXHIBIT "C"

### GRANT OF EASEMENT DISCLOSURE STATEMENT

This Disclosure Statement will assist you in evaluating the request for granting an easement to Pacific Gas and Electric Company (PG&E) to accommodate a utility service extension to PG&E's applicant. **Please read this disclosure carefully before signing the Grant of Easement.**

- You are under no obligation or threat of condemnation by PG&E to grant this easement.
- The granting of this easement is an accommodation to PG&E's applicant requesting the extension of PG&E utility facilities to the applicant's property or project. Because this easement is an accommodation for a service extension to a single customer or group of customers, PG&E is not authorized to purchase any such easement.
- By granting this easement to PG&E, the easement area may be used to serve additional customers in the area. Installation of any proposed facilities outside of this easement area will require an additional easement.
- Removal and/or pruning of trees or other vegetation on your property may be necessary for the installation of PG&E facilities. You have the option of having PG&E's contractors perform this work on your property, if available, or granting permission to PG&E's applicant or the applicant's contractor to perform this work. Additionally, in order to comply with California fire laws and safety orders, PG&E or its contractors will periodically perform vegetation maintenance activities on your property as provided for in this grant of easement in order to maintain proper clearances from energized electric lines or other facilities.
- The description of the easement location where PG&E utility facilities are to be installed across your property must be satisfactory to you.
- The California Public Utilities Commission has authorized PG&E's applicant to perform the installation of certain utility facilities for utility service. In addition to granting this easement to PG&E, your consent may be requested by the applicant, or applicant's contractor, to work on your property. Upon completion of the applicant's installation, the utility facilities will be inspected by PG&E. When the facility installation is determined to be acceptable the facilities will be conveyed to PG&E by its applicant.

By signing the Grant of Easement, you are acknowledging that you have read this disclosure and understand that you are voluntarily granting the easement to PG&E. Please return the signed and notarized Grant of Easement with this Disclosure Statement attached to PG&E. The duplicate copy of the Grant of Easement and this Disclosure Statement is for your records.

Attach to LD 2305-03-2243  
Area 1, Peninsula Division  
San Jose Land Service Office  
Operating Department: Electric Distribution  
MDB&M, T.05S., R.03W., SEC. 23, NW¼ of SE¼  
FERC License Number(s): N/A  
PG&E Drawing Number: SJL-15406  
PLAT NO.: D-10-09, D-10-10  
LD of any affected documents: 2305-03-1503  
LD of any Cross-referenced documents: 2305-03-1881  
TYPE OF INTEREST: 4, 6, 43  
SBE Parcel Number: N/A  
(For Quitclaims, % being quitclaimed): N/A  
Order # or PM #: 31157047-0070  
JCN: N/A  
County: San Mateo  
Utility Notice Numbers: N/A  
851 Approval Application No. N/A Decision N/A  
Prepared By: kv2  
Checked By: dan9 D.N.

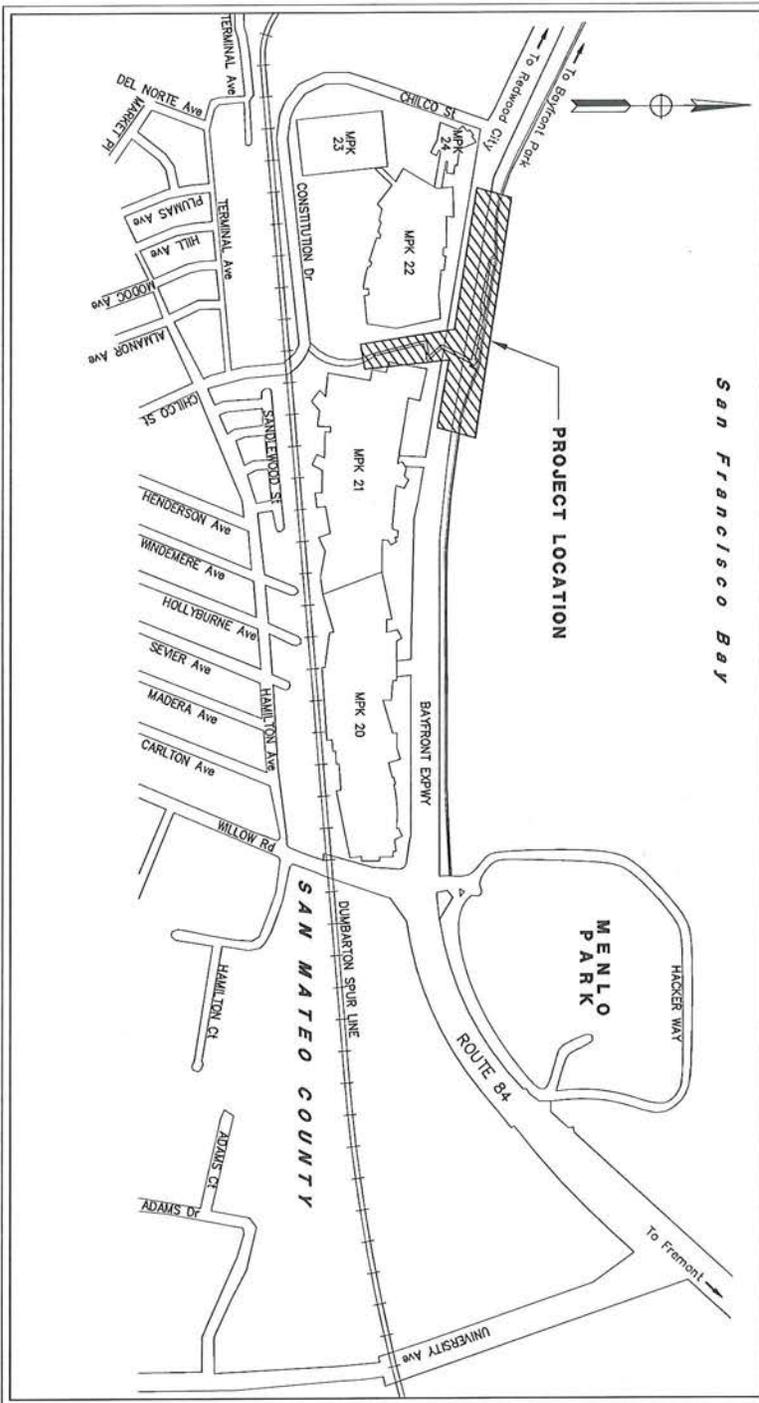
## **Attachment 7**

**Location Map with Bridge Layout**

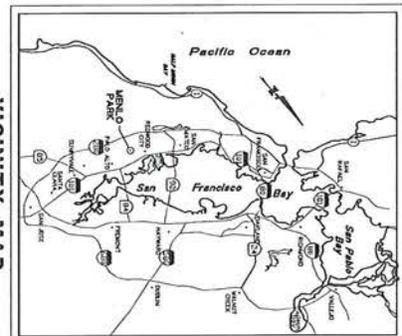
**EXHIBIT "C"**

**CITY OF MENLO PARK  
IN SAN MATEO COUNTY  
BAYFRONT POC (BRIDGE No. 35-0358)  
STATE ROUTE 84/ BAYFRONT EXPRESSWAY**

TO BE SUPPLEMENTED BY CALTRANS STANDARD PLANS & SPECIFICATIONS 2015



**LOCATION MAP**  
NOT TO SCALE



**VICINITY MAP**  
NOT TO SCALE

Revisions	No.	Date	By
		8/10/17	

**SR 84/BAYFRONT EXPRESSWAY  
BAYFRONT POC (BRIDGE No.35-0358)  
TITLE SHEET**

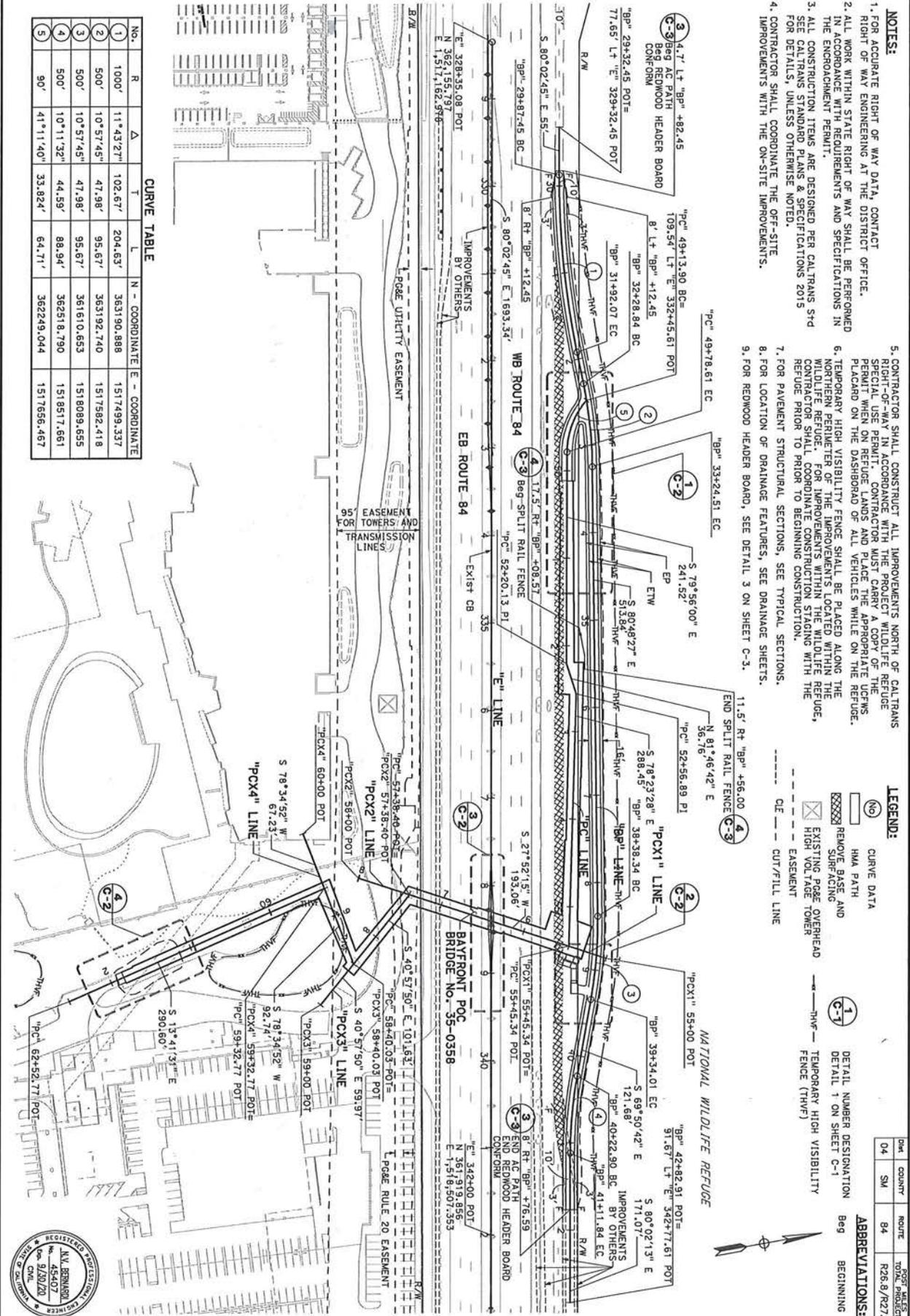
MENLO PARK SAN MATEO COUNTY CALIFORNIA

**BKF100** YEARS  
ENGINEERS . SURVEYORS . PLANNERS

BKF ENGINEERS  
4670 WILLOW ROAD  
SUITE 250  
PLEASANTON, CA 94588  
925-396-7700  
925-396-7799 (FAX)

NO.	R	Δ	T	L	N - COORDINATE	E - COORDINATE
1	1000'	11°43'27"	102.67'	204.63'	363190.888	1517499.337
2	500'	10°57'45"	47.98'	95.67'	363192.740	1517582.418
3	500'	10°57'45"	47.98'	95.67'	363161.053	1518089.655
4	500'	10°11'32"	44.59'	89.94'	362518.790	1518817.661
5	90'	41°11'40"	33.824'	64.71'	362249.044	1517656.467

CURVE TABLE



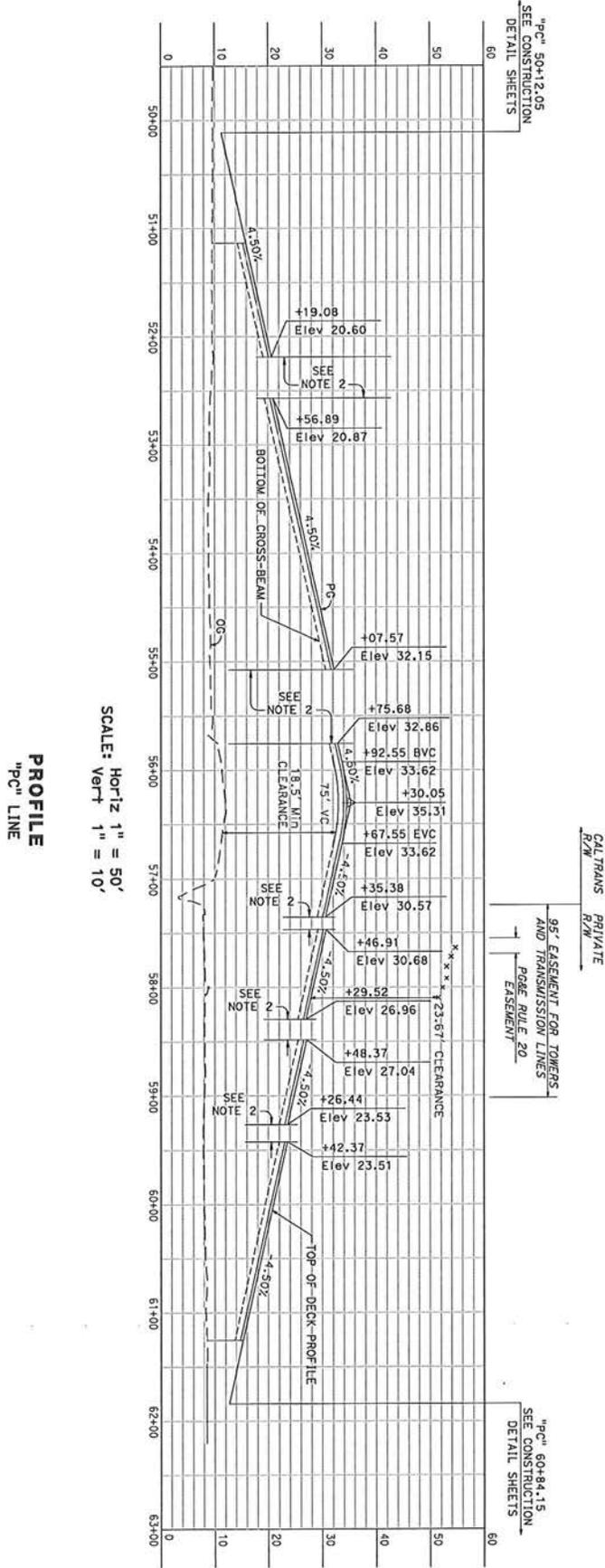
- NOTES:**
1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT-OF-WAY ENGINEERING AT THE DISTRICT OFFICE.
  2. ALL WORK WITHIN STATE RIGHT OF WAY SHALL BE PERFORMED IN ACCORDANCE WITH REQUIREMENTS AND SPECIFICATIONS IN THE ENCROACHMENT PERMIT.
  3. ALL CONSTRUCTION ITEMS ARE DESIGNED PER CALTRANS STD IN ACCORDANCE WITH REQUIREMENTS AND SPECIFICATIONS 2015 FOR DETAILS, UNLESS OTHERWISE NOTED.
  4. CONTRACTOR SHALL COORDINATE THE OFF-SITE IMPROVEMENTS WITH THE ON-SITE IMPROVEMENTS.
  5. CONTRACTOR SHALL CONSTRUCT ALL IMPROVEMENTS NORTH OF CALTRANS RIGHT-OF-WAY IN ACCORDANCE WITH THE PROJECT WILDLIFE REFUGE SPECIAL USE PERMIT. CONTRACTOR MUST CARRY A COPY OF THE PERMIT WHEN ON REFUGE LANDS AND PLACE THE APPROPRIATE UCHS PLACARD ON THE DISBOARD OF ALL VEHICLES WHILE ON THE REFUGE.
  6. TEMPORARY HIGH VISIBILITY FENCE SHALL BE PLACED ALONG THE NORTHERN PERIMETER OF THE IMPROVEMENTS LOCATED WITHIN THE WILDLIFE REFUGE. FOR IMPROVEMENTS WITHIN THE WILDLIFE REFUGE, CONTRACTOR SHALL FURNISH CONSTRUCTION STAKES WITH REFUGE PRIOR TO BEGINNING CONSTRUCTION.
  7. FOR PAVEMENT STRUCTURAL SECTIONS, SEE TYPICAL SECTIONS.
  8. FOR LOCATION OF DRAINAGE FEATURES, SEE DRAINAGE SHEETS.
  9. FOR REMOVAL HEADER BOARD, SEE DETAIL 3 ON SHEET C-3.

- LEGEND:**
- (No) CURVE DATA
  - (HMA) HMA PATH
  - (R) REMOVE BASE AND SURFACING
  - (E) EXISTING POE OVERHEAD
  - (H) HIGH VOLTAGE TOWER
  - (C) CUT/FILL LINE
  - (1) DETAIL NUMBER DESIGNATION
  - (C-1) DETAIL 1 ON SHEET C-1
- ABBREVIATIONS:**
- 869 BEGINNING



Date	By	Revisions
10-20-18		
11-14-18		
12-11-18		
01-23-19		
02-20-19		
03-14-19		
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04-08-47		</

- NOTES:**
1. ELEVATIONS SHOWN ARE BASED UPTON NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) IN FEET.
  2. FOR DECK CONTOUR GRADING AND LANDING ELEVATIONS, SEE STRUCTURAL SHEETS.
  3. PROFILE GRADE REFLECTS TOP OF DECK ELEVATION.



SCALE: Horiz 1" = 50'  
Vert 1" = 10'

PROFILE  
"PC" LINE

CALTRANS PRIVATE  
R/W  
95' EASEMENT FOR TOWERS AND TRANSMISSION LINES  
PAGE RULE 20  
EASEMENT

POST MILES	ROUTE	COUNTY	BLK
R26.8/R27.1 <td>84 <td>SM <td>04</td> </td></td>	84 <td>SM <td>04</td> </td>	SM <td>04</td>	04



No.	Date	Revisions
1	10-28-18	
2		
3		
4		
5		

**SR 84/BAYFRONT EXPRESSWAY  
BAYFRONT POC (BRIDGE No.35-0358)  
PROFILE**  
MENLO PARK SAN MATEO COUNTY CALIFORNIA

**BKF 100 YEARS**  
ENGINEERS . SURVEYORS . PLANNERS

BKF ENGINEERS  
14670 WILLOW ROAD  
SUITE 250  
PLEASANTON, CA 94588  
925-396-7700  
925-396-7799 (FAX)

# INDEX TO BRIDGE PLANS

SHEET	TITLE	SHEET	TITLE
1	GENERAL PLAN	41	GIRDER DETAILS No. 1
2	STRUCTURE PLAN No. 1	42	GIRDER DETAILS No. 2
3	STRUCTURE PLAN No. 2	43	GIRDER DETAILS No. 3
4	STRUCTURE PLAN No. 3	44	GIRDER DETAILS No. 4
5	INDEX TO PLANS	45	GIRDER DETAILS No. 5
6	DECK CONTOURS No. 1	46	GIRDER DETAILS No. 6
7	DECK CONTOURS No. 2	47	GIRDER SPLICE DETAILS No. 1
8	CAMBER DIAGRAM No. 1	48	GIRDER SPLICE DETAILS No. 2
9	CAMBER DIAGRAM No. 2	49	GIRDER SPLICE DETAILS No. 3
10	CAMBER DIAGRAM No. 3	50	GIRDER SPLICE DETAILS No. 4
11	FOUNDATION PLAN	51	HINGE DETAILS
12	FOUNDATION DATA	52	BEARING DETAILS No. 1
13	ABUTMENT LAYOUT	53	BEARING DETAILS No. 2
14	ABUTMENT DETAILS No. 1	54	BEARING DETAILS No. 3
15	ABUTMENT DETAILS No. 2	55	DECK DETAILS No. 1
16	ABUTMENT DETAILS No. 3	56	DECK DETAILS No. 2
17	BENT 2 LAYOUT & DETAILS	57	DECK JOINT DETAILS No. 1
18	BENT 3 LAYOUT & DETAILS	58	DECK JOINT DETAILS No. 2
19	BENTS 4, 5, 8, 9, 10 & 11 LAYOUT	59	CHAIN LINK RAILING POST ANCHORAGE DETAILS
20	BENT 6 LAYOUT	60	PROPOSED CONSTRUCTION SEQUENCE No. 1 (MOD)
21	BENT 7 LAYOUT & DETAILS	61	PROPOSED CONSTRUCTION SEQUENCE No. 2
22	BENT DETAILS No. 1	62	PROPOSED CONSTRUCTION SEQUENCE No. 3
23	BENT DETAILS No. 2	63	PROPOSED CONSTRUCTION SEQUENCE No. 4
24	BENT DETAILS No. 3	64	PROPOSED CONSTRUCTION SEQUENCE No. 5
25	BENT DETAILS No. 4	65	PROPOSED CONSTRUCTION SEQUENCE No. 6
26	BENT DETAILS No. 5	66	STEEL GIRDER SHOP SPLICE DETAILS
27	MISCELLANEOUS BENT DETAILS No. 1	67	STEEL GIRDER TRANSVERSE STIFFENER DETAILS
28	MISCELLANEOUS BENT DETAILS No. 2	68	STEEL GIRDER CONNECTION STIFFENER DETAILS
29	TYPICAL SECTION	69	LOG OF TEST BORINGS 1 OF 2
30	GIRDER FRAMING PLAN No. 1	70	LOG OF TEST BORINGS 2 OF 2
31	GIRDER FRAMING PLAN No. 2		
32	GIRDER FRAMING PLAN No. 3		
33	BENT CAP FRAMING LAYOUT No. 1		
34	BENT CAP FRAMING LAYOUT No. 2		
35	GIRDER ELEVATION No. 1		
36	GIRDER ELEVATION No. 2		
37	GIRDER ELEVATION No. 3		
38	FCM DESIGNATIONS No. 1		
39	FCM DESIGNATIONS No. 2		
40	FCM DESIGNATIONS No. 3		

# GENERAL NOTES

## LOAD & RESISTANCE FACTOR DESIGN

For project specific design criteria, see "Bayfront Pedestrian Overcrossing Structural Design Criteria Memorandum" dated June 09, 2017.

DESIGN: AASHTO LRFD Bridge Design Specifications, 6th Edition, supplemented by LRFD Guide Specifications for the Design of Pedestrian Bridges dated December 2009, and California Amendments, preface dated January 2014

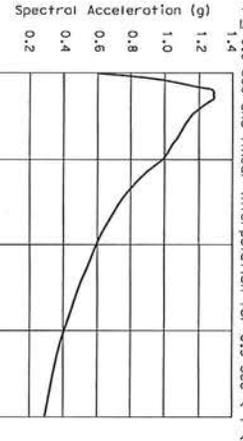
SEISMIC DESIGN: California Seismic Design Criteria (SDC) Version 1.7 supplemented by the California Seismic Design of Steel Bridges, dated May 2015

DEAD LOAD: 150 pcf for reinforced concrete  
400 pcf for composite metal deck with normal weight concrete  
60 pcf for composite metal deck with normal weight concrete  
35 pcf for railing (each side of structure)  
125 pcf for utilities

WIND LOAD & FATIGUE LOAD: AASHTO Standard Specification for Structural Supports for Highway Signs, Luminaires, and Traffic Signals

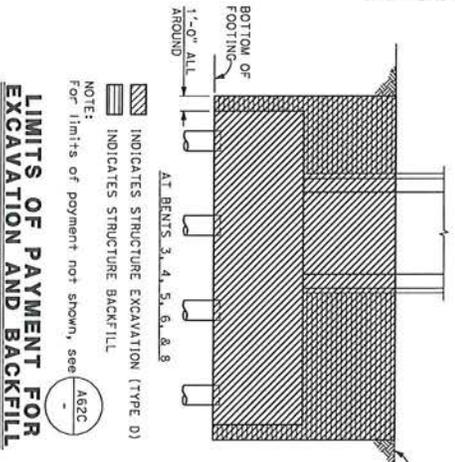
LIVE LOAD: 90 psf pedestrian load

SEISMIC LOAD: Soil profile: Medium  
Peak ground acceleration = 0.1598g  
20% increase for T > 1.0 sec, no modification for T < 0.5 sec and linear interpolation for 0.5 sec < T < 1.0 sec



fy = 60 ksi  
fc = 4.0 ksi  
Lengths for epoxy coated bar lap splices not shown on plans must be 1.5 times the length required in California Standard Specifications.  
Built-up girder flanges and webs, splices.  
Welded flanges and stiffener plates  
Welded flanges and stiffener plates  
Hollow Structural Steel Shapes  
Structural Steel Pipes  
All other structural steel  
Anchor Rods and Anchor Bolts  
High Strength Bolts  
High Strength Anchor Bolts  
High Strength Restrainer Rods  
Nuts  
Washers for Carbon Steel Bolts  
Washers for HS fasteners  
Gosner connector studs  
Pins  
ASTM A53 Structural Steel (SS)  
Grade 40 minimum C163 Galvanized Coating  
Minimum Section Modulus

## LIMITS OF PAYMENT FOR EXCAVATION AND BACKFILL



## 2015 STANDARD PLANS

LEGEND:  
 B1-11 Indicates Standard Plan sheet No.  
 U42 Indicates Detail No.

ABBREVIATIONS:  
 FCM Feature Critical Member  
 GB Grade Break  
 HSB High Strength Bolt(s)  
 WP Work Point  
 WWF Welded Wire Fabric  
 WWM Welded Wire Mesh

SECTION REVISION	SECTION	DATE	BY	CHKD	APP'D
	19" P. PARK				
	18" P. PARK				
	B. AVON				
SECTION REVISION	SECTION	DATE	BY	CHKD	APP'D
	CASTILLO				
	CASTILLO				
	SMARTCLOCK				
SECTION REVISION	SECTION	DATE	BY	CHKD	APP'D
	CASTILLO				
	CASTILLO				
	SMARTCLOCK				

PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION

PROJECT NUMBER & PHASE: CONTRACT NO.: 2K590

DESIGNED BY: JES S. LI

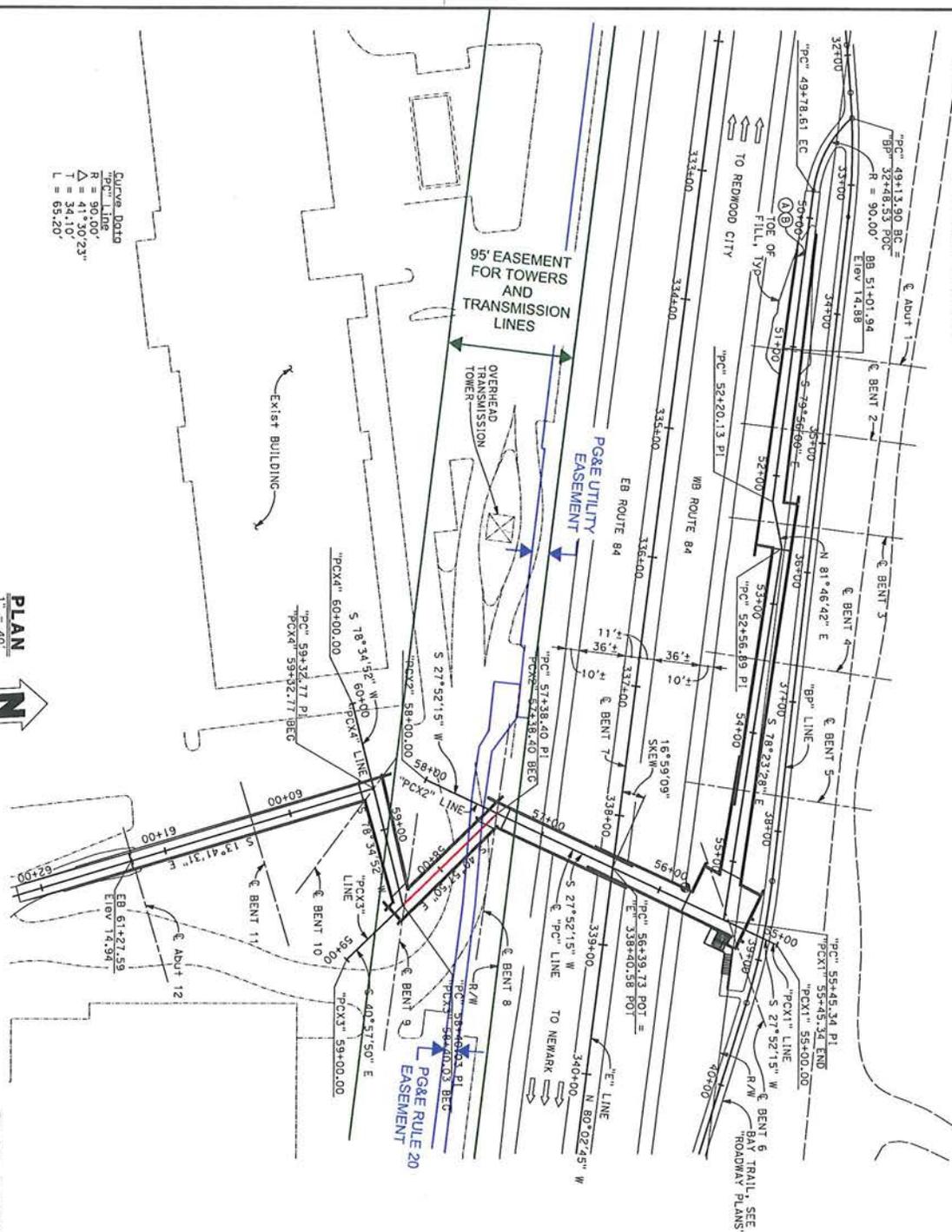
CHECKED BY: R7.0

DATE PLOTTED: 8/10/17

USERNAME: BUSER

FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT  
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE

BCA PROGRESS PLANS 2017-09-07



PLAN  
N  
1" = 40'

PLAN CHECK SET/NOT FOR CONSTRUCTION (8/10/17)

DESIGN REVISION	NO.	DATE	BY	REASON
DESIGN	1		P. PARK	
DETAILS	2		P. PARK	
QUANTITIES	3		B. AYON	

DESIGNER	ENGINEER	CHECKER	DATE
P. PARK	F. CASTILLO	F. CASTILLO	8/10/17

PROJECT NUMBER & PHASE	CONTRACT NO.
INES S. LI	21590

REGISTERED CIVIL ENGINEER	DATE
INES S. LI	8/10/17

PLANS APPROVAL DATE	NO.	DATE
	51551	8/10/17

REGISTERED CIVIL ENGINEER	DATE
INES S. LI	8/10/17

REGISTERED CIVIL ENGINEER	DATE
INES S. LI	8/10/17

REGISTERED CIVIL ENGINEER	DATE
INES S. LI	8/10/17

**NOTES:**

1. Point "BAYFRONT POC"
2. Point "INDEX TO BRIDGE PLANS" and "GENERAL NOTES", see "INDEX TO PLANS" sheet
3. For "PILE DATA TABLE" see "FOUNDATION DATA" sheet
4. For "BENCH MARK AND DATUM", see "FOUNDATION PLAN" sheet.
5. For lighting information, see "ELECTRICAL PLANS".
6. For bridge railing limits and details, see "ARCHITECTURAL PLANS".
7. For utilities information, see "ROADWAY PLANS".

**LEGEND:**

- Indicates Point of Minimum Vertical Clearance
- Indicates Existing Structure
- ⇨ Indicates Traffic Direction

**REGISTERED CIVIL ENGINEER**  
INES S. LI  
No. 51551  
Exp. 8/30/18  
COMPLETION OF ANNUAL REPORT OF CIVIL ENGINEER  
SIN 0055, CALIFORNIA 95178

**REGISTERED CIVIL ENGINEER**  
INES S. LI  
No. 51551  
Exp. 8/30/18  
COMPLETION OF ANNUAL REPORT OF CIVIL ENGINEER  
SIN 0055, CALIFORNIA 95178

PREPARED FOR THE  
**STATE OF CALIFORNIA**  
DEPARTMENT OF TRANSPORTATION

**BAYFRONT POC**  
**GENERAL PLAN**

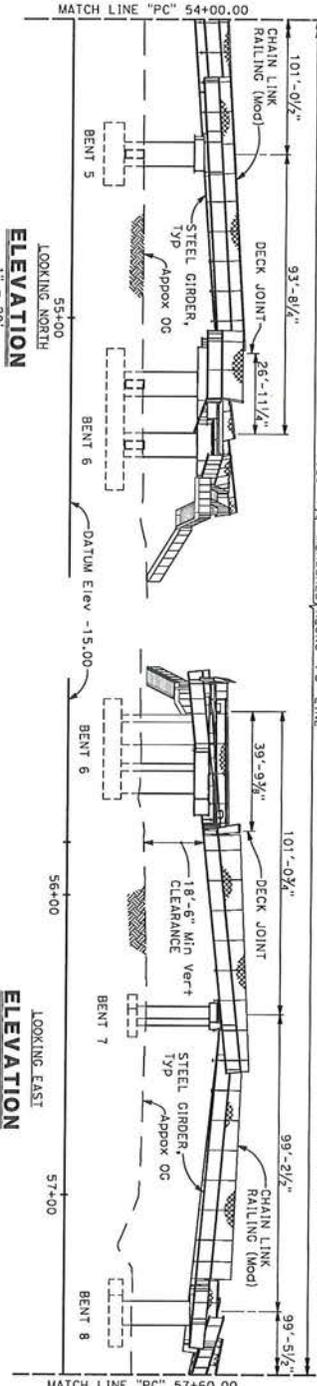
PROJECT NUMBER & PHASE: INES S. LI  
CONTRACT NO.: 21590

REGISTERED CIVIL ENGINEER: INES S. LI  
DATE: 8/10/17



FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT  
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE

**PROFILE GRADE**  
 NO SCALE



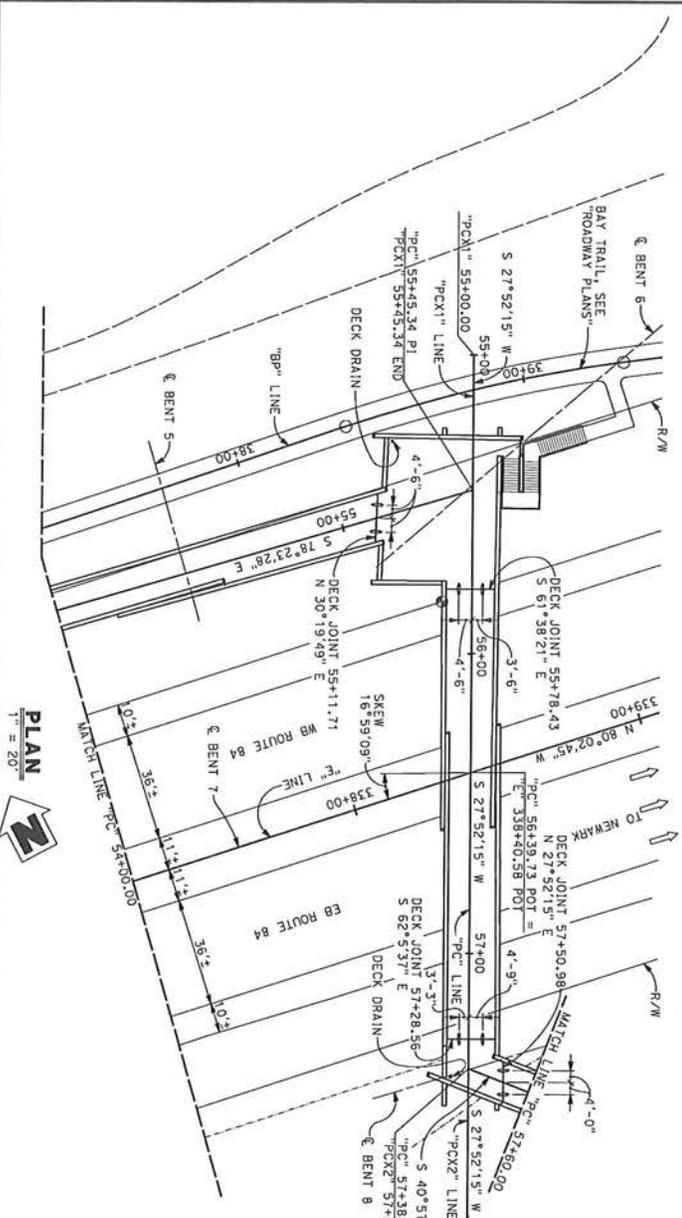
LOOKING NORTH  
 ELEVATION  
 1" = 20'

LOOKING EAST  
 ELEVATION  
 1" = 20'

CB 55+07.57 Elev 32.15	SEE DECK CONTOURS SHEETS	CB 55+75.68 Elev 32.88	BVC 55+82.55 Elev 33.62	VC 56+75.00 Elev 33.62	EVC 56+67.55 Elev 33.62	CB 57+46.91 Elev 30.68
1025'-7 1/2" MEASURED ALONG "PC" LINE						

**PROFILE GRADE**  
 NO SCALE

CB 57+35.38 Elev 30.57	SEE DECK CONTOURS SHEETS	CB 57+46.91 Elev 30.68
---------------------------	--------------------------	---------------------------



**PLAN**  
 1" = 20'

- NOTES:
1. For girder layout information, see "GIRDER FRAMING PLANS" sheet.
  2. For girder and beam sizes, see "BENT CAP FRAMING LAYOUT" sheets and GIRDER ELEVATION sheets.
  3. For deck drain locations, see "BENT CAP FRAMING LAYOUT" sheets.
  4. For deck joint shear lug details, see "DECK JOINT DETAILS" No. 2 sheet.

- LEGEND:
- Indicates Deck Joint Shear Lug
  - Indicates Deck Drain. See "BENT CAP FRAMING PLANS" sheets for exact locations and details.
  - Indicates Point of Minimum Vertical Clearance

SECTION	SECTION DATE	DESIGNER	CHECKER	DATE
SECTION	SECTION DATE	DESIGNER	CHECKER	DATE
SECTION	SECTION DATE	DESIGNER	CHECKER	DATE
SECTION	SECTION DATE	DESIGNER	CHECKER	DATE

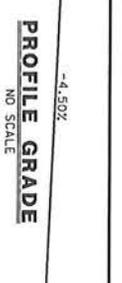
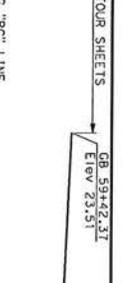
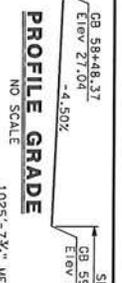
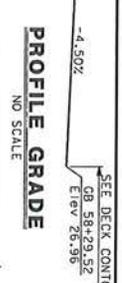
PREPARED FOR THE  
**STATE OF CALIFORNIA**  
 DEPARTMENT OF TRANSPORTATION

PROJECT NUMBER & PHASE:  
**STRUCTURE PLAN No. 2**

REGISTERED CIVIL ENGINEER  
 DATE

PLANS APPROVAL DATE  
 No. 031561  
 EXP. 6/30/18  
 REGISTERED PROFESSIONAL ENGINEER  
 STATE OF CALIFORNIA

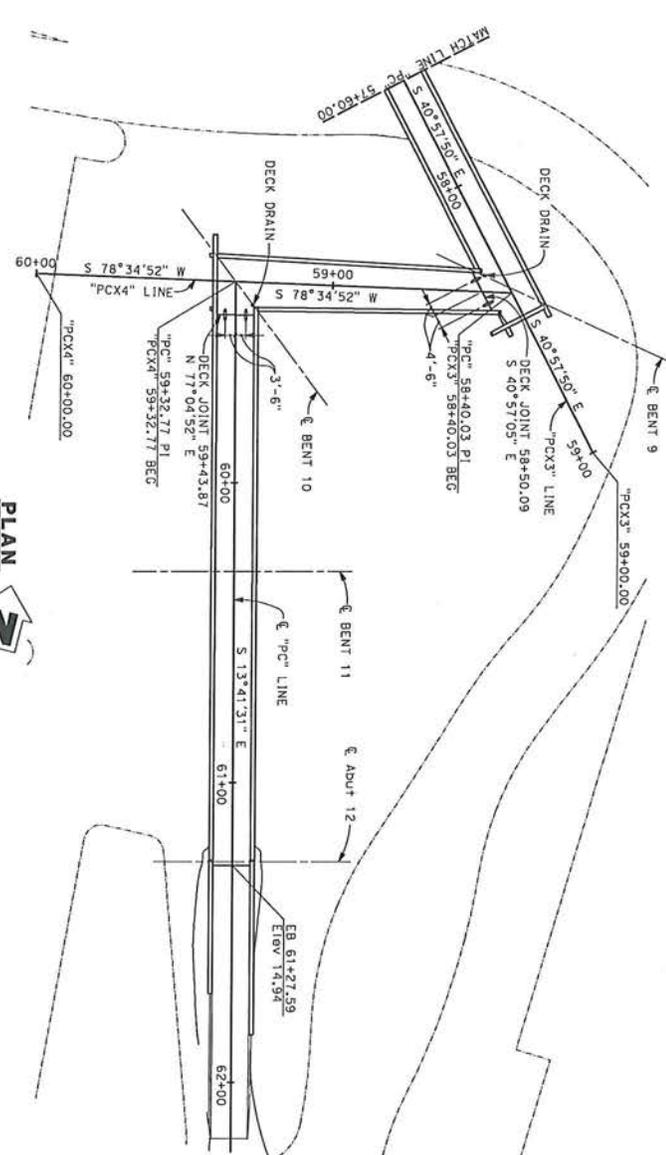
BLISS CARDOSA ASSOCIATES INC.  
 845 THE ALAMOND DRIVE  
 SAN JOSE, CALIFORNIA 95128



LOOKING NORTH EAST  
ELEVATION  
1" = 20'

LOOKING SOUTH  
ELEVATION  
1" = 20'

LOOKING EAST  
ELEVATION  
1" = 20'



PLAN  
1" = 20'

- NOTES:
1. For girder layout information, see "GIRDER FRAMING PLANS" sheet.
  2. For girder and beam sizes, see "BENT CAP FRAMING LAYOUT" sheets and "GIRDER ELEVATION" sheets.
  3. For deck drain locations, see "BENT CAP FRAMING LAYOUT" sheets.
  4. For deck joint shear lug detail, see "DECK JOINT DETAILS No. 2" sheet.

LEGEND:

- Indicates Deck Joint Shear Lug
- Indicates Deck Drain. See "BENT CAP FRAMING PLANS" sheets for exact locations and details.

ACTION OVERSIGHT		REGION		SECTION		PREPARED FOR THE		DATE		PROJECT NO.		CONTRACT NO.		SHEET NO.	
DESIGN DETAIL SHEET (ENGLISH) (REV. 03/11/12)		S.F. PARK		CASTILLO		STATE OF CALIFORNIA		35-0358		R27.0		21590		4	
		S.F. PARK		CASTILLO		DEPARTMENT OF TRANSPORTATION		INES S. LI		R27.0		21590		71	
		B. AVON		SMARTCHICK		DEPARTMENT OF TRANSPORTATION		PROJECT ENGINEER		R27.0		21590		71	
						DEPARTMENT OF TRANSPORTATION		PROJECT ENGINEER		R27.0		21590		71	
						DEPARTMENT OF TRANSPORTATION		PROJECT ENGINEER		R27.0		21590		71	

PLAN CHECK SET/NOT FOR CONSTRUCTION (8/10/17)

**BAYFRONT POC**

**STRUCTURE PLAN No. 3**

PLANS APPROVAL DATE

The State of California or its officers or agents shall not be responsible for the accuracy or completeness of these plans or this plan sheet.

BIGGS CARDOZA ASSOCIATES, INC.  
885 THE ALAMEDA  
SAN JOSE, CALIFORNIA 95128

REGISTERED CIVIL ENGINEER DATE

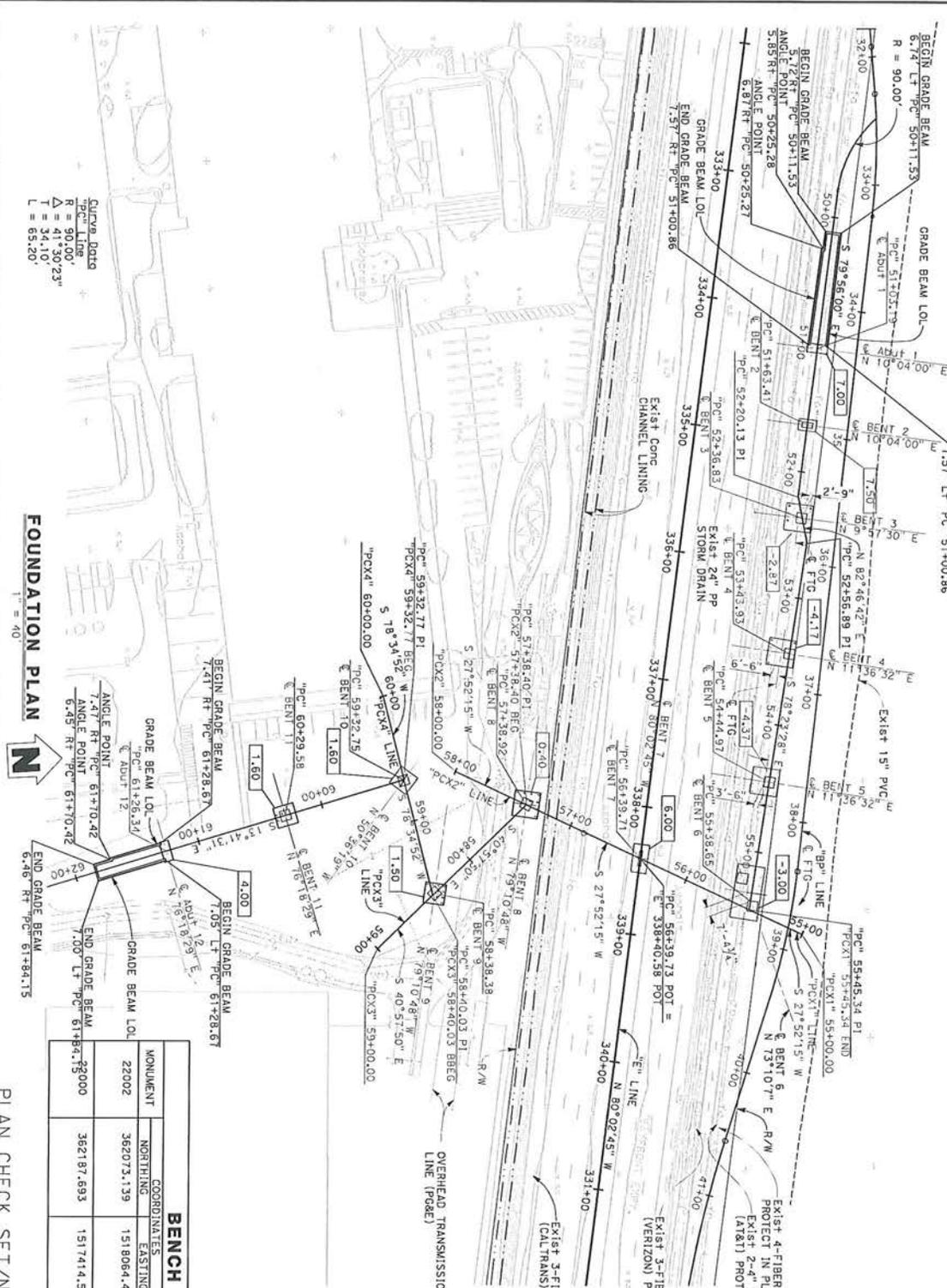
REGISTERED PROFESSIONAL ENGINEER No. 55156 Exp. 12/30/18

0151	COUNTY	ROUTE	POST MILES	SHEET TOTALS
04	SM	84	R26.8/R27.1	4/71





FOR ACCURATE RIGHT OF WAY AND ACCESS DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE



**FOUNDATION PLAN**

SCALE: 1"=40' (VERTICAL DIMENSIONS USE NOTE 8) HORIZONTAL DIMENSIONS USE NOTE 9

DESIGNED BY: F. CASTILLO  
 CHECKED BY: B. MARCHIONDI  
 DATE: 9/6/17

**FOUNDATION PLAN SHEET (EACH) (REV. 03-14-17)**

PROJECT: STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION  
 SHEET NO. 11 OF 71

**BENCH MARK AND DATUM**

MONUMENT	COORDINATES	ELEVATION	DESCRIPTION/LOCATION
	NORTHING	EASTING	
22002	362073.139	1518064.419	9.24 MAG NAIL SET IN ASPHALT PATH ON NORTHERLY SIDE OF BAYFRONT EXPRESSWAY
362187.693	362187.693	1517414.588	9.14 MAG NAIL SET IN ASPHALT PATH ON NORTHERLY SIDE OF BAYFRONT EXPRESSWAY

**PLAN CHECK SET/NOT FOR CONSTRUCTION (9/6/17)**

PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION  
 BY: INES S. LI  
 PROJECT ENGINEER

DATE: 9/6/17

CONTRACT NO.: 2K590

PROJECT NUMBER & PHASE: 111

FILE NO.: 2K590-111

DATE PLOTTED: 9/20/17

TIME PLOTTED: 11:11 AM

**NOTES:**

1. NAVD88 elevations are derived from the City of San Jose Vertical Control with a datum shift of 0.100 feet above the NAVD83 published elevation.
2. Not all piles shown for clarity.
3. For the offset dimensions of footings or bents 8, 9 & 10, see "FOUNDATION DATA" sheet.
4. For pile layout of abutments and bents, see "ABUTMENT LAYOUT" and "BENT LAYOUT" sheets.
5. Install at least one indicator pile at Bent 6 and one at Bent 8. Indicator pile must be at least 5 feet longer than that required per plans. Indicator piles may be used as production piles.
6. Verify utility locations with "Roadway Plans".

**LEGEND:**

- 5.00' Indicates Bottom of Footing Elevation
- Indicates Spot Elevation
- Indicates Vertical Pile

**EXIST 3-FIBER OPTICS (VERIZON) PROTECT IN PLACE**

**EXIST 3-FIBER OPTICS (CALTRANS) PROTECT IN PLACE**

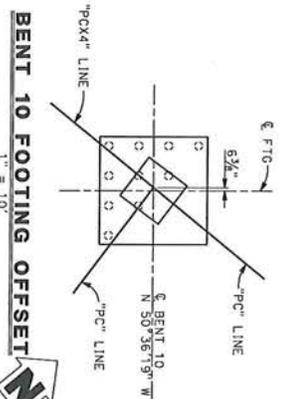
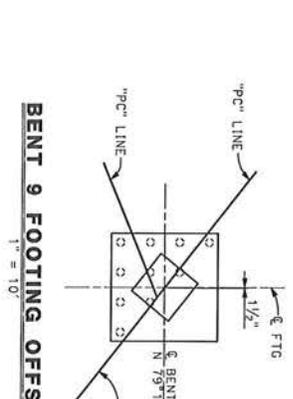
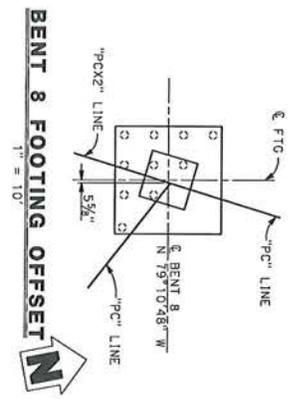
**EXIST 4-FIBER OPTICS (XO) PROTECT IN PLACE**

**EXIST 2-4 CPC DUCTS (AT&T) PROTECT IN PLACE**

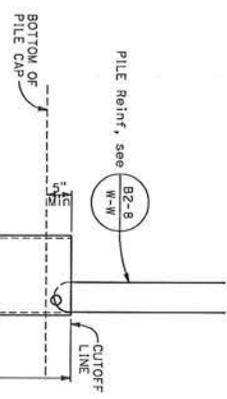
**EXIST 15\"/>**

LOCATION	PILE TYPE	NOMINAL RESISTANCE		DESIGN TIP Elev (+ft)	SPECIFIED TIP Elev (+ft)	REQUIRED NOMINAL DRIVING RESISTANCE (kips)
		COMPRESSION	TENSION			
ABUTMENT 1	CLASS 200 AIT "W"	230	0	-18.0(d)	-	-
BENT 2	CLASS 200 AIT "W"	150	50	-28.0(d)	-	-
BENT 3	CLASS 200 AIT "W"	380	200	-33.0(d)	-	-
BENT 4	CLASS 200 AIT "W"	380	150	-34.0(d)	-	-
BENT 5	CLASS 200 AIT "W"	250	60	-34.0(d)	-	-
BENT 6	CLASS 200 AIT "W"	390	180	-38.0(d)	-	-
BENT 7	CLASS 200 AIT "W"	310	220	-29.0(d)	-	-
BENT 8	CISS 16 x 0.50	240	180	-35.0(d)	-	-
BENT 9	CISS 16 x 0.50	250	190	-34.0(d)	-	-
BENT 10	CISS 16 x 0.50	220	150	-38.0(d)	-	-
BENT 11	CISS 16 x 0.50	220	160	-38.0(d)	-	-
ABUTMENT 12	CISS 16 x 0.50	230	0	-20.0(d)	-	-

NOTES:  
 1. Design tip elevations for Abutments and Bents are controlled by: (a) Compression, (b) Tension, (c) Settlement, (d) Lateral load.  
 2. Pile locations that conflict with abandoned utilities must be pre-identified to 6" below utilities' invert prior to pile driving.  
 3. The specified tip elevation shall not be raised above the design tip elevation for tension load, lateral load, & tolerable settlement.



- NOTES:  
 1. See B2-8 for "DESIGN NOTES" and details not noted.  
 2. Pile splice may be shop welded or field welded.  
 3. Concrete fill: f'c = 4000 psi



REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

NO. 031561  
 EXPIRES 6/30/18

REGISTERED PROFESSIONAL ENGINEER

BILOS CASADOSA ASSOCIATES, INC.  
 855 THE ALAMDA  
 SAN JOSE, CALIFORNIA 95128

GEOTECHNICAL PROFESSIONAL APPROVAL DATE

SCALE:	IDENTIFICATION:	HORIZONTAL:	SECTION:	DATE:
PROJ: BAYFRONT POC	NO. 031561	1" = 10'	1" = 10'	8/10/17
DESIGNER: J. CASTILLO	CHECKED: B. SMARTCHICK	DATE: 8/10/17	SCALE: 1" = 10'	PROJECT NO.: 26590
DESIGNED BY: J. CASTILLO	DESIGNED BY: B. SMARTCHICK	DATE: 8/10/17	SCALE: 1" = 10'	CONTRACT NO.: 26590
DESIGNED BY: J. CASTILLO	DESIGNED BY: B. SMARTCHICK	DATE: 8/10/17	SCALE: 1" = 10'	CONTRACT NO.: 26590

PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION

PROJECT NUMBER & PHASE: CONTRACT NO.: 26590

FOUNDATION DATA

FOUNDATION PLAN SHEET (REVISED) (REV. 03-14-17)

DATE PLOTTED => #DATE TIME PLOTTED => #TIME



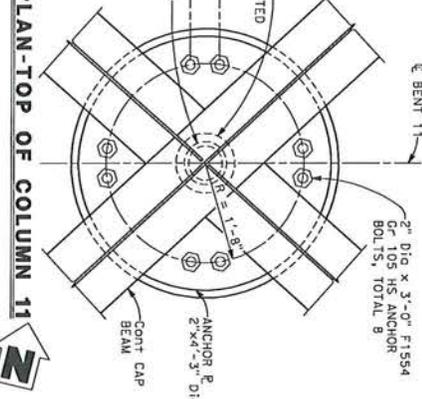
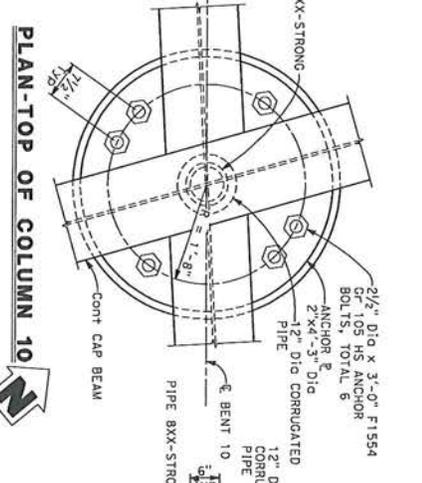
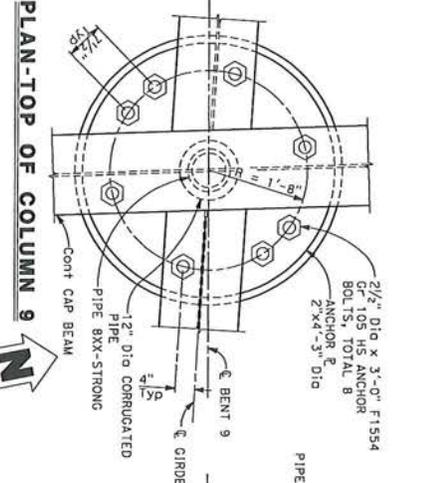
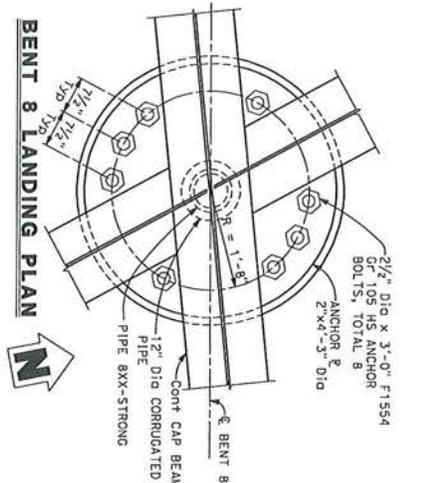






NOTE:  
For details not shown, see 'BENT DETAILS No. 2' sheet.

COUNTY	ROUTE	POST MILES	SHEET TOTAL
04	84	R26.8/27.1	NS SHEETS
REGISTERED CIVIL ENGINEER		DATE	REGISTERED PROFESSIONAL ENGINEER
PLANS APPROVAL DATE		NO. C51554	Exp. 6/20/18
<p>The State of California or its officers or agents shall not be responsible for the accuracy or completeness of these plans shown.</p>			
<p>REGISTERED PROFESSIONAL ENGINEER          CIVIL          BICOS CARDOZA ASSOCIATES INC.          855 THE ALAMEDA          SAN JOSE, CALIFORNIA 95128</p>			



1" = 1'-0"

PLAN CHECK SET/NOT FOR CONSTRUCTION (8/10/17)

ACTION OPERATOR		DESIGN		PREPARED FOR THE		PROJECT NUMBER & PHASE:	
DESIGNER		CHECKED		STATE OF CALIFORNIA		CONTRACT NO.: 2K590	
DATE		DATE		DEPARTMENT OF TRANSPORTATION		CONTRACT NO.: 2K590	
SECTION DETAIL SHEET (ENGLISH) (REV. 02/14/12)		SECTION DETAIL SHEET (ENGLISH) (REV. 02/14/12)		BENT DETAILS No. 4		BENT DETAILS No. 4	
SECTION DETAIL SHEET (ENGLISH) (REV. 02/14/12)		SECTION DETAIL SHEET (ENGLISH) (REV. 02/14/12)		BENT DETAILS No. 4		BENT DETAILS No. 4	



DIST COUNTY ROUTE 8653 MILES TOTAL PROJECT SHEET TOTAL

04 SM 84 R26.8/R27.1 28 SHEETS

REGISTERED CIVIL ENGINEER DATE

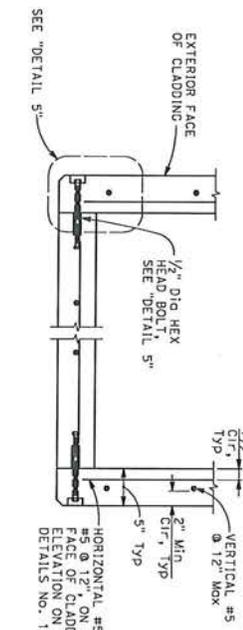
PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER

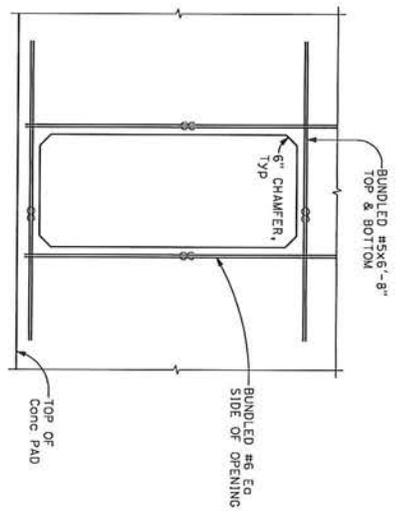
NO. 031561  
EXP. 6/30/18  
STATE OF CALIFORNIA

THE STATE OF CALIFORNIA or its officers or agents shall not be responsible for the accuracy or completeness of assumed copies of this plan set.

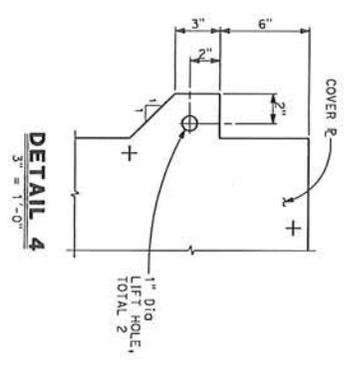
BIGOS CARDOSA ASSOCIATES, INC.  
845 THE ALAMITOS  
SAN JOSE, CALIFORNIA 95128



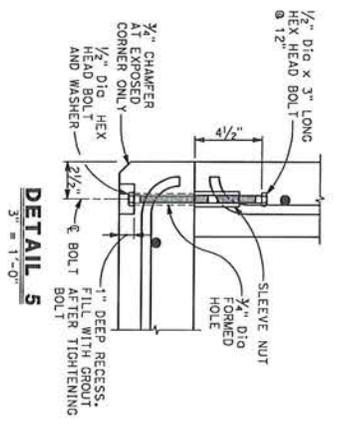
**DETAIL 2**  
1 1/2" = 1'-0"



**DETAIL 3**  
3/4" = 1'-0"



**DETAIL 4**  
3" = 1'-0"

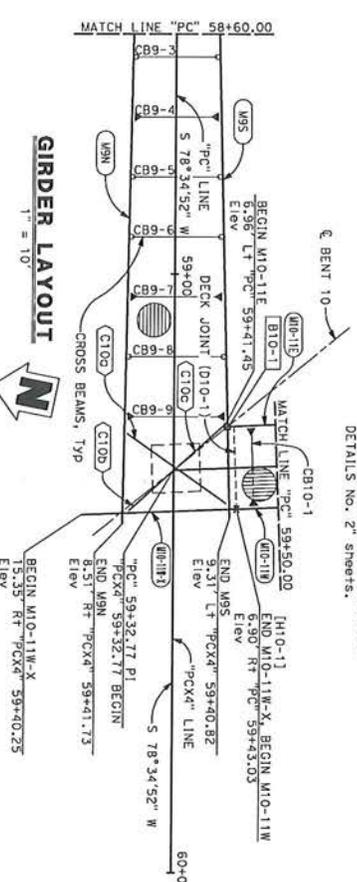
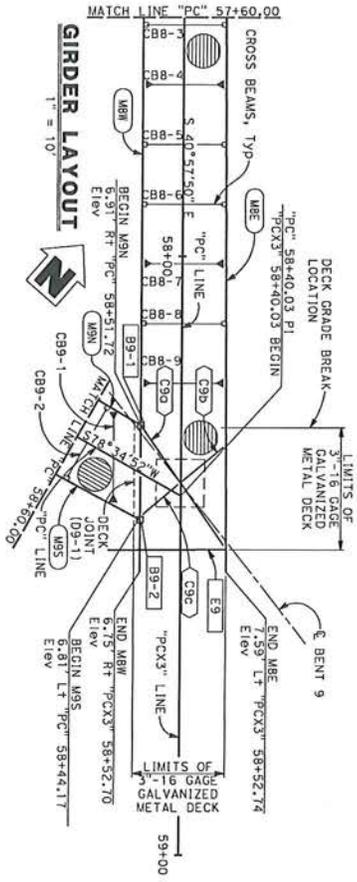
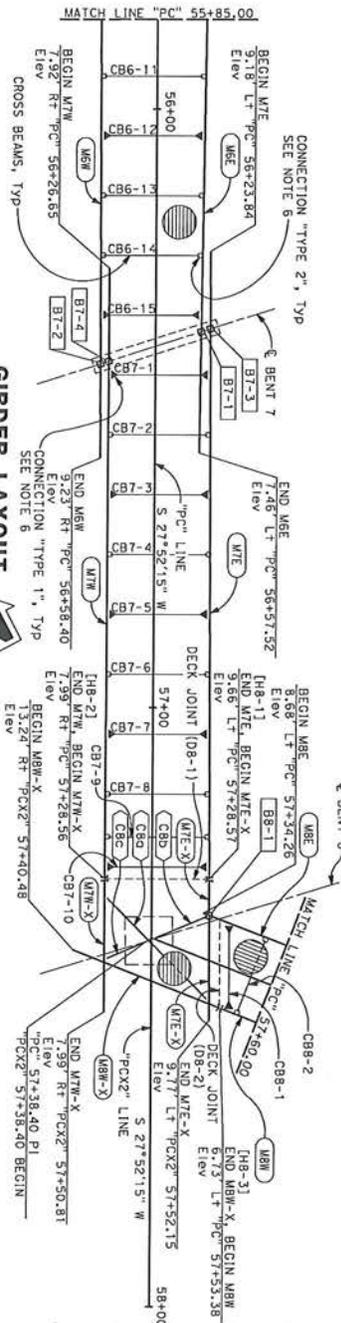


**DETAIL 5**  
3" = 1'-0"

PLAN CHECK SET/NOT FOR CONSTRUCTION (8/10/17)

SECTION	10" P. PARK						
DETAILS	10" P. PARK						
QUANTITIES	B. AVON						
SECTION	CASTILLO	SECTION	CASTILLO	SECTION	CASTILLO	SECTION	CASTILLO
DETAILS	CASTILLO	DETAILS	CASTILLO	DETAILS	CASTILLO	DETAILS	CASTILLO
QUANTITIES	B. SMARTCHICK						
SECTION	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	SECTION	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	SECTION	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	SECTION	PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION
DETAILS	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DETAILS	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DETAILS	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DETAILS	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION
QUANTITIES	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	QUANTITIES	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	QUANTITIES	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	QUANTITIES	STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION
SECTION	INES S. LI						
DETAILS	INES S. LI						
QUANTITIES	INES S. LI						
SECTION	MISCELLANEOUS BENT DETAILS No. 2						
DETAILS	MISCELLANEOUS BENT DETAILS No. 2						
QUANTITIES	MISCELLANEOUS BENT DETAILS No. 2						
SECTION	CONTRACT NO.: 26590						
DETAILS	CONTRACT NO.: 26590						
QUANTITIES	CONTRACT NO.: 26590						
SECTION	BAYFRONT POC						
DETAILS	BAYFRONT POC						
QUANTITIES	BAYFRONT POC						
SECTION	MISCELLANEOUS BENT DETAILS No. 2						
DETAILS	MISCELLANEOUS BENT DETAILS No. 2						
QUANTITIES	MISCELLANEOUS BENT DETAILS No. 2						
SECTION	CONTRACT NO.: 26590						
DETAILS	CONTRACT NO.: 26590						
QUANTITIES	CONTRACT NO.: 26590						
SECTION	CONTRACT NO.: 26590						
DETAILS	CONTRACT NO.: 26590						
QUANTITIES	CONTRACT NO.: 26590						





- LEGEND:**
- (BT-2X) Indicates girder ID (Main Girders)
  - E-3W Indicates Edge Beam ID
  - (C-3D) Indicates Cap Beam ID
  - CB6-12 Indicates Cross Beam ID
  - Indicates Cross Beams with "Type 1" Connections, see Note 7
  - Indicates Cross Beams with "Type 2" Connections, see Note 7
  - Indicates Bearing Locations
  - Indicates Girder Hinge Locations
  - Indicates Metal Deck Rib Orientation

- NOTES:**
1. Layout information shown is taken of of girder/deck.
  2. Vertical slope of girder and beam varies.
  3. For edge beams and cap beam layout see "BENT CAP FRAMING LAYOUT No. 1" and "BENT AP FRAMING LAYOUT No. 2" sheets.
  4. Elevations shown do not include camber.
  5. For cross beams of bent 8, bent 9, and bent 10 landings not shown, see "BENT CAP FRAMING LAYOUT No. 2" sheet.
  6. For connection "Type 1" and connection "Type 2" details see "GIRDER DETAILS No. 2" sheet.
  7. All cross beams are W12x50 UN.
  8. Metal deck system must be 3" nominal weight concrete fill over 3"-18 gage galvanized metal deck UN. Metal deck must be continuous over at least 2 spans.
  10. For hinge details, see "HINGE DETAILS No. 1" sheet.
  11. For deck joint details, see "DECK JOINT DETAILS No. 2" sheets.

REGISTERED CIVIL ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

The State of California, in witness whereof, I have hereunto set my hand and the seal of the State Engineer at the City of Sacramento, California, this 15th day of August, 2015.

REGISTRAR OF CIVIL ENGINEERS

REGISTERED PROFESSIONAL ENGINEER  
No. 551561  
Exp. 6/30/18

BIGGS CARROSSA ASSOCIATES INC.  
815 THE ALAMITOS  
SAN JOSE, CALIFORNIA 95128

SECTION: BAYFRONT POC  
PROJECT: BAYFRONT POC  
SHEET: 31 OF 71

DATE PLOTTED: 8/10/17

USER: BAYFRONT POC

DESIGN ENGINEER	DESIGN	DATE	PROJECT
REVISIONS	BY	DATE	DESCRIPTION
1	B. AVON	8/10/17	ISSUE FOR PERMIT

PREPARED FOR THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION

PROJECT NUMBER & PHASE: BAYFRONT POC

CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

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DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

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CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

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CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

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DATE: 8/10/17

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PROJECT NUMBER & PHASE: BAYFRONT POC

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DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

DATE: 8/10/17

PROJECT NUMBER & PHASE: BAYFRONT POC

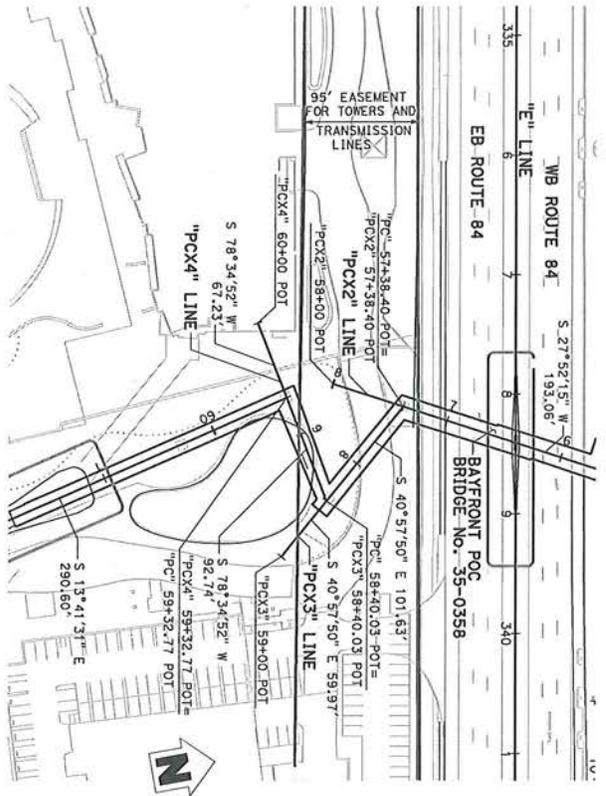
CONTRACT NO.: 2R590

DESIGNED BY: B. AVON

CHECKED BY: B. SWARTZICK

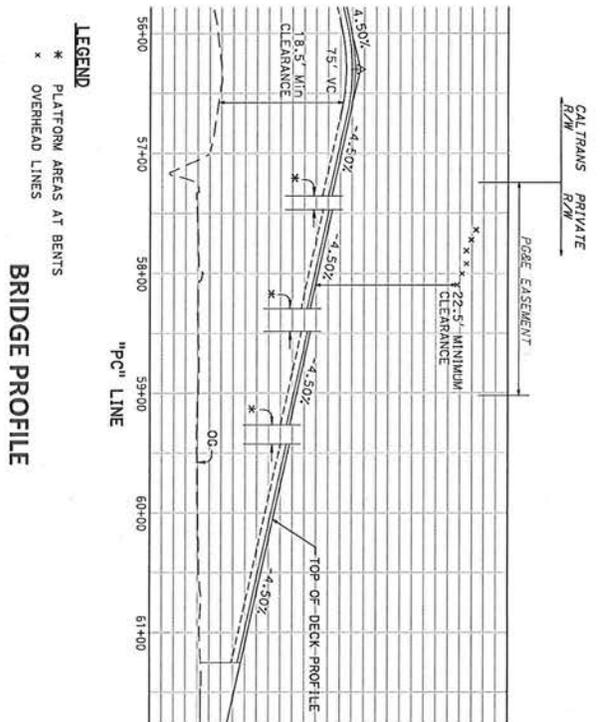
DATE: 8/10/17



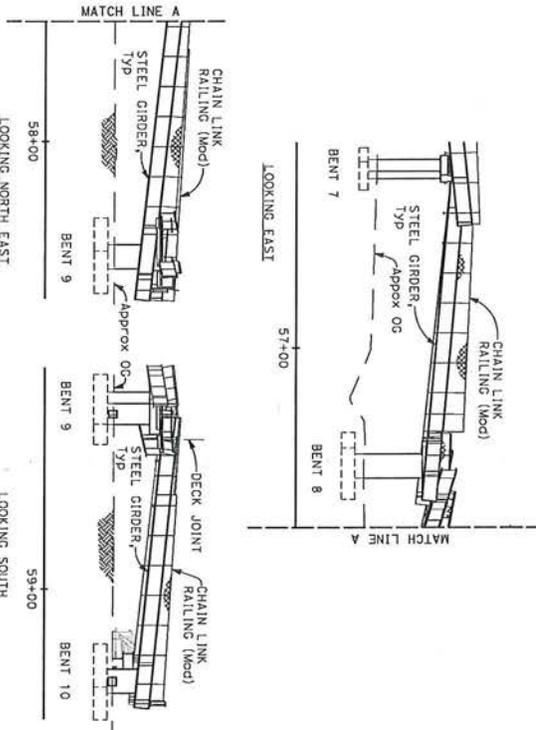


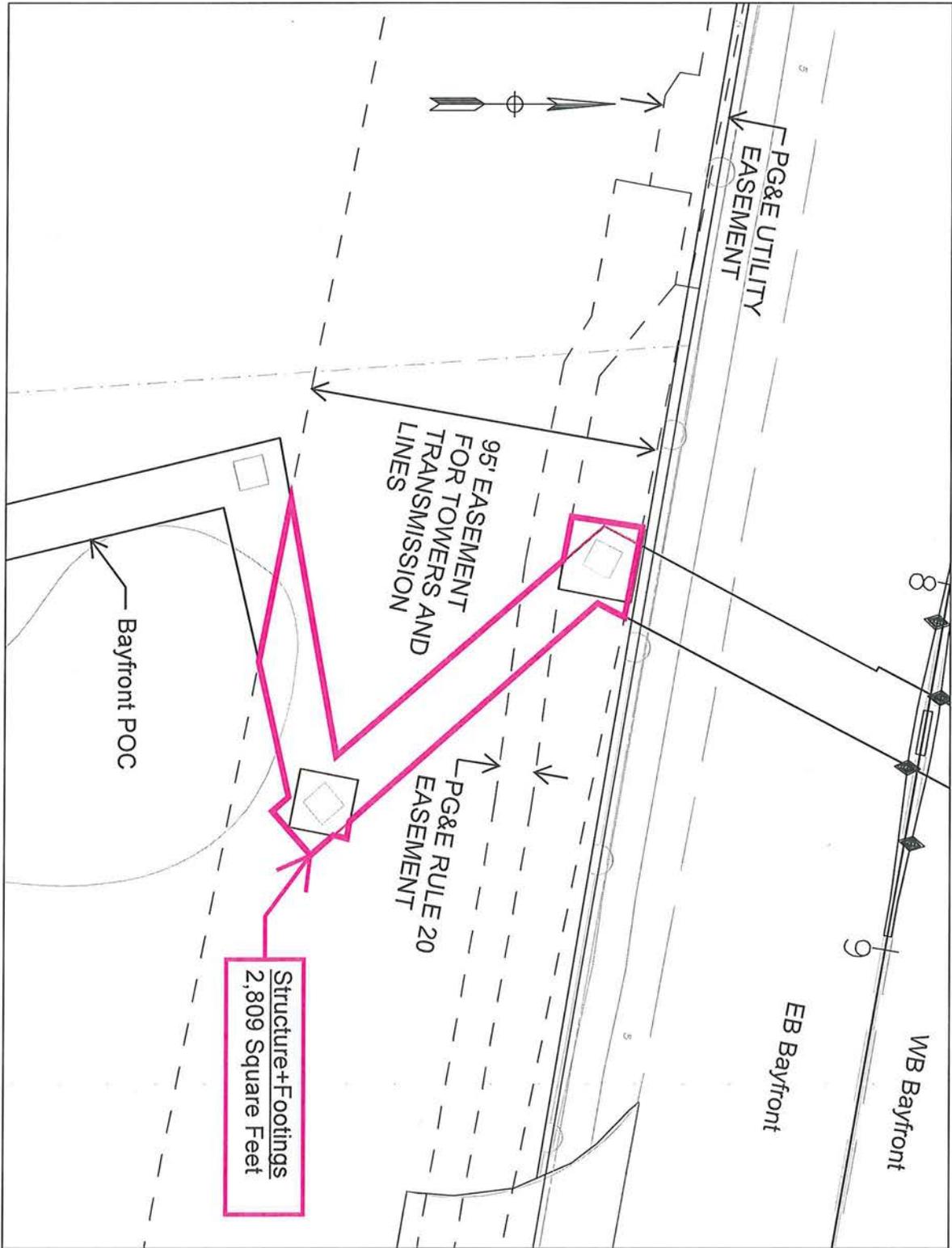
**NOTE**  
1. MAX FOOTING DIMENSIONS ARE 20' X 20'

**BRIDGE FOOTING LAYOUT**



**LEGEND**  
\* PLATFORM AREAS AT BENTS  
\* OVERHEAD LINES





## **Attachment 8**

### **Notice of Determination**

FILED ENDORSED  
IN THE OFFICE OF THE  
COUNTY CLERK RECORDER  
SAN MATEO COUNTY CALIF.

NOV 04 2016

Notice of Determination

Appendix D

MARK CHURCH, County Clerk

To:  Office of Planning and Research  
 U.S. Mail: Street Address:  
 P.O. Box 3044 1400 Tenth St., Rm 113  
 Sacramento, CA 95812-3044 Sacramento, CA 95814

By: BESZ DE LA VEGA

From: Lead Agency: City of Menlo Park  
Address: 701 Laurel Street  
Menlo Park, CA 94025  
Contact: Kyle Perata  
Phone: 650.330.6721

County Clerk  
 County of: San Mateo  
 Address: 555 County Center  
Redwood City, CA 94063

Lead Agency (if different from above):  
 Address: \_\_\_\_\_  
 Contact: \_\_\_\_\_  
 Phone: \_\_\_\_\_

**SUBJECT: Filing of Notice of Determination in compliance with Section 21108 or 21152 of the Public Resources Code.**

State Clearinghouse Number (if submitted to State Clearinghouse): 2015062056

Project Title: Facebook Campus Expansion Project

Project Applicant: Hibiscus Properties, LLC

Project Location (include county): 301-309 Constitution Drive, Menlo Park, San Mateo County

Project Description:

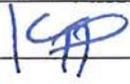
The Project includes the demolition of bldgs at the site and the construction of two new office buildings (Buildings 21 and 22), encompassing approximately 962,400 gsf. The proposed Bldg 21 would contain approximately 512,900 gsf of office and event uses and would be located on the eastern portion of the site. The proposed Bldg 22 would include approximately 449,500 gsf of office uses and would be located on the western portion of the Project site. Maximum bldg heights would be approximately 75 feet. The Project would also include a 200-room limited service hotel with approximately 174,800 gsf of space in the northern portion of the site. Project includes 274 heritage tree removals.

This is to advise that the the City of Menlo Park has approved the above  
 Lead Agency or  Responsible Agency)

described project on November 1, 2016 and has made the following determinations regarding the above  
(date)  
described project.

1. The project  will  will not] have a significant effect on the environment.
2.  An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.  
 A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures  were  were not] made a condition of the approval of the project.
4. A mitigation reporting or monitoring plan  was  was not] adopted for this project.
5. A statement of Overriding Considerations  was  was not] adopted for this project.
6. Findings  were  were not] made pursuant to the provisions of CEQA.

This is to certify that the final EIR with comments and responses and record of project approval, or the negative Declaration, is available to the General Public at:  
http://www.menlopark.org/1012/Environmental-Impact-Report

Signature (Public Agency):  Title: Senior Planner

Date: November 2, 2016 Date Received for filing at OPR: \_\_\_\_\_



State of California - Department of Fish and Wildlife  
**2016 ENVIRONMENTAL FILING FEE CASH RECEIPT**  
 DFW 753.5a (Rev. 12/15/15) Previously DFG 753.5a

**Print**    **StartOver**    **Finalize&Email**

RECEIPT NUMBER:  
 41 — 11042016 —  
 STATE CLEARINGHOUSE NUMBER (If applicable)

SEE INSTRUCTIONS ON REVERSE. TYPE OR PRINT CLEARLY.

LEAD AGENCY: **City of Menlo Park**    LEAD AGENCY EMAIL:    DATE: **11042016**

COUNTY/STATE AGENCY OF FILING: **San Mateo**    DOCUMENT NUMBER:

PROJECT TITLE: **Facebook Campus Expansion Project**

PROJECT APPLICANT NAME: **K Perata**    PROJECT APPLICANT EMAIL:    PHONE NUMBER: ( )

PROJECT APPLICANT ADDRESS:    CITY:    STATE:    ZIP CODE:

PROJECT APPLICANT (Check appropriate box):  
 Local Public Agency     School District     Other Special District     State Agency     Private Entity

CHECK APPLICABLE FEES:

<input checked="" type="checkbox"/> Environmental Impact Report (EIR)	\$3,070.00	\$	<u>3,070.00</u>
<input type="checkbox"/> Mitigated/Negative Declaration (MND)(ND)	\$2,210.25	\$	<u>0.00</u>
<input type="checkbox"/> Certified Regulatory Program document (CRP)	\$1,043.75	\$	<u>0.00</u>
<input type="checkbox"/> Exempt from fee			
<input type="checkbox"/> Notice of Exemption (attach)			
<input type="checkbox"/> CDFW No Effect Determination (attach)			
<input type="checkbox"/> Fee previously paid (attach previously issued cash receipt copy)			

<input type="checkbox"/> Water Right Application or Petition Fee (State Water Resources Control Board only)	\$850.00	\$	<u>0.00</u>
<input checked="" type="checkbox"/> County documentary handling fee		\$	<u>50.00</u>
<input type="checkbox"/> Other		\$	<u>          </u>

PAYMENT METHOD:  
 Cash     Credit     Check     Other

TOTAL RECEIVED    \$    3,120.00

SIGNATURE: **X**    AGENCY OF FILING PRINTED NAME AND TITLE: **Besz de la Vega - County Clerk**

**County of San Mateo  
Assessor-County Clerk-Recorder  
Mark Church**

**555 County Center  
Redwood City, CA, 94063**

Finalization 2016069037  
11/4/16 1:44 pm  
021 36

Item Title

-----  
1 EIR  
Fish & Game: Env Impact Report  
Document ID Amount  
-----  
DOC# 2016-000325 3120.00  
Time Recorded 1:44 pm

-----  
Total 3120.00

-----  
Payment Type Amount  
-----  
Credit Card 3120.00  
# 25700

Amount Due 0.00

SAN MATEO COUNTY RECORDER  
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## **Attachment 9**

**Environmental Assessment and Finding of  
No Significant Impact  
(U.S. Department of the Interior Fish and Wildlife  
Service)**

# ENVIRONMENTAL ASSESSMENT

## Don Edwards San Francisco Bay National Wildlife Refuge And Facebook Multi-Use Bridge Project

May 2017

### 1. PURPOSE OF AND NEED FOR ACTION

#### 1.1 Introduction

This environmental assessment (EA) evaluates the environmental effects of issuance of a General Activities Special Use Permit (SUP) to Facebook for use of Don Edwards San Francisco Bay National Wildlife Refuge (Refuge) property during construction of a multi-use bridge over State Route (SR) 84, as well as construction and placement of a portion of the Bay Trail on the Refuge. This EA draws on information contained in the City of Menlo Park's Final Environmental Impact Report (EIR) for the Facebook Campus Expansion Project (ICF International 2016), which includes the proposed bridge and Bay Trail segment construction. The U.S. Fish and Wildlife Service (Service) will use this EA to solicit public involvement and to determine whether issuance of an SUP would have a significant effect on the quality of the environment. This is part of the Service's decision-making process in accordance with the National Environmental Policy Act (NEPA), amended, and its implementing regulations. NEPA requires Federal agencies to consider the environmental effects of all actions<sup>1</sup> they undertake. This EA addresses public use activities specific to the Ravenswood Complex of the Refuge.

#### 1.2 Purpose and Need for the Proposed Action

The purpose of the proposed action is to expand public priority uses (e.g., wildlife-oriented recreational opportunities) by facilitating construction of a multi-use bridge over SR 84 and relocating a portion of the Bay Trail onto Refuge property. As part of its Campus Expansion Project, Facebook is proposing to construct a new bicycle/pedestrian bridge over SR 84. A new connection to the Bay Trail from the proposed bridge would provide the public with an additional Refuge access opportunity, and may reduce access demand from the existing trailhead at Bedwell Bayfront Park. The bridge overlook would include an interpretive display that highlights key resources and issues associated with the adjacent Refuge, such as its use as wildlife habitat for Federally listed species including the western snowy plover (*Charadrius alexandrinus nivosus*), its importance as a stopover for migratory birds, and the importance of urban wildlife refuges.

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<sup>1</sup> Under NEPA and implementing regulations, *action* refers to a policy, plan, program, or project that is implemented, funded, permitted, or controlled by a Federal agency or agencies.

Although the foundation and vertical supports of the bridge touchdown north of SR 84 would be located within the California Department of Transportation (Caltrans) easement, construction of the proposed bridge would not be feasible without temporary access to Refuge lands during construction (i.e., for use as a staging area for construction equipment and to provide the necessary access for construction of the northern bridge landing). With the construction of the northern bridge landing, a portion of the existing east-west running Bay Trail would be impacted. As a result, the Bay Trail would be realigned to the north, into the Refuge right-of-way (ROW) and would continue to provide east-west connectivity around the northern ramp.

The Refuge was established with three major purposes: (1) the preservation of the natural resources of the South San Francisco Bay (Bay), (2) the provision of environmental education and wildlife interpretation opportunities to Bay Area schools and residents, and (3) the protection of an important open space resource and other wildlife-oriented recreation opportunities for the enjoyment of local residents and visitors. Implementation of the proposed action would help to fulfill all three of the Refuge's major purposes (EDAW 1974).

### **1.3 Proposed Action**

The Service proposes to implement Alternative B, as described in this EA. This Alternative would result in the issuance of a SUP to Facebook for use of Refuge property during construction of a multi-use bridge over SR 84 and construction of a portion of the Bay Trail on Refuge property. The proposed action is consistent with the current public use activities of wildlife observation and photography in the Ravenswood Complex which is in the project vicinity. It is also consistent with the Refuge's Comprehensive Conservation Plan and Environmental Assessment (CCP/EA) goal to "provide the local community and other visitors with compatible wildlife-oriented outdoor recreation opportunities to enjoy, understand, and appreciate the resources of the Refuge" (Service 2012). The proposed action also supports the strategy under Objective 3.1 of the CCP, "ensure that public access opportunities at the Refuge are maintained or expanded, especially in light of restoration and enhancement activities".

### **1.4 Project Area**

The Refuge, which was established in 1972 as the first urban National Wildlife Refuge, currently consists of 30,000 acres at the south end of the Bay (Figure 1). It is a major wintering area for Pacific Flyway waterfowl and consists of a mixture of salt marsh, ponds, mudflat, seasonal wetlands, and uplands. The Refuge is divided into four management units: Newark, Mowry, Alviso, and West Bay Units. The project site is located within the Ravenswood Complex, which is part of the West Bay Unit (Figure 2). The Ravenswood Complex consists of seven ponds (R1, R2, R3, R4, R5, S5, and SF2) located along both sides of SR 84 west of the Dumbarton Bridge, and on the bayside of the developed areas of the City of Menlo Park in San Mateo County. The project site is located immediately south of Pond R3.

The approximately 0.68-acre portion of the Refuge covered in this EA consists of all areas that would be permanently impacted by construction of the Bay Trail, as well as all areas that will be temporarily impacted due to construction activities (Figure 3).

## **1.5 Decision to be Made**

Based on the analysis documented in this EA, the Refuge will determine whether the proposed action would have a significant effect on the quality of the environment and whether it qualifies for issuance of an SUP. If the selected alternative has no significant impacts, then the Service would prepare a Finding of No Significant Impact (FONSI). If the proposed management alternative is found to have significant impacts, then the Service would prepare an Environmental Impact Statement (EIS) before making a decision.

## **1.6 Public Involvement**

The Draft EA will be available for public review and comment for a 14-day period from June 8, 2017 through June 24, 2017. Interested stakeholders, State and local agencies, local landowners, and others were sent notification letters of where they could obtain or review the Draft EA. An electronic copy of the Draft EA is also posted on the Refuge's website ([www.fws.gov/refuge/don\\_edwards\\_san\\_francisco\\_bay/planning](http://www.fws.gov/refuge/don_edwards_san_francisco_bay/planning)).

## **1.7 U.S. Fish and Wildlife Service and National Wildlife Refuge System**

The mission of the Service is working with others to conserve, protect, and enhance the nation's fish and wildlife and their habitats for the continuing benefit of the American people. The Service is the primary Federal agency responsible for migratory birds, endangered plants and animals, certain marine mammals, and anadromous fish. This responsibility to conserve our nation's fish and wildlife resources is shared with other Federal agencies and State and Tribal governments.

As part of this responsibility, the Service manages the National Wildlife Refuge System (Refuge System). The Refuge System is the only nationwide system of Federal lands managed and protected for wildlife and their habitats. The mission of the Refuge System is to administer a national network of lands and waters for the conservation, management, and where appropriate, restoration of the fish, wildlife and plant resources and their habitats within the United States for the benefit of present and future generations of Americans.

The Don Edwards San Francisco Bay National Wildlife Refuge is managed as part of the Refuge System in accordance with the National Wildlife Refuge System Administration Act of 1966 as amended by the Improvement Act, and other relevant legislation, executive orders, regulations, and policies.

## **2. ALTERNATIVES, INCLUDING THE PROPOSED ACTION**

### **2.1 Alternative A: No Action**

Under the no action alternative, an SUP would not be issued. Facebook would not construct a multi-use bridge over SR 84 enhancing connectivity between the Belle Haven neighborhood, and a portion of the existing Bay Trail south of Pond R3 would not be realigned to the north within Refuge property.

### **2.2 Alternative B: Proposed Action**

Under the proposed action, the Refuge would issue an SUP for Facebook's use of Refuge property during construction of a multi-use bridge over SR 84. Although a portion of the aerial walkway would cantilever beyond the Caltrans easement, the foundation and vertical supports of the bridge touchdown north of SR 84 would be located entirely within the Caltrans ROW adjacent to the Bay Trail and the footprint would not extend into the Refuge (Figure 3). However, temporary access to Refuge lands would be needed to facilitate construction of the northern bridge landing (sufficient space is not available within the Caltrans ROW) and to provide a staging area for construction equipment. In addition, as described above, an approximately 800-foot long section of the existing east-west running Bay Trail would be realigned to the north, into the Refuge ROW, to provide east-west connectivity around the bridge's northern ramp. The trail would be paved with asphalt. Further, a low fence would be installed north of the Bay Trail within Refuge property.

#### **2.2.1 Proposed Wildlife-Oriented Recreation**

The Refuge would team with Facebook to promote wildlife-oriented recreation through the facilitation of the construction of a multi-use bridge over SR 84, which would provide the public with an additional Refuge access opportunity, the realignment of the Bay Trail to provide east-west connectivity around the bridge's northern ramp, and construction of protective fencing along a portion of the Bay Trail. A low fence would be installed north of the Bay Trail within the project site. The purpose of the fence is to deter people and their pets from leaving the trail and entering the managed pond habitat there. The fence would also help keep trash from blowing into the ponds.

### 3. AFFECTED ENVIRONMENT

A complete description of the Refuge environment can be found in the CCP/EA and the South Bay Salt Pond Restoration Project (SBSPRP) Phase 2 Final EIS/EIR (Service and California State Coastal Conservancy 2016). This chapter primarily addresses the existing condition and resources within the Ravenswood Complex at the project site.

#### 3.1 Physical Environment

##### 3.1.1 Geology, Soils, and Seismicity

The proposed area is located on the San Francisco Peninsula, a ridge of rocks and sediments in the Santa Cruz Mountains portion of the Coast Ranges that separates the San Francisco Bay from the Pacific Ocean. In the project area, Quaternary alluvial sediment derived from the Santa Cruz Mountains overlies older Cretaceous and Jurassic sedimentary rocks (ICF International 2016). The youngest of this alluvial material consists of Holocene-age (11,000 years or younger) unconsolidated clay, which is interbedded with sand and fine gravel (ICF International 2016). One soil association occurs within the project site: Novato clay, 0 to 1 percent slopes (Natural Resources Conservation Service 2016).

No faults run through the project site; however, the San Jose Fault traverses a portion of Pond R3. Other faults near the project site include the Stanford Fault and Palo Alto Fault, which are 0.5 mile west of Ponds R5 and S5, respectively (Service and California State Coastal Conservancy 2016). The Stanford, Palo Alto, and San Jose Faults are concealed Quaternary faults, meaning they have less potential for surface rupture but are still considered active faults (Service and California State Coastal Conservancy 2016).

##### 3.1.2 Hydrology

The project site is located within the alluvial fan of the lower San Francisquito Creek watershed. Major surface waters in the project vicinity include the Ravenswood Slough to the north, San Francisquito Creek to the southeast, Atherton Channel (also known as Atherton Creek) to the west, Flood Slough to the northwest, and Lower San Francisco Bay to the east. The entirety of the project site is within the Federal Emergency Management Agency (FEMA) 100-year floodplain and subject to tidal flooding from the Bay (FEMA 2015).

**Surface Water.** No surface waters are present within the project site. However, to the north, northwest, and northeast, the site is bordered by Pond R3, which is wet during the winter but becomes a salt panne in summer.

**Groundwater.** The project area is located within the San Mateo subbasin of the larger Santa Clara Valley groundwater basin. A relatively shallow water table aquifer overlies confined and semi-confined aquifers near the margins of the Bay, with most wells drawing from deeper deposits. The direction of groundwater flow is generally to the east and north. The basin is composed of alluvial fan deposits formed by tributaries to the

Bay, which drain the basin. A relatively shallow water table aquifer overlies confined and semi-confined aquifers.

Groundwater beneath the project site is not considered to be a source of drinking water, according to the San Francisco Regional Water Quality Control Board, because of the elevated salinity in the groundwater (City of Menlo Park 2012).

### **3.1.3 Water Quality and Contaminants**

The quality of the stormwater runoff from the project site is typical of urban watersheds where water quality is affected primarily by discharges from both point and nonpoint sources. Point and nonpoint sources include winter storms, overland flow, exposed soil, and streets. Water quality in the project vicinity is directly affected by stormwater runoff from adjacent streets and properties. Grease, oil, hydrocarbons, and metals deposited by vehicles and heavy equipment can accumulate on streets and paved parking lots and be carried into storm drains by runoff. Polychlorinated biphenyls (PCBs) are also listed as 3(d) impairments in the Lower San Francisco Bay. PCBs can be found in automobile engines and other sources common in urban areas.

Pond R3 is located immediately adjacent to the northern boundary of the project site. The levees of Pond R3 are unbreached. Pond R3 receives direct precipitation, groundwater inflows, and minimal overland runoff during the wet season. During the dry season, it dries out due to seepage and evaporation, forming a dry salt pan, although historic slough traces retain water. Salinities and metal concentrations in sediments and in the slough traces are expected to be elevated in comparison to concentrations in open Bay water because of concentration by evaporation. In 2003, total mercury concentrations in Pond R4 (located immediately north of Pond R3) averaged 0.05 milligrams/kilogram, which is lower than typical concentrations found in the Bay. Methylmercury concentrations averaged 0.37 microgram/kilogram, which is also less than concentrations generally found in the Bay (U.S. Geological Survey 2005).

### **3.1.4 Air Quality**

The project site is within the San Francisco Bay Area Air Basin (SFBAAB), which includes all of Alameda, Contra Costa, Marin, Napa, San Francisco, San Mateo, Santa Clara, the western half of Solano, and the southern half of Sonoma Counties. A wide range of emissions sources, such as dense population centers, heavy vehicular traffic, and industrial activity, influence the air quality within the SFBAAB. The primary pollutants of concern in the project area are ozone (including nitrogen oxides [NOx]), carbon dioxide (CO), and particulate matter (PM) (ICF International 2016).

Existing air quality conditions in the project area can be characterized by monitoring data collected in the region. Local monitoring data are used to designate areas as nonattainment, maintenance, attainment, or unclassified for the National Ambient Air Quality Standards (NAAQS) and California Ambient Air Quality Standards (CAAQS). The four designations are defined as follows:

- **Nonattainment** – assigned to areas where monitored pollutant concentrations consistently violate the standard in question.
- **Maintenance** – assigned to areas where monitored pollutant concentrations exceeded the standard in question in the past but are no longer in violation of that standard.
- **Attainment** – assigned to areas where pollutant concentrations meet the standard in question over a designated period of time.
- **Unclassified** – assigned to areas where data are insufficient to determine whether a pollutant is violating the standard in question.

Table 1 summarizes the attainment status of San Mateo County with regard to the NAAQs and CAAQS.

**Table 1. Federal and State Attainment Status for San Mateo County**

Criteria Pollutant	Federal Designation	State Designation
O <sub>3</sub> (8-hour)	Marginal nonattainment	Nonattainment
CO	Maintenance	Attainment
PM <sub>10</sub>	Attainment	Nonattainment
PM <sub>2.5</sub>	Nonattainment	Nonattainment
NO <sub>2</sub>	Attainment	Attainment
SO <sub>2</sub>	Attainment	Attainment
Lead	Attainment	Attainment
Sulfates	(No Federal standard)	Attainment
Hydrogen sulfide	(No Federal standard)	Unclassified
Visibility reducing particles	(No Federal standard)	Unclassified

Sources: California Air Resources Board (2014), U.S. Environmental Protection Agency (2015)

### 3.1.5 Noise

The existing ambient noise environment at the project site is characteristic of an urban environment. Although aircraft overflights are periodically audible from the portion of the Refuge within the project boundaries, the noise environment in this area was determined to be influenced primarily by vehicle traffic on SR 84 (ICF International 2016). The noise level along SR 84 is approximately 70 A-weighted decibels (dBA) (Community Noise Equivalent Level [CNEL]) based on the vehicle volume traveling along the highway (Service and California State Coastal Conservancy 2016). Noise levels decrease to 60 dBA (CNEL) at a distance of approximately a 0.25 mile to almost a 0.5 mile from SR 84 (Service and California State Coastal Conservancy 2016). No major noise sources exist within the project site. No sensitive receptors occur within the project site or adjacent Pond R3. The nearest sensitive receptors are more than 700 feet south of Pond R3 (Service and California State Coastal Conservancy 2016).

## 3.2 Biological Environment

### 3.2.1 Vegetation

The approximately 0.68-acre portion of the Refuge covered in this EA consists of ruderal grassland located immediately south of Pond R3 (Figure 4). Ruderal grassland habitat is characterized by an abundance of species, including both non-native annual grasses and forbs, that occur in recently or regularly disturbed areas. On the project site, this habitat consists of a mixture of wild oats (*Avena fatua*), black mustard (*Brassica nigra*), yellow star-thistle (*Centaurea solstitialis*), bristly oxtongue (*Helminthotheca echioides*), cats ear (*Hypochaeris radica*), and sugar beet (*Beta vulgaris*).

As described above, no aquatic, wetland, or riparian habitat, including jurisdictional wetlands or others waters of the United States, is present within the project site. However, to the north, northwest, and northeast, the site is bordered by Pond R3, which is wet during the winter but becomes a salt panne in summer, and which has a narrow fringe of salt marsh and peripheral halophyte marsh habitat at its edges. The pond bottom, and remnant slough channels and borrow ditches, have extremely high salinity, which inhibits most plant life. Vegetation growing on the pond bottom is limited to salt-tolerant vegetation, such as small flowered iceplant (*Mesembryanthemum nodiflorum*) (Service and California State Coastal Conservancy 2016).

### 3.2.2 Wildlife Resources

The wildlife species found in the ruderal grasslands on the project site are primarily common, widespread species adapted to periodic human disturbance. Reptiles such as the western fence lizard (*Sceloporus occidentalis*), gopher snake (*Pituophis melanoleucus*), and southern alligator lizard (*Elgaria multicarinata*) occur here. Birds such as the mourning dove (*Zenaidura macroura*), Brewer's blackbird (*Euphagus cyanocephalus*), house finch (*Haemorrhous mexicanus*), and white-crowned sparrow (*Zonotrichia leucophrys*) forage in the ruderal grasslands on the site. However, due to the structural simplicity of the ruderal grasslands, their limited size, and the level of disturbance, few birds (if any) are expected to nest here. Mammals including the California vole (*Microtus californicus*), western harvest mouse (*Reithrodontomys megalotis*), valley pocket gopher (*Thomomys bottae*), and striped skunk (*Mephitis mephitis*) also occur in the ruderal grasslands in the project area. The adjacent Pond R3 provides foraging, roosting, and nesting habitat for western snowy plovers. A variety of other shorebirds uses this pond for foraging, primarily during the nonbreeding season.

### 3.2.3 Sensitive Species

The project does not contain critical habitat for any federally listed animal or plant species subject to the jurisdiction of the Service or National Marine Fisheries Service (NMFS). The closest designated critical habitat is for the southern green sturgeon (*Acipenser medirostris*) and Central California Coast steelhead (*Oncorhynchus mykiss*) and is located in tidal waters associated with Flood Slough approximately 0.8 mile northwest of the project site.

Appendix E of the CCP (Service 2012) contains a list of sensitive species that occur and potentially occur on the Refuge. Six federally and/or state listed species are known to occur, or could potentially occur, regularly in the project vicinity. These are the southern green sturgeon, Central California Coast steelhead, salt marsh harvest mouse (*Reithrodontomys raviventris*), California Ridgway's rail (*Rallus obsoletus obsoletus*)<sup>2</sup>, California black rail (*Laterallus jamaicensis coturniculus*), and western snowy plover. However, none of these species are expected to occur on the project site itself, and only the western snowy plover may be indirectly impacted. This species and its potential for occurrence in the project vicinity is discussed in detail below.

**Western Snowy Plover (*Charadrius alexandrinus nivosus*). Federal Status: Endangered; State Status: California Species of Special Concern.** The Pacific coast population of western snowy plovers nests on sandy beaches and salt panne habitat from Washington to Baja Mexico. Much of their nesting habitat, particularly in southern California, has been lost to development and high levels of human use. In San Francisco Bay, snowy plovers typically nest on dry, relatively homogenous salt pond bottoms. These nesting areas are located near water, where prey (usually brine flies and other insects) are abundant. In some areas, snowy plovers nest within dry saline managed ponds; in other areas where ponds typically hold water through the summer, nests are located primarily on levees. Levees used for nesting include the low, sparsely vegetated internal levees between managed ponds or cells within ponds; larger flood-control levees with extensive vegetation on their banks, such as the ones lining sloughs, are not used for nesting.

Nearly all of the San Francisco Bay western snowy plover nesting occurs south of SR 92 (San Mateo Bridge) in the South Bay (Page and Stenzel 1981, Page et al. 1991, Service 2007). Snowy plovers are known to nest in the Ravenswood Complex, which consists of Ponds R1, R2, RSF2, R3, R4, and R5, immediately north of the project site. Since 2003, San Francisco Bay Bird Observatory (SFBBO) has conducted annual western snowy plover monitoring of the Ravenswood Complex in support of the goals set forth by the Refuge. Specifically, they have (1) identified areas used by snowy plovers through regular surveys of all potential nesting habitat from March through September, (2) participated in Service-coordinated breeding and winter window counts to estimate Recovery Unit 3 snowy plover numbers, and (3) recorded nest fates, nest densities, and chick fledging rates through nest-monitoring and chick-banding. Of 41 nests detected in the Ravenswood ponds in 2014, 16 were located in Pond R1, nine in RSF2, six in R4, and nine in R3 (Tokatlian et al. 2014). In 2015, nest numbers doubled to 82, with eight located in Pond R1, one in R2, 29 in RSF2, 29 in R4, four in R5, and 11 in R3 (SFBBP unpubl.).

However, the vast majority of western snowy plover nesting in these ponds occurs well away from the project site and more than 600 feet from active nests, which is the typical non-disturbance buffer recommended around active western snowy plover nests by the Service. Between 2009 and 2015, only one western snowy plover nest was located within 600 feet of the project footprint; in 2015, this nest was recorded in Pond R3 approximately 400 feet north of the Bay Trail at the eastern end of the project footprint. However, this nest was over 790 feet from the easternmost end of the proposed multi-use bridge and the platform overlooking the NWR. Therefore, no plovers have been recorded nesting within 790 feet from the proposed bridge and

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<sup>2</sup> Formerly known as the California clapper rail (*Rallus longirostris obsoletus*)

platform. The next closest recorded nests occurred in 2015 (approximately 1,000 feet to the northeast of the project footprint) and 2010 (approximately 1,200 feet northeast of the project footprint). These nests were even greater distances from the proposed bridge. Therefore, snowy plover nesting does not occur very close to the project site, and nesting even at distances of 400 feet and greater from the project site is very infrequent. Areas used by foraging snowy plovers have not been well studied within Pond R3, and it is possible foraging plovers may occur closer to the project site than nesting plovers do. However, given the distribution of nesting birds, foraging plovers are not expected to make heavy use of the portion of the pond closest to the project site.

The Service designated seven units of critical habitat for the western snowy plover in the San Francisco Bay, including Ravenswood (southwestern portion of RSF2; 89 acres). The designated critical habitat in Pond RSF2 is far enough from the project site (> 1 mile) that no effects on snowy plover critical habitat, either direct or indirect, from the project would occur.

### **3.3 Social and Economic Environment**

#### **3.3.1 Recreation**

No public access or recreational features are present within the project site, although a portion of the Bay Trail is located immediately south of the project boundary along SR 84. No boating, waterfowl hunting, or fishing is allowed in Pond R3. The nearest interpretive exhibits are located in the parking area at the entrance to Bedwell Bayfront Park on Marsh Road (approximately 0.8 mile to the northwest) and the nearest viewing point is located at the top of the hill near the northeast corner of Bedwell Bayfront Park, and along the Pond SF2 Trail.

#### **3.3.2 Local Economy**

A substantial portion (32.7%) of the population of East Palo Alto occurs within 1 mile of the Ravenswood Complex, and the socioeconomic climate around the project site is that of a developed community (Service and California State Coastal Conservancy 2016). Approximately 68% of Menlo Park residents age 16 and older were in the work force in 2010, nearly identical to the County rate and a few percentage points higher than the state rate, and steady employment growth is expected between 2015 and 2020 (ICF International 2016).

#### **3.3.3 Cultural Resources**

The project site lies within an area once occupied by the Ohlone group of Native Americans. Native American archaeological sites in this area of San Mateo County tend to be situated near the historic margin of the Bay's tidal marshland and along creeks that drain upland terrain that borders the Bayshore plain (ICF International 2016). The project site is located on former marshland. A record search for an area including the project site was performed as part of the SBSRP (EDAW 2005) and again in 2013 by the Northwest Information Center (NWIC) of the California Historical Resources Information System in Rohnert Park,

California (NWIC file 13-0330) (Service and California State Coastal Conservancy 2016). In addition, the project site was subject to an on-site visual survey by URS cultural resource personnel in November 2013 (Service and California State Coastal Conservancy 2016). Based on the results of the record search and on-site survey, no known cultural resources occur within the project site (Service and California State Coastal Conservancy 2016).

### **3.3.4 Traffic**

The project site is on the Peninsula side of the Bay, west of the Dumbarton Bridge, and adjacent to the City of Menlo Park in San Mateo County. Bedwell Bayfront Park in Menlo Park is directly west of the project site, and a portion of SR 84 occurs along its southern border. U.S. 101 is approximately 0.5 mile southwest of the project site. In 2013, the traffic volume for SR 84 between University Avenue (SR 109) and Willow Road (SR 114) was 56,000 peak hour trips (Caltrans 2014).

There is no public vehicle access and no public trails within the project site. However, bicycle and pedestrian access to the southern perimeter of the site is available from the Bay Trail. The Bay Trail is accessible from U.S. 101 and SR 84 via Marsh Road. Levee roads around Pond R3 are accessible only to Refuge vehicles for operations and maintenance activities.

There is limited public transit to the Ravenswood Complex. The Caltrain Marsh Road shuttle travels from the Menlo Park Caltrain Station to the intersection of SR 84 and Marsh Road. The Dumbarton Express, run by a consortium of transit agencies and administered by AC Transit, runs in both directions across the Dumbarton Bridge and passes just south of the project site on SR 84. The Menlo Park Caltrain Station is approximately 2 miles to the southwest. The closest airport to the project site is the San Francisco International Airport, which is located approximately 21 miles to the northwest.

## **4. ENVIRONMENTAL CONSEQUENCES**

This chapter analyzes the environmental impacts expected to occur from the implementation of the alternatives described in Chapter 2. Impact evaluation has been conducted for each aspect of the environment described in Chapter 3, including physical, biological, and social and economic resources. Direct, indirect, and cumulative impacts are described where applicable for each alternative. Alternative A (No Action) serves as a baseline against which Alternative B is compared.

NEPA regulations require a discussion of mitigation measures when adverse impacts on habitats, wildlife, or the human environment are identified. All potential impacts were considered and mitigation measures were identified for Alternative B.

In describing the significance of impacts, the Service defers to the following NEPA implementing regulations (40 CFR 1508.27).

“Significantly” as used in NEPA requires considerations of both context and intensity:

- a) Context. This means that the significance of an action must be analyzed in several contexts such as society as a whole (human, national), the affected region, the affected interests, and the locality. Significance varies with the setting of the proposed action. For instance, in the case of a site-specific action, significance would usually depend upon the effects in the locale rather than in the world as a whole. Both short- and long-term effects are relevant.
- b) Intensity. This refers to the severity of impact. Responsible officials must bear in mind that more than one agency may make decisions about partial aspects of a major action.

Significance of impacts on the environment determines whether preparation of an EIS is warranted. Thus, an EA provides a discussion of the magnitude of the impacts within the context of the situation for each impact topic.

## 4.1 Physical Environment

### 4.1.1 Geology, Soils, and Seismicity

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effects related to geology, soils, and seismicity due to the no action alternative.

**Alternative B.** Alternative B would not place recreational facilities on the San Jose Fault trace. Therefore, potential effects on people and property due to a rupture immediately on or adjacent to a fault during an earthquake under Alternative B would be less than significant.

Localized soil disturbance (e.g., compaction and erosion) would occur within the project site due to the use of motorized heavy equipment and foot traffic during construction. However, this impact would be temporary. Project implementation would also involve grading to construct the realigned portion of the Bay Trail within Refuge property. The project sponsor would be required to implement the specifications in Appendix J of the 2013 California Building Standards Code, which regulates grading activities, including associated drainage and erosion control. Compliance with City requirements and the 2013 California Building Standards Code, which are within the authority of the City to enforce and monitor, would ensure that erosion impacts resulting from project construction would be less than significant. Therefore, there would be no substantial adverse effect related to geology, soils, and seismicity from Alternative B.

### 4.1.2 Hydrology

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effects related to hydrology due to the no action alternative.

**Alternative B.** Under Alternative B, construction of the project would not involve work within surface waters and would not alter the course of an existing stream or river because these features do not exist onsite. Therefore, the Service anticipates no adverse effects related to hydrology due to Alternative B.

#### 4.1.3 Water Quality and Contaminants

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no substantial adverse effect on water quality or contaminants due to the no action alternative.

**Alternative B.** As described above, all structural elements of the proposed bridge, including ramps and foundations, would be located outside of Refuge property, within Caltrans ROW. However, localized soil disturbance (e.g., compaction and erosion) would occur within the project site due to the use of motorized heavy equipment and foot traffic during construction. Further, project implementation would involve grading to construct the realigned portion of the Bay Trail within Refuge property. These construction activities could temporarily expose soils to erosive effects from stormwater runoff, an adverse effect.

Construction-related activities could lead to transient adverse water quality impacts in Pond R3 during or shortly after the period of construction. Grading could affect water and sediment quality and result in short-term increases in turbidity. Construction activities would also bring equipment and materials not normally present in the project area onto the site. These activities would increase the possibility of exposure to or release of hazardous materials and waste associated with construction, such as fuels or oils, because of accidents or equipment malfunction or maintenance, an adverse effect.

Additional wildlife-oriented recreation, interpretation, environmental education, and volunteer opportunities under this alternative would also result in additional foot traffic on the Refuge. Increased visitation may also result in soil disturbance. However, the proposed construction of a low fence along the southern edge of Pond R3 adjacent to the realigned Bay Trail would discourage Bay Trail users from leaving the trail and minimize soil disturbance.

Measures to address potential water-quality impacts are described below.

##### **Mitigation Measure to Address Erosion Risks and Water Quality Impacts:**

- The project sponsors will implement a Stormwater Pollution Prevention Plan (SWPPP) to reduce potential erosion and subsequent sedimentation in stormwater runoff during construction. The SWPPP will be developed to ensure that, during rain events, construction activities do not increase the levels of erosion and sedimentation. This plan will include the use of erosion-control materials (e.g., baffles, fiber rolls, or hay bales; temporary containment berms) and erosion-control measures such as straw application or hydroseeding with native grasses on disturbed slopes; and floating sediment booms and/or curtains to minimize any impacts that may occur due to increased mobilization of sediments. Suitable erosion control, sediment control,

source control, treatment control, material management, and non-stormwater management best management practices (BMPs) will be implemented consistent with the latest edition of the California Stormwater Quality Association “Stormwater Best Management Practices Handbook.”

- Staging, access, and parking areas will be located outside of sensitive habitats.
- Areas of disturbance will be limited to the smallest footprint necessary.
- All equipment will be maintained free of petroleum leaks. All vehicles operated within 150 feet of any water body will be inspected daily for leaks and, if necessary, repaired before leaving the staging area. Inspections will be documented in a record that is available for review on request.
- Spill prevention kits will always be in close proximity to construction activities (e.g., crew trucks and other logical locations) when using hazardous materials. Feasible measures will be implemented to ensure that hazardous materials are properly handled and the quality of aquatic resources is protected.
- No fueling will be performed in wetland or aquatic habitats. For stationary equipment that must be fueled on site, containment will be provided in such a manner that any accidental spill of fuel will not be able to enter the water or contaminate sediments that may come in contact with water.
- Vehicles will be washed only at an approved area. No washing of vehicles will occur at the job site.
- A berm or other sediment-control device will be installed around stockpiled soil material to prevent runoff from transporting sediment into sensitive habitats.
- The work site, areas adjacent to the work site, and access roads will be maintained in an orderly condition, free and clear from debris and discarded materials. Personnel will not sweep, grade, or flush surplus materials, rubbish, debris, or dust into storm drains, water bodies, or waterways. Upon completion of work, all building materials, debris, unused materials, concrete forms, and other construction-related materials will be removed from the work site.
- A hazardous materials management/fuel spill containment plan will be developed and implemented by the construction contractor and given to all contractors and biological monitors working on the project, with at least one copy of the plan located onsite at all times. The purpose of the plan is to provide onsite construction managers, environmental compliance monitors, and regulatory agencies with a detailed description of hazardous materials management, spill prevention, and spill response/cleanup measures associated with the construction of the Project elements. The primary objective of the plan is to prevent a spill of hazardous materials. Elements of the plan include, but are not limited to the following:
  - A discussion of hazardous materials management, including delineation of hazardous material and hazardous waste storage area, access and egress routes, waterways, emergency assembly areas, and temporary hazardous waste storage areas
  - Materials Safety Data Sheets for all chemicals used and stored on site
  - An inventory list of emergency equipment
  - Spill control and countermeasures including employee spill prevention/response training

- Notification and documentation procedures
- Monthly reporting plan

With implementation of these mitigation measures, the Service anticipates no substantial adverse effect on water quality or contaminants due to Alternative B.

#### 4.1.4 Air Quality

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no substantial adverse effect on air quality due to the no action alternative.

**Alternative B.** Under Alternative B, construction activities would result in temporary increases in dust (PM10) from earth moving activities and tailpipe emissions (e.g., NO<sub>x</sub>, sulfur oxides [SO<sub>x</sub>], CO) from construction vehicle use. Construction would also generate on-road vehicle trips from workers, construction staff, and material hauling, a potential adverse effect on air quality. Grading and construction activities would require 6-8 people working five days per for 4-5 months to complete. Measures to address potential air-quality impacts are described below.

##### **Mitigation Measure to Address Impacts on Air Quality:**

The state is divided into Air Pollution Control Districts and Air Quality Management Districts. These agencies are county or regional governing authorities that have primary responsibility for controlling air pollution from stationary sources. The Bay Area Air Quality Management District (BAAQMD) has local air quality jurisdiction over projects in San Mateo County. The project sponsor will require all construction contractors to implement the basic construction mitigation measures recommended by the BAAQMD to reduce fugitive dust emissions. Emission reduction measures will include, at a minimum, the following measures (additional measures may be identified by BAAQMD or contractor as appropriate):

- All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) will be watered two times per day. Recycled water, to be purchased through advance arrangement with the City of Redwood City or the City of Palo Alto, will be used to water all exposed surfaces.
- All haul trucks transporting soil, sand, or other loose material offsite will be covered.
- All visible mud or dirt track-out onto adjacent public roads will be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
- All vehicle speeds on unpaved roads will be limited to 15 miles per hour.
- All roadways, driveways, and sidewalks to be paved will be completed as soon as possible.
- A publicly visible sign will be posted with the telephone number and name of the person to contact at the lead agency regarding dust complaints. This person will respond and take corrective action within 48 hours. BAAQMD's phone number will also be visible to ensure compliance with applicable regulations.

Following construction, visitor activities would not substantially affect air quality. It is reasonable to assume that an increase in visitor use at the Refuge may reflect visitors choosing the Refuge as their destination rather than another location offering similar opportunities in the Bay Area (such as East Bay Regional Park System, in proximity to the Refuge). For this reason, the Service anticipates that there would be few to no new vehicle trips, but if there is an increase in tailpipe emissions resulting from increased visitor opportunities, it is likely to be negligible.

With implementation of the mitigation measures described above, the Service anticipates no substantial adverse effect on air quality due to Alternative B.

#### 4.1.5 Noise

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no substantial adverse effect on noise levels due to the no action alternative.

**Alternative B.** Types of equipment used for project construction would include earthwork equipment (e.g., excavator, backhoe, dozer, and compactor) that would be used for general grading, excavating, and backfilling, and pile installation equipment. Deep pile supported foundations are proposed for the bridge footings. This type of pile installation method would eliminate vibration created by conventional pile driving hammers. However, depending on the footing, depths and the soil condition at the site, use of vibratory or impact pile drivers may be necessary over a short period for installing temporary excavation shoring systems. Additional construction equipment may include concrete mixer trucks, pump trucks, flatbed trucks, and cranes of various sizes. Smaller cranes would be used for general lifting purposes. The larger cranes would be used for lifting the assembled steel girder spans during erection. A summary of anticipated equipment type and the associated operation noise levels is provided in Table 1 below.

**Table 2. Construction Equipment and Operation Noise Levels**

Equipment	Acoustical Utilization Factor (%)	Typical Noise Level (dBA) at 50 feet from Source	
		L <sub>max</sub>	L <sub>eq</sub>
Backhoe	40	78	74
Blade <sup>a</sup>	40	85	81
Bobcat <sup>b</sup>	40	79	75
Concrete crusher <sup>c</sup>	20	90	83
Crane	16	81	73
Dump truck	40	77	72
Excavator	40	81	77

Equipment	Acoustical Utilization Factor (%)	Typical Noise Level (dBA) at 50 feet from Source	
		L <sub>max</sub>	L <sub>eq</sub>
Generator	50	81	78
Gradall	40	83	79
Pickup truck	40	75	82
Pile driver	20	101	94
Scraper	40	84	80
Tire wash <sup>d</sup>	10	82	72

Source: Federal Highway Administration. 2006, FHWA Roadway Construction Noise Model User's Guide. FHWA-HEP-05-054. January.

Notes:

<sup>a</sup> Based on FHWA noise level for a grader.

<sup>b</sup> Based on FHWA noise level for a front-end loader.

<sup>c</sup> Based on FHWA noise level for a concrete saw.

<sup>d</sup> Based on FHWA noise level for a vacuum street sweeper.

Construction activities could result in noise that would exceed the Menlo Park 85 dBA limit for powered equipment at 50 feet during the hours between 8:00 a.m. and 6:00 p.m. and be in excess of 60 dBA equivalent sound level ( $L_{eq}$ ) during “daytime” (7:00 a.m. to 10:00 p.m.) hours before or after this range, an adverse effect. Measures to address potential noise impacts are described below; however, measures to address potential noise impacts on sensitive species are discussed separately under Section 4.2.

#### **Mitigation Measure to Address Noise Impacts during Construction:**

- To the extent feasible, the noisiest construction activities, such as demolition and grading activities, will be scheduled during times that would have the least impact on nearby residential and other receptors. This could include restricting construction activities in the areas of potential impact to the early and late hours of the workday, such as from 8:00 a.m. to 10:00 a.m. or 4:00 p.m. to 6:00 p.m., Monday through Friday.
- Best available noise control techniques (e.g., improved mufflers, equipment redesign, intake silencers, ducts, engine enclosures, acoustically attenuating shields or shrouds) will be used on equipment and trucks used for project construction wherever feasible.
- Hydraulically or electrically powered impact tools (e.g., pile drivers, jackhammers, pavement breakers, rock drills) will be used for project construction wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler will be used on the compressed air exhaust. External jackets will be used on the tools themselves where feasible. Quieter equipment, such as drills, will be used rather than impact equipment whenever feasible.

- “Quiet” gasoline-powered compressors or electric compressors will be used and electric rather than gasoline-or diesel-powered forklifts will be used for small lifting to the extent feasible.
- Stationary noise sources, such as temporary generators, will be located as far from nearby receptors as possible; such sources will be muffled and enclosed within temporary enclosures and shielded by barriers or other measures to the extent feasible.
- Temporary noise barriers (generally approximately 8 feet in height) will be installed around construction areas adjacent to sensitive receptors to reduce construction noise from equipment to acceptable levels. Specifically, the noise barriers will reduce noise levels during the hours of 8:00 a.m. to 6:00 p.m. on weekdays to 85 dBA at a distance of 50 feet from the construction equipment. In addition, the noise barriers will reduce overall construction noise to less than 60 dBA  $L_{eq}$ , as measured at the applicable property lines of adjacent uses, during the hours of 7:00 a.m. to 8:00 a.m. and 6:00 p.m. to 10:00 p.m. weekdays and 7:00 a.m. to 10:00 p.m. on Saturdays. The noise barriers will be installed unless an acoustical engineer submits documentation that confirms that the barriers are not necessary to achieve these attenuation levels or provides specific locations and heights that would achieve the required attenuation.
- Trucks will be prohibited from idling along streets serving the construction site.
- Prior to any pile-driving activities, all surrounding property owners and occupants within 300 feet of the project site will be notified, informing them of the estimated start date and duration.
- “Quiet” pile-driving technology (e.g., vibratory pile driving or pre-drilled pile holes) will be implemented where feasible, in consideration of geotechnical and structural requirements and conditions.
- The effectiveness of noise attenuation measures will be monitored by taking noise measurements during pile-driving activities to ensure compliance with the 85-dBA standard at 50 feet for construction equipment and during general construction occurring during non-exempted daytime hours to ensure compliance with the 60-dBA  $L_{eq}$  daytime standard.

Following construction, visitor activities would not substantially affect noise levels. The anticipated noise levels generated by the passive recreational uses would not substantially alter the ambient noise environment; especially given recreational use (i.e., Bay Trail) already exists adjacent to the project site. The low and occasional noise levels generated by recreational users would not be noticeable from off-site locations, especially in relation to the distance from sensitive receptors. Noise generated by area roadways and highways, railroads, and overflights, the dominant noise source in the area, would be much higher than any noise generated from passive recreational users.

With implementation of the mitigation measures described above, the Service anticipates no substantial adverse effect due to noise from Alternative B.

## 4.2 Biological Environment

### 4.2.1 Vegetation

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effect on vegetation due to the no action alternative.

**Alternative B.** The proposed project would result in the permanent replacement of up to 0.19 acre of ruderal grassland with a 10-foot wide asphalt pathway. Project implementation would also result in temporary impacts on up to 0.4949 acre of ruderal grassland habitat. Areas subject to temporary disturbance during construction would be seeded, as appropriate, following completion of construction.

Ruderal grassland is relatively abundant and widespread regionally, and is not particularly sensitive or valuable (from the perspective of providing important plant or wildlife habitat), and impacts on the project site would not result in substantial impacts on the vegetation community that occurs there. Further, the ruderal grasslands on the project site are characterized by an abundance of non-native species including yellow-star thistle, bristly oxtongue, and black mustard. Therefore, the potential introduction of non-native seeds to the site by construction equipment is not expected to substantially degrade the existing community. Further, the project would not result in any impacts on sensitive natural communities. Thus, the Service anticipates no substantial adverse effect on vegetation due to Alternative B.

### 4.2.2 Wildlife

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effect on common (i.e., non-sensitive) wildlife due to the no action alternative.

**Alternative B.** Implementation of Alternative B could result in impacts on common (i.e., non-sensitive) wildlife due to injury or mortality of individuals by equipment, vehicle traffic, and worker foot traffic. In addition, clearing and grading would result in the loss of breeding and/or foraging habitat. The area to be disturbed under Alternative B provides limited habitat value due to the lack of significant vegetation and the proximity to SR 84. This relatively narrow, linear corridor of habitat loss would have little, if any, impact on the wildlife species that occur on the project site, which are regionally abundant, present in widely available habitats in the region, and would continue to be present on the site following construction. Additionally, the proposed project would affect only a small proportion of their regional populations, and the number of individuals likely to be displaced by habitat disturbance and loss would be quite small with respect to the amount of suitable habitat available in the area. The habitat within the project site is also not essential for the support of any local population of fish or wildlife species and does not include any designated critical habitat, and the implementation of Alternative B would not substantially interfere with the movement of native resident or migratory wildlife species. Further, the project design under Alternative B includes installation of a low fence north of the trail edge between the trail and the remaining Refuge property. The fence would

minimize the potential for disturbance to habitats managed and maintained by the Refuge to support mammals, birds, and other wildlife. Therefore, the Service anticipates no substantial adverse effect on non-sensitive wildlife due to Alternative B.

#### 4.2.3 Sensitive Species

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effect on sensitive species due to the no action alternative.

**Alternative B.** Although several federally listed species, such as the California Ridgway's rail, salt marsh harvest mouse, Central California Coast steelhead, and southern green sturgeon occur in the general project vicinity, no suitable habitat for these species is present within 0.5 mile or more of the project site. As a result, the project would have no effect on these species. Only one Federal or State listed species, the western snowy plover, could potentially be affected by Project activities.

Western snowy plovers are not expected to nest or forage within the project site due to the extent of urban development, roadways, and general lack of suitable habitat. Therefore, no direct loss of habitat, eggs, or nests, of individuals would occur as a result of the project. There is some potential for project construction to disturb individuals, habitat, and nests located north of the project site in Pond R3. Disturbances such as loud noise or the presence and movement of people and heavy equipment near snowy plover habitat may alter bird behavior. Such disturbance could result in temporary plover avoidance of areas that have suitable habitat but intolerable levels of disturbance; abandonment of nests, eggs, or young by nesting pairs; a reduction in foraging efficiency if high-quality foraging areas are impacted; and increased movement or flushing that reduce energy reserves and increase predation risk. Examples of project activities that may cause such disturbance if they occur near (i.e., within 600 feet of) active snowy plover nests include pile installation and general grading, excavating, and backfilling activities. In addition, loss of individual snowy plovers (including eggs and chicks) due to predation could be exacerbated by the proposed action, at least in localized areas. Human food and trash could attract ravens (*Corvus corax*) or other predators, and temporary construction fencing could provide perch sites for predatory raptors.

However, as discussed above, only a single snowy plover nest has been documented within 600 feet of the project site since 2009, and this nest, although located within approximately 400 feet of the easternmost extent of the Bay Trail within the project site boundary, is located over 790 ft from the multi-use bridge overlooking the Refuge. Construction activities along the Bay Trail east and west of the proposed bridge would be minimal, primarily involving the installation of the Bay Trail fence, and would not involve the use of pile driving equipment, large cranes, or other heavy construction equipment necessary for the construction of the bridge. Thus, noise levels associated with construction within this portion of the project site, and hence the potential for disturbance of nesting snowy plovers, are expected to be substantially lower than noise levels associated with bridge construction. In addition, moderate to fairly heavy human use already exists along the Bay Trail adjacent to snowy plover habitat in the project area. Numerous visitors and local residents use the Bay Trail on a daily basis. As a result, the plovers that use habitat adjacent to the project area are habituated to

the presence of humans to some extent. This reduces the extent to which snowy plovers will be disturbed by bridge construction and human use of the bridge following project completion. Nevertheless, project implementation could result in the disturbance of nesting western snowy plovers, a substantial adverse effect. Measures to address potential impacts on the western snowy plover are described below.

**Mitigation Measure to Address Impacts on the Western Snowy Plover:**

- A Service-approved biologist will conduct employee education training for all construction personnel. Personnel will be required to attend the presentation, which will describe the western snowy plover; avoidance, minimization, and conservation measures; legal protection of the animal; and other related issues. All attendees will sign an attendance sheet along with their printed name, company or agency, email address, and telephone number. The original sign-in sheet will be sent to the Service within seven calendar days of the completion of the training.
- Nighttime work near Pond R3 will be avoided to the extent feasible. If nighttime work cannot be avoided, lighting will be directed to the work area and away from habitat for the western snowy plover.
- The bridge and northern bridge approaches will incorporate perch deterrents so that snowy plover predators and nest predators such as raptors, ravens, and crows (*Corvus brachyrhynchos*), cannot use the bridge as a plover hunting perch.
- No activities will be performed within 600 feet<sup>3</sup> of an active snowy plover nest during the snowy plover breeding season, March 1 through September 14 (or as determined through surveys).
- If construction activities are initiated during the western snowy plover breeding season, within 3 days prior to the start of construction, a survey will be conducted by a qualified biologist to determine whether western snowy plovers are nesting within 600 feet of the project site. If an active nest is identified, a buffer of 600 feet will be established between project activities and the nest, and the nest will be periodically monitored by a qualified biologist to determine when it is no longer active (at which point the buffer will no longer be needed). If there is a visual barrier, such as screening, between a project activity and the nest, such that the plover will not be able to see the activity from the nest, then the applicant will coordinate with the Service to determine what reduced buffer will be appropriate to allow work to occur without disturbing the nesting plovers.

In addition, the project includes several measures that are beneficial to snowy plovers. These include fence construction, which may benefit western snowy plovers using pond R3 to the north of the site by reducing access by humans and pets, and funding of on-going western snowy plover monitoring and management. With implementation of the mitigation measures described above, the Service anticipates no substantial adverse effect on sensitive species due to Alternative B.

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<sup>3</sup> This distance is based on the minimum buffer required between active western snowy plover nests and maintenance/monitoring activities conducted under the Biological Opinion issued for the SBSRP Long-term Plan and the Project-level Phase 1 Actions (Service 2008).

## 4.3 Social and Economic Environment

### 4.3.1 Recreation

**Alternative A (No Action).** Under the no action alternative, no new recreation or public access facilities would be constructed and the existing Bay Trail along SR 84 would continue to be used and maintained separately. Therefore, the Service anticipates no adverse effect on recreation due to the no action alternative.

**Alternative B.** Under Alternative B, there would be new public access and recreational facilities that would increase recreational enjoyment of the Ravenswood Complex. Improved recreation and public access facilities, including addition of viewing facilities, would be a beneficial impact on recreation resources. Their provision is consistent with Service and Bay Conservation and Development Commission (BCDC) policies to provide public access and wildlife viewing opportunities.

A new connection to the Bay Trail from the proposed bridge over SR 84 would provide an additional access opportunity, and may reduce access demand from the existing trailhead at Bedwell Bayfront Park. This alternative would not substantially increase recreation use or cause substantial physical deterioration of adjacent recreational facilities.

Under Alternative B, the portion of the Bay Trail adjacent to the project site would be temporarily rerouted during construction. However, no activities would occur that would result in the closure of the Bay Trail. Therefore, the Service anticipates no substantial adverse effect on recreation due to Alternative B.

### 4.3.2 Local Economy

**Alternative A (No Action).** Under the no action alternative, no new recreation or public access facilities would be constructed and the existing Bay Trail along SR 84 would continue to be used and maintained separately. Recreation activities would remain similar to existing conditions, and would not be expected to change business conditions in the long term. No impact to area businesses would occur, and there would be few, if any, substantial changes in the local employment opportunities or community tax bases. Therefore, the Service anticipates no adverse effect on the local economy due to the no action alternative.

**Alternative B.** Under Alternative B, the proposed recreation and public access features could have a small but beneficial effect on the lifestyles and social interactions of the communities surrounding the project site. With more access to outdoor activities, the effects of this increase in opportunities for recreation would be beneficial due to Alternative B.

### 4.3.3 Cultural Resources

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no adverse effect on cultural resources due to the no action alternative.

**Alternative B.** No potential disturbance of known cultural resources would occur because of Alternative B. As described above, the project area is underlain by late Holocene bay mud. Project impacts would be confined to the bay mud, which has no potential for harboring unique paleontological resources. Therefore, there is no need for additional consideration of paleontological impacts. However, the project site also lies within an area once occupied by the Ohlone group of Native Americans and there is a low potential that previously undocumented cultural resources are present below the surface that were not evident during the survey. If archaeological resources were encountered during project construction, such resources could be damaged or destroyed, resulting in an adverse effect. Measures to address potential impacts on archaeological resources are described below.

**Mitigation Measure to Address Impacts on Archaeological Resources:**

Prior to grading or other construction-related activities on the project site, the project sponsor will hire a qualified professional archaeologist (i.e., one who meets the Secretary of the Interior’s professional qualifications for archaeology or one under the supervision of such a professional) to monitor, to the extent determined necessary by the archaeologist, project-related earth-disturbing activities. In the event that any prehistoric or historic-period subsurface archaeological features or deposits, including locally darkened soil (midden), that could conceal cultural deposits, animal bone, obsidian, and/or mortar are discovered during demolition/construction-related earthmoving activities, all ground-disturbing activity within 100 feet of the discovery will be halted immediately. The Service will consult with the project archaeologist to assess the significance of the find. Impacts on any significant resources will be mitigated through data recovery or other methods determined adequate by the Service that are consistent with the Secretary of the Interior’s Standards for Archaeological Documentation. If Native American archaeological, ethnographic, or spiritual resources are discovered, all identification and treatment of the resources will be conducted by a qualified archaeologist and Native American representatives who are approved by the local Native American community as scholars of the cultural traditions. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected will be consulted. When historic archaeological sites or historic architectural features are involved, all identification and treatment is to be carried out by historical archaeologists or architectural historians who meet the Secretary of the Interior’s professional qualifications for archaeology and/or architectural history.

With implementation of the mitigation measures described above, the Service anticipates no substantial adverse effect on cultural resources due to Alternative B.

**4.3.4 Traffic**

**Alternative A (No Action).** Under the no action alternative, no new activities would be implemented. Therefore, the Service anticipates no substantial adverse effect on traffic due to the no action alternative.

**Alternative B.** Impacts of project construction were previously analyzed as part of the larger Facebook Campus Expansion Project (ICF International 2016). Based on the results of that analysis, project

construction may result in an adverse effect on routes of regional significance and daily traffic volumes on roadways in the project area due to project traffic. Measures to address potential traffic impacts are described below.

**Mitigation Measure to Address Traffic Impacts:**

- The project sponsor will provide measures to reduce cut-through traffic in the Belle Haven Neighborhood via Chilco Street (South of the Dumbarton Rail Corridor), Newbridge Street, and Ivy Drive. The provision of physical traffic-calming measures could also be included where such measures would not affect emergency access and/or transit service, subject to community and City approval. Because community members and other potentially affected stakeholders may be affected by such improvements, the project sponsor will fund a Neighborhood Traffic Plan to identify appropriate measures for reducing cut-through traffic.
- The project sponsor will provide measures to improve mobility options (e.g., walking, bicycling, transit), consistent with the City’s Complete Streets goals, which would help to offset the effect of daily traffic generated by the project. In particular, such measures could include pedestrian enhancements across Willow Road at Hamilton Drive, Ivy Drive, and Newbridge Street as well as at other affected study segment locations.

With implementation of these mitigation measures, the Service anticipates no substantial adverse effect on traffic due to Alternative B.

**4.4 Cumulative Effects**

Cumulative effects are those effects on the environment resulting from incremental consequences of the Service’s proposed actions when added to other past, present, and reasonably foreseeable future actions, regardless of who undertakes those actions. Cumulative effects can be the result of individually minor impacts that can become significant when added over time. This section assesses how these other activities in addition to the proposed action would affect the physical, biological, and social and economic environment.

Cumulative effects take into account projects on and in the vicinity of the Refuge. Other projects in the area include both development and maintenance projects that could adversely affect sensitive species and restoration projects that will benefit these species. Development projects include the Facebook Campus Expansion Project and restoration projects, which are described in the CCP, including the SBSRP, Initial Stewardship Plan, South San Francisco Bay Corps Shoreline Study, San Francisco Estuary Invasive Spartina Project. In addition, additional office space, hotels, and schools are proposed or under construction in the project area, including a research and development campus at 333 Ravenswood Avenue, a hotel at 555 Glenwood Avenue, a hotel/restaurant/health club on Constitution Drive, the Laurel Upper School at 275 Elliot Drive, and the German American School on 475 Pope Street.

#### 4.4.1 Cumulative Effects on the Physical Environment

The implementation of the proposed action would result in a minimal impact on the physical environment. Construction of the Bay Trail and staging of equipment for construction of a bridge over SR 84 would occur entirely within ruderal grassland habitat and there would be no direct impact or conversion of sensitive natural communities. Most of the other foreseeable projects in the San Francisco Bay area that will affect Refuge habitat are tidal restoration projects. These projects are expected to result in the restoration of thousands of acres of much higher-quality habitat than would be impacted by the proposed project. Further, the project would implement a number of BMPs and mitigation measures to reduce impacts on water quality and contaminants, air quality, noise, and traffic as described above.

Construction noise effects may result in a cumulative noise impact on nearby sensitive receptors. As discussed under Section 4.1.5, construction activity associated with the project would generate noise that could affect existing adjacent land uses. However, with implementation of the proposed mitigation measure to address noise impacts during construction, such impacts are expected to be less than significant. Further, it is likely that noise mitigation measures similar to those of the project would be applied to other projects in the area for compliance with applicable City noise standards. Given this, and the fact that construction noise is highly localized, it is not anticipated that significant cumulative construction noise impacts would occur.

Thus, provided the proposed project successfully incorporates the mitigation measures described in this EA, neither Alternative A nor Alternative B would contribute to substantial cumulative effects on the physical environment.

#### 4.4.2 Cumulative Effects on Biological Resources

Other projects near salt-marsh and managed pond habitat in the Bay could result in potentially significant impacts on salt-marsh associated species such as the western snowy plover, California Ridgway rail, salt marsh harvest mouse, and other special-status species. However, most of the other foreseeable projects within these species' habitats are restoration projects that will have long-term benefits for native wildlife species and habitats in the area. The SBSPRP will restore tidal marsh and other wetlands in the South Bay, providing additional habitat to wildlife resources, including extensive habitat for special-status species, migratory waterfowl and shorebirds, and fish and other aquatic species.

In addition to the impacts described in Section 4.2 above, climate change may also have cumulative effects on species that utilize tidal marshes. The global average temperature has risen by approximately 0.6 degrees centigrade during the 20th Century (IPCC 2007, Adger et al. 2007). There is an international scientific consensus that most of the warming observed has been caused by human activities (IPCC 2001 2007, Adger et al. 2007), and that it is "very likely" that it is largely due to anthropogenic emissions of CO and other greenhouse gases (Adger et al. 2007). Ongoing climate change (Inkley et al. 2004, Adger et al. 2007, Kanter 2007) likely imperils species like the Ridgway's rail and salt marsh harvest mouse, and the resources necessary for their survival, because climate change threatens to disrupt annual weather patterns, it may result in a loss

of their habitats and/or prey, and/or increased numbers of their predators, parasites, and diseases. Where populations are isolated, increasing tide height due to changing climate may result in local extinction, with range shifts precluded by lack of suitable habitat at higher elevations. However, the project would not result in the loss of any tidal marsh habitat, and tidal restoration projects that are currently being implemented, such as the SBSPRP and South San Francisco Bay Corps Shoreline Study, are taking sea level rise into consideration and incorporating broad upland transition zones where possible to allow for the upward “migration” of tidal marsh and high-tide refugia.

Further, the implementation of the proposed action would result in a minimal impact on biological resources as no sensitive plant or wildlife species are expected to occur on the project site and potential temporary impacts on nesting western snowy plovers during project construction would be adequately mitigated. Thus, provided the proposed project successfully incorporates the mitigation measures described in this EA, neither Alternative A nor Alternative B would contribute to substantial cumulative effects on biological resources.

#### **4.4.3 Cumulative Effects on the Social and Economic Environment**

The proposed project is expected to result in a positive benefit on the social and economic environment by providing additional recreational and transportation opportunities. Under Alternative B, the portion of the Bay Trail adjacent to the project site would be temporarily rerouted during construction. However, no activities would occur that would result in the closure of the Bay Trail. Therefore, the Service anticipates no substantial adverse effect on recreation.

Most of the other foreseeable projects in the area would also result in beneficial impacts for residents of the Bay Area through expansion of wildlife-dependent recreation, interpretation, and environmental education. Further, the Service adheres to the policies and regulations pertaining to the protection of cultural resources in order to avoid or mitigate for any significant adverse effects resulting from management activities. The actions in the proposed project would continue to adhere to those policies and regulations. Thus, provided the proposed project successfully incorporates the mitigation measures described in this EA, neither Alternative A nor Alternative B would contribute to substantial cumulative effects on the social and economic environment.

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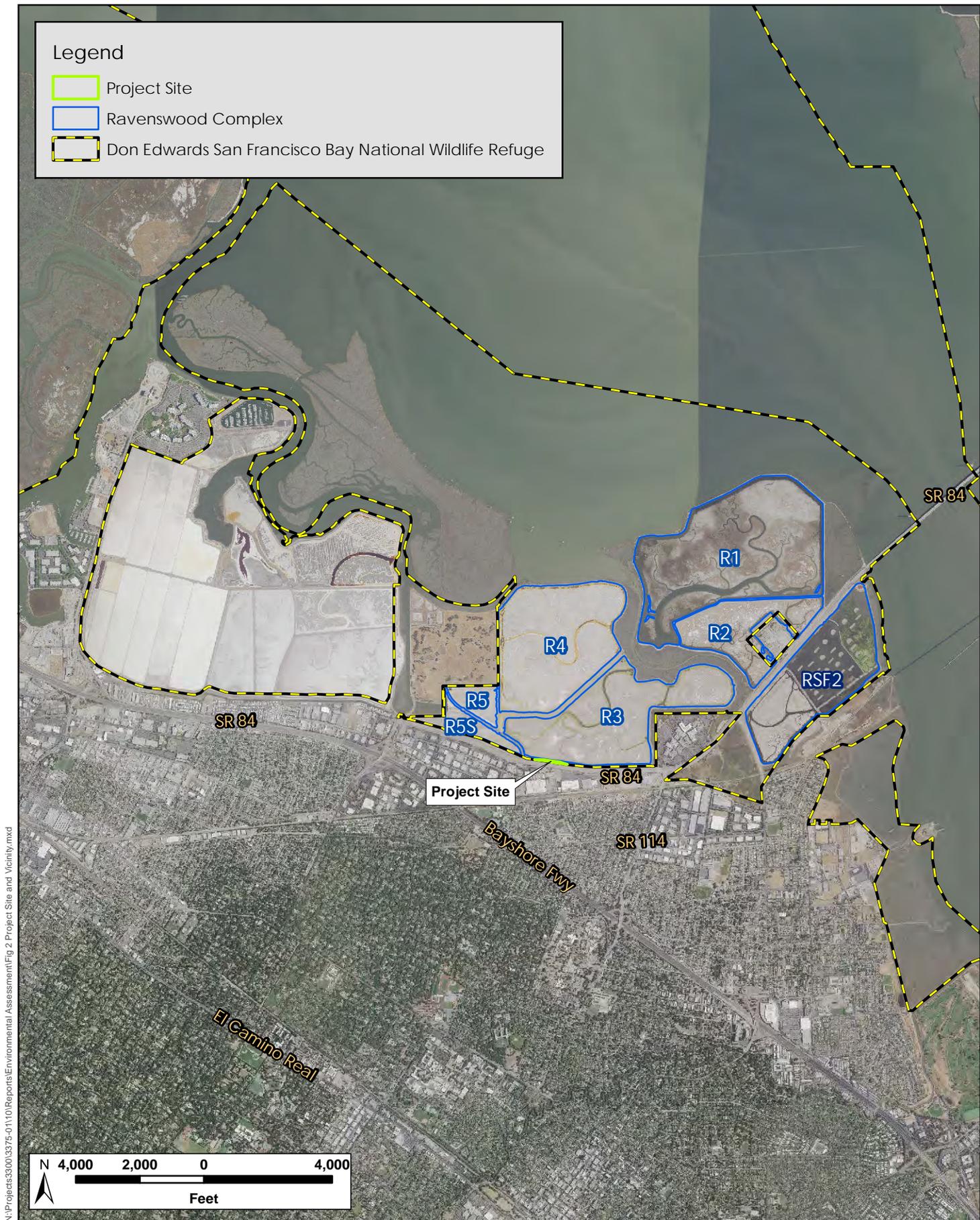
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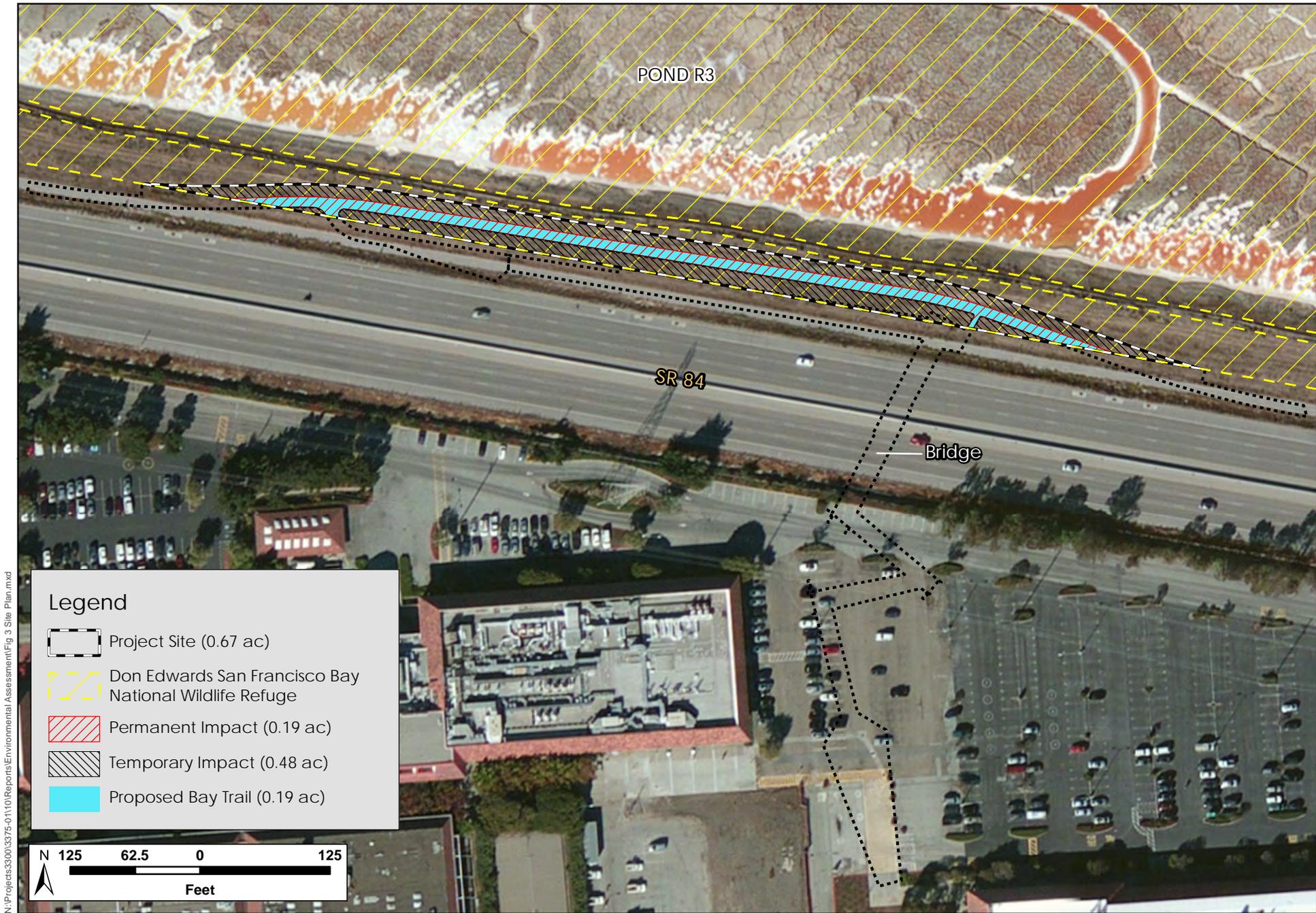
N:\Projects\3300\3375-01\10\Reports\Environmental Assessment\Fig 1 Don Edwards National Wildlife Refuge.mxd

Figure 1. Don Edwards San Francisco Bay National Wildlife Refuge  
 Facebook Multi-Use Bridge Project Environmental Assessment (3375-10)  
 May 2017



N:\Projects\3300\3375-01\10\Reports\Environmental Assessment\Fig. 2, Project Site and Vicinity.mxd

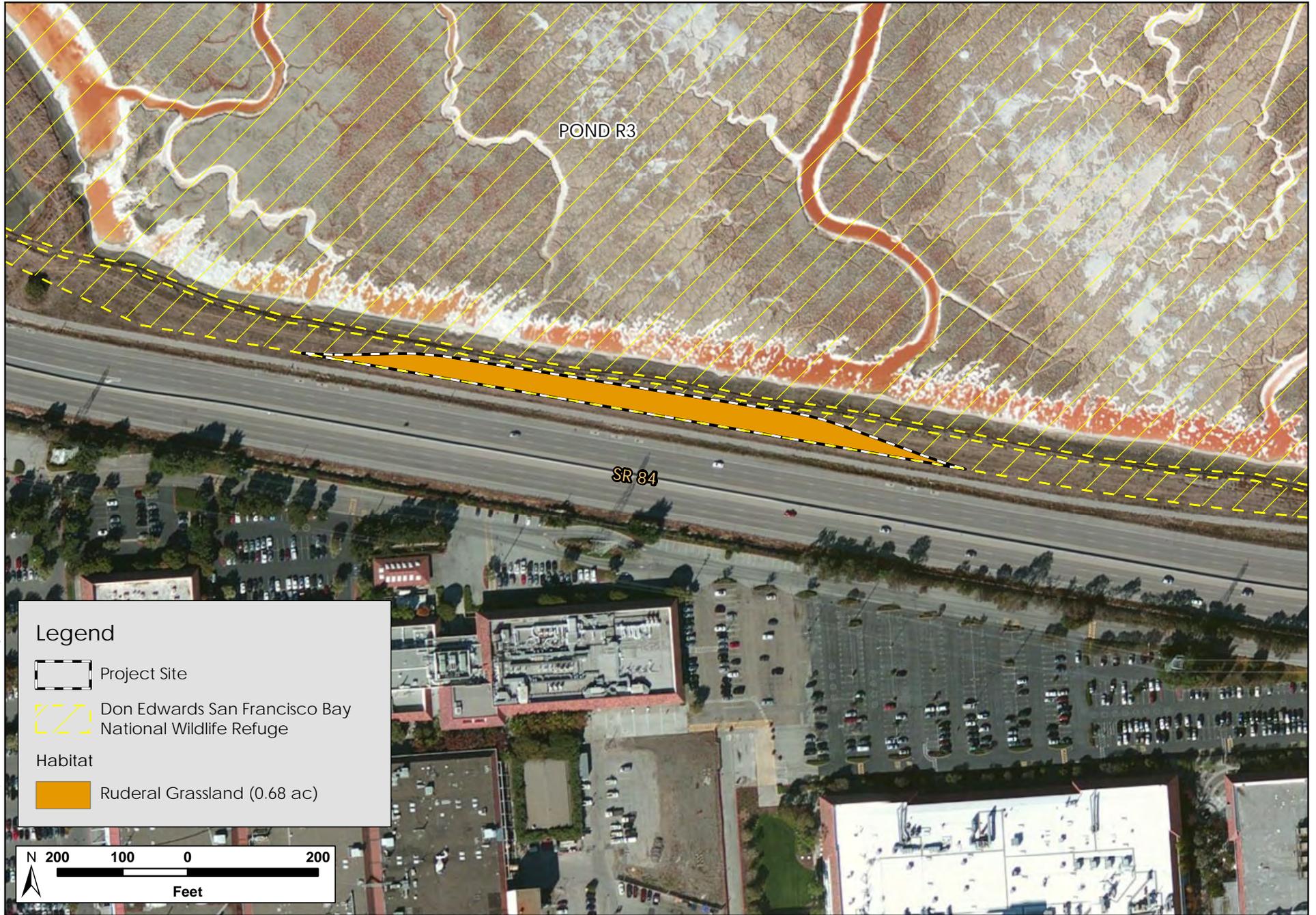
Figure 2. Project Site and Vicinity  
 Facebook Multi-Use Bridge Project Environmental Assessment (3375-10)  
 May 2017



N:\Projects\3300\3375-01\10\Reports\Environmental Assessment\Fig. 3 Site Plan.mxd

Figure 3. Site Plan

N:\Projects\3300\3375-01\10\Reports\Environmental Assessment\Fig 4 Habitat Map.mxd



### Legend



Project Site



Don Edwards San Francisco Bay  
National Wildlife Refuge

### Habitat



Ruderal Grassland (0.68 ac)



Figure 4. Habitat Map

Facebook Multi-Use Bridge Project Environmental Assessment (3375-10)

May 2017

U.S. Department of the Interior  
Fish and Wildlife Service  
Pacific Southwest Region

**FINDING OF NO SIGNIFICANT IMPACT**

**Environmental Assessment for Issuance of a Special Use Permit to Facebook for Use of the  
Refuge during Construction of a Multi-Use Bridge and Trail at  
Don Edwards San Francisco Bay National Wildlife Refuge  
Alameda County, California**

The U.S. Fish and Wildlife Service (Service) has completed an Environmental Assessment (EA) of the environmental effects of issuance of a General Activities Special Use Permit (SUP) to Facebook for use of Don Edwards San Francisco Bay National Wildlife Refuge (Refuge) property during construction of a multi-use bridge over State Route (SR) 84, as well as construction and placement of a portion of the Bay Trail on the Refuge. The purpose of the project is to expand public priority uses (e.g., wildlife-oriented recreational opportunities) by facilitating construction of a multi-use bridge over SR 84 and relocating a portion of the Bay Trail onto Refuge property.

The Refuge was established with three major purposes: (1) the preservation of the natural resources of the South San Francisco Bay (Bay), (2) the provision of environmental education and wildlife interpretation opportunities to Bay Area schools and residents, and (3) the protection of an important open space resource and other wildlife-oriented recreation opportunities for the enjoyment of local residents and visitors. Implementation of the selected action will help to fulfill the Refuge's major purposes.

**Decision**

Following review and analysis, the Service selected the Project Alternative for implementation because it is the alternative that will assist the Service in:

- Achieving the mission of the National Wildlife Refuge System;
- Achieving the purposes of the Don Edwards San Francisco Bay National Wildlife Refuge;
- Implementing the Service's vision and goals for the Refuge;
- Expanding access to the Refuge for use by the general public.

**Alternatives Considered**

The following is a brief description of the alternatives evaluated in the EA.

*Alternative A*

Under the no action alternative, an SUP would not be issued. Facebook would not construct a multi-use bridge over SR 84 enhancing connectivity between the Belle Haven neighborhood, and a portion of the existing Bay Trail south of Pond R3 would not be realigned to the north within Refuge property.

### *Alternative B (selected alternative)*

Under the proposed action, the Refuge would issue an SUP for Facebook's use of Refuge property during construction of a multi-use bridge over SR 84. Although a portion of the aerial walkway would cantilever beyond the Caltrans easement, the foundation and vertical supports of the bridge touchdown north of SR 84 would be located entirely within the Caltrans ROW adjacent to the Bay Trail and the footprint would not extend into the Refuge. However, temporary access to Refuge lands would be needed to facilitate construction of the northern bridge landing (sufficient space is not available within the Caltrans ROW) and to provide a staging area for construction equipment. In addition, as described above, an approximately 800-foot long section of the existing east-west running Bay Trail would be realigned to the north, into the Refuge ROW, to provide east-west connectivity around the bridge's northern ramp. The trail would be paved with asphalt. Further, a low fence would be installed north of the Bay Trail within Refuge property.

### **Effects of management of the Refuge on the human environment**

As described in the EA, implementing the selected alternative is not expected to have significant impacts on any of the environmental resources identified in the EA. A summary of the impacts analysis and conclusions follows:

#### *Cultural Resources*

Records search and pedestrian surveys did not locate prehistoric or historic-era resources, therefore construction. Prior to grading or other construction-related activities on the project site, the project sponsor will hire a qualified professional archaeologist to monitor, to the extent determined necessary by the archaeologist, project-related earth-disturbing activities. In the event that any prehistoric or historic-period subsurface archaeological features or deposits, including locally darkened soil (midden), that could conceal cultural deposits, animal bone, obsidian, and/or mortar are discovered during demolition/construction-related earthmoving activities, all ground-disturbing activity within 100 feet of the discovery will be halted immediately. Impacts on any significant resources will be mitigated through data recovery or other methods determined adequate by the Service that are consistent with the Secretary of the Interior's Standards for Archaeological Documentation. Thus, the Service has concluded that implementation of the selected action will have no significant impact to water quality.

#### *Geology and Soils*

Localized soil disturbance (e.g., compaction and erosion) would occur within the project site due to the use of motorized heavy equipment and foot traffic during construction. However, this impact would be temporary. Project implementation would also involve grading to construct the realigned portion of the Bay Trail within Refuge property. The project sponsor will be required to implement the specifications in the 2013 California Building Standards Code, which regulates grading activities, including associated drainage and erosion control. Therefore, there would be no substantial adverse effect related to geology, soils, and seismicity from selected alternative. The Service has concluded that implementation of the selected action will have no significant impact to soils.

#### *Biological Resources*

Potential impacts to biological resources are not significant. Construction and activities would result in temporary disturbance to wildlife and habitats; however, the disturbance is localized and would not adversely impact overall populations. Construction of the project would not involve work within surface waters and would not alter the course of an existing stream or river because these features do not exist onsite. With implementation of the mitigation measures, the Service anticipates no substantial adverse effect on air quality or the climate of the area. The habitat within the project site is also not essential for the support of any local population of fish or wildlife species and does not include any designated critical habitat, and the implementation of the project would not substantially interfere with the movement of native resident or migratory wildlife species. Further, the project design includes installation of a low fence north of the trail edge between the trail and the remaining Refuge property. The fence would minimize the potential for disturbance to habitats managed and maintained by the Refuge to support mammals, birds, and other wildlife. Thus, the Service has concluded that implementation of the selected action will have no significant impact to biological resources.

#### *Endangered Species*

Although several federally listed species, such as the California Ridgway's rail, salt marsh harvest mouse, Central California Coast steelhead, and southern green sturgeon occur in the general project vicinity, no suitable habitat for these species is present within 0.5 mile or more of the project site. As a result, the project would have no effect on these species. Only one Federal or State listed species, the western snowy plover, could potentially be affected by Project activities. Moderate to fairly heavy human use already exists along the Bay Trail adjacent to snowy plover habitat in the project area. Numerous visitors and local residents use the Bay Trail on a daily basis. As a result, the plovers that use habitat adjacent to the project area are habituated to the presence of humans to some extent. This reduces the extent to which snowy plovers will be disturbed by bridge construction and human use of the bridge following project completion. Nevertheless, project implementation could result in the disturbance of nesting western snowy plovers. Measures to address potential impacts on the western snowy plover will be implemented which will reduce the impacts of the construction to less than significant. Thus, the Service has concluded that implementation of the selected action will have no significant impact to endangered species.

#### *Public Use*

Potential impacts to public use are not significant. Some temporary closures may be required during construction in the immediate vicinity of project area. The portion of the Bay Trail adjacent to the project site would be temporarily rerouted during construction. However, no activities would occur that would result in the closure of the Bay Trail. Therefore, the Service anticipates no substantial adverse effect on recreation due to the project.

#### **Public Review**

The EA was available for public review and comment from June 8, 2017 through June 26, 2017. The document was posted on the Refuge's website and the public was notified of its availability. No comments were received.

**Conclusions**

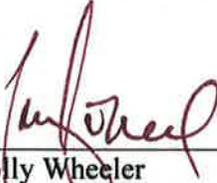
Based on review and evaluation of the information contained in the supporting references, it is my finding that the proposed action, Alternative B, does not constitute a major federal action that would significantly affect the quality of the human environment, within the meaning of section 102(2)(c) of the National Environmental Policy Act of 1969, as amended. Accordingly, the Service is not required to prepare an environmental impact statement.

This Finding of No Significant Impact and supporting references are on file at the U.S. Fish and Wildlife Service, San Francisco Bay National Wildlife Refuge Complex, 1 Marshlands Road, Fremont, California, 94536 (telephone 510/ 792 0222); these documents are available to the public on the Internet at [https://www.fws.gov/refuge/don\\_edwards\\_san\\_francisco\\_bay/](https://www.fws.gov/refuge/don_edwards_san_francisco_bay/) Interested and affected parties will be notified of this decision through a news release.

**Supporting References**

ICF International. 2016. Final Facebook Campus Expansion Project Environmental Impact Report. Prepared for City of Menlo Park. September 2016.

U.S. Fish and Wildlife Service. 2012. Don Edwards San Francisco Bay National Wildlife Refuge Comprehensive Conservation Plan and Environmental Assessment. October 2012.

  
\_\_\_\_\_  
Polly Wheeler  
Assistant Regional Director, Refuges  
Pacific Southwest Region  
U.S. Fish and Wildlife Service

7.3.17  
\_\_\_\_\_  
Date

Attachment: Environmental Assessment Facebook Multi-Use Bridge Final

**PG&E Gas and Electric  
Advice Submittal List  
General Order 96-B, Section IV**

AT&T	Downey & Brand	Pioneer Community Energy
Albion Power Company	East Bay Community Energy	Praxair
Alcantar & Kahl LLP	Ellison Schneider & Harris LLP	
	Energy Management Service	
Alta Power Group, LLC	Engineers and Scientists of California	Redwood Coast Energy Authority
Anderson & Poole	Evaluation + Strategy for Social Innovation	Regulatory & Cogeneration Service, Inc.
	GenOn Energy, Inc.	SCD Energy Solutions
Atlas ReFuel	Goodin, MacBride, Squeri, Schlotz & Ritchie	
BART	Green Charge Networks	SCE
	Green Power Institute	SDG&E and SoCalGas
Barkovich & Yap, Inc.	Hanna & Morton	
P.C. CalCom Solar	ICF	SPURR
California Cotton Ginners & Growers Assn	International Power Technology	San Francisco Water Power and Sewer
California Energy Commission	Intestate Gas Services, Inc.	Seattle City Light
California Public Utilities Commission	Kelly Group	Sempra Utilities
California State Association of Counties	Ken Bohn Consulting	Southern California Edison Company
Calpine	Keyes & Fox LLP	Southern California Gas Company
	Leviton Manufacturing Co., Inc. Linde	Spark Energy
Cameron-Daniel, P.C.	Los Angeles County Integrated Waste Management Task Force	Sun Light & Power
Casner, Steve	Los Angeles Dept of Water & Power	Sunshine Design
Cenergy Power	MRW & Associates	Tecogen, Inc.
Center for Biological Diversity	Manatt Phelps Phillips	TerraVerde Renewable Partners
City of Palo Alto	Marin Energy Authority	Tiger Natural Gas, Inc.
	McKenzie & Associates	
City of San Jose	Modesto Irrigation District	TransCanada
Clean Power Research	Morgan Stanley	Troutman Sanders LLP
Coast Economic Consulting	NLine Energy, Inc.	Utility Cost Management
Commercial Energy	NRG Solar	Utility Power Solutions
County of Tehama - Department of Public Works		Utility Specialists
Crossborder Energy	Office of Ratepayer Advocates	
Crown Road Energy, LLC	OnGrid Solar	Verizon
Davis Wright Tremaine LLP	Pacific Gas and Electric Company	Water and Energy Consulting Wellhead Electric Company
Day Carter Murphy	Peninsula Clean Energy	Western Manufactured Housing Communities Association (WMA)
		Yep Energy
Dept of General Services		
Don Pickett & Associates, Inc.		
Douglass & Liddell		